

CITY OF LANGFORD
REGULAR MEETING OF COUNCIL
Tuesday, May 20th, 2008 @ 7:00 p.m.
Council Chambers, 3rd Floor, 877 Goldstream Avenue

AGENDA

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1. <u>CALL TO ORDER</u>	
2. <u>APPROVAL OF THE AGENDA</u>	
3. <u>PUBLIC HEARINGS</u>	
a) BYLAW NO. 1157 (File No. 3900-1157) "Langford Zoning Bylaw, Amendment No. 229, (2710 Peatt Road), 2008".	
b) BYLAW NO. 1200 (File No. 3900-1200) "Langford Official Community Plan Bylaw, 2008".	
c) BYLAW NO. 1201 (File No. 3900-1201) "Langford Zoning Bylaw, Amendment No. 232, (Design Guidelines), 2008".	
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PUBLIC HEARING
Tuesday, May 20, 2008

Bylaw No. 1157

*A Bylaw to Rezone 2710 Peatt Road from the R1
(One-Family Residential) zone to the
MU1A (Mixed Use Residential Commercial A)
Zone to Allow Approximately Six Townhouses,
Two of Which May Be for Live/Work.*

CITY OF LANGFORD

BYLAW NO. 1157

A BYLAW TO AMEND BYLAW NO. 300, LANGFORD ZONING BYLAW, 1999"

The Council of the City of Langford, in open meeting assembled, hereby enacts as follows:

A. Langford Zoning Bylaw No. 300, 1999 is amended as follows:

1. By deleting from the R1 (ONE-FAMILY RESIDENTIAL) ZONE and adding to the MU1A (MIXED USE RESIDENTIAL COMMERCIAL A) ZONE the property legally described as Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.
2. By adding to the list of permitted uses in Section 6.51.01A in appropriate alphabetical order the words "townhouses, on land legally described as Lot 6 Section 5 Esquimalt District Plan 8120" and renumbering the following subsection accordingly.
3. By replacing the words "the floor area ratio may not exceed 2.0" in Section 6.51.04A(1) with the words "not more than one dwelling unit per 550 m² of lot area may be constructed on any lot and the floor area ratio for all uses may not exceed 0.3" and by replacing the words "the minimum floor area ratio may be increased to 2.0" in Section 6.51.04A(3) with the words "the density of development shall be regulated by floor area ratio only and the maximum floor area ratio is 2.0".
4. By adding the following paragraphs to Section 6.51.04A(3) in alphabetical order:
 - (j) pays to the City the amount specified in Column 11 of Table 1 below at time of building permit, to be deposited in the School Crossing Guards Reserve Fund;
 - (k) pays to the City the amount specified in Column 12 of Table 1 below at time of building permit, to be deposited in the Affordable Housing Reserve Fund;
 - (l) pays to the City the amount specified in Column 13 of Table 1 below at the time of building permit, to be deposited in the Equipment Replacement Reserve Fund.

A. By adding to Table 1 in the section 6.51.04(A)

Table 1

1. Legal description	2.	3.	4.	5.	6.	7.	8.	9.	10.	11.	12.	13
Lot 6, Section 5, Esquimalt District, Plan 8120 (2710 Peatt Road)	\$500 per dwelling unit	\$100 per dwelling unit	\$0	\$1,000 per dwelling unit	\$0	\$300 per dwelling unit	\$0	\$0	\$0	\$100 per dwelling unit	\$500 per dwelling unit	\$200 per dwelling unit

B. This Bylaw may be cited for all purposes as "Langford Zoning Bylaw, Amendment No. 229, (2710 Peatt Road), 2008".

READ A FIRST TIME this 21ST day of April, 2008.

PUBLIC HEARING held this day of, 2008.

READ A SECOND TIME this day of, 2008.

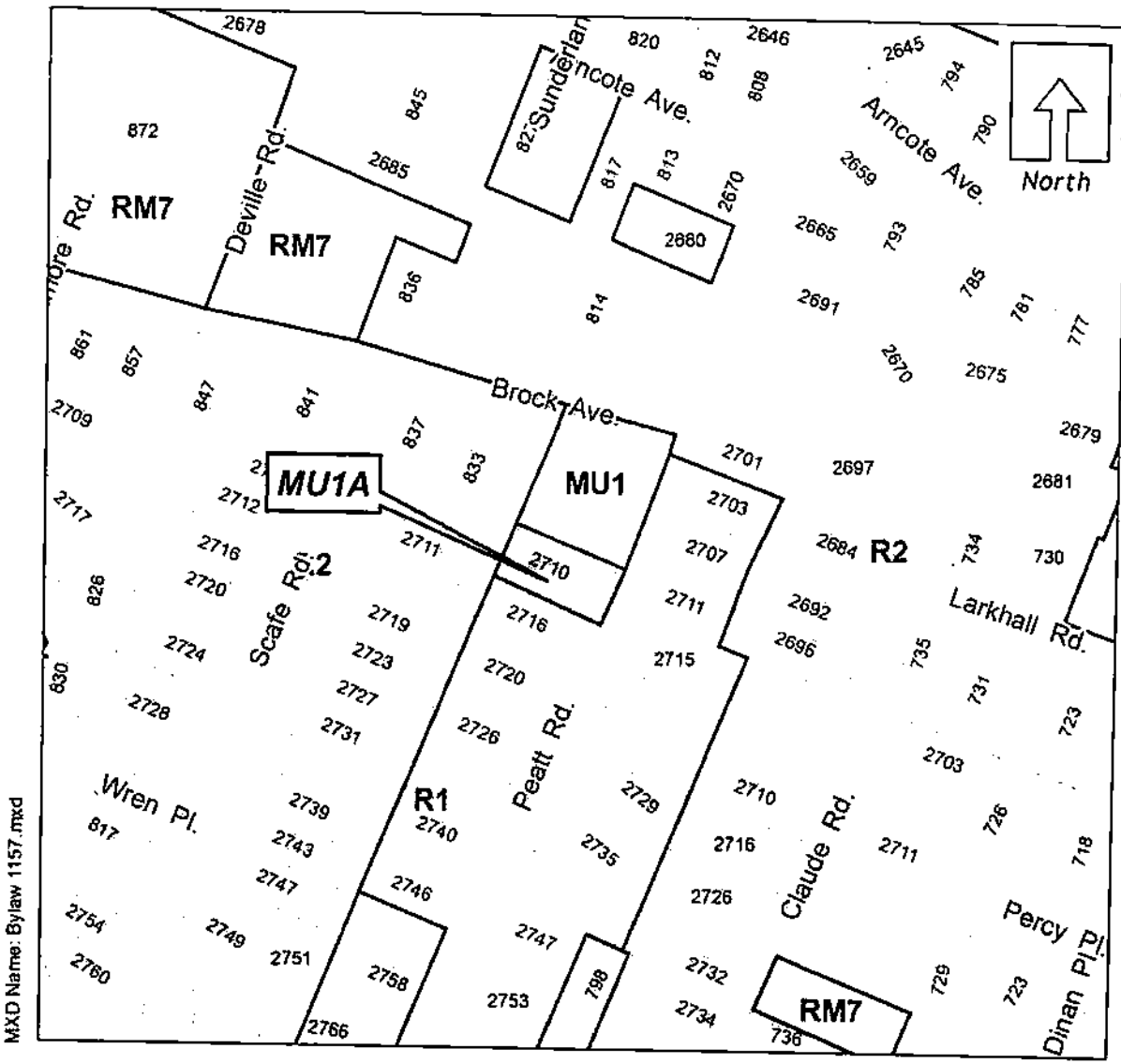
READ A THIRD TIME this day of, 2008.

APPROVED BY THE MINISTRY OF TRANSPORTATION this day of, 2008.

ADOPTED this day of, 2008.

MAYOR

CLERK-ADMINISTRATOR



LEGAL DESCRIPTION: Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.

I HERBY CERTIFY THIS TO BE A TRUE COPY
OF PLAN No. 1 AS DESCRIBED
IN SECTION _____ OF BYLAW No. 1157

MAYOR

ADOPTION: _____

CLERK - ADMINISTRATOR

Scale: N.T.S.

Last Revised: February 08, 2008

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Edward Jones
MAKING SENSE OF INVESTING

Continued from page A11

But 14,940 million tonnes released by Canadians going about their human activities.

Even if we reduce our emissions by 30 per cent tomorrow and maintained that level, we would still generate 4,482 million tonnes over the next 20 years — four times as much as the tree decay.

I just hope this isn't justification for killing healthy trees because they might decay and emit carbon as well. Figure out a way to market the blue fungus stained products as an eco-friendly alternative — I'd buy it.

Karen Kennedy
View Royal

Royal Roads not a 'hybrid' university

Re: Nine universities, more changes ahead. News, April 25, 2003

The story "Nine universities, more changes ahead" contained three inaccuracies about Royal Roads University.

The story said RRU's masters programs are focused on international business when in fact we offer 15 masters programs in many disciplines

— the common element being an applied, professional focus.

The article also said RRU offers third and fourth-year programs for college graduates. While it is true our undergraduate completion degrees cover years three and four of a bachelor degree, they are open to university or college transfer students (not necessarily graduates) — as well as to mature students with academic and life experience equivalent to two years of undergraduate study.

Perhaps most misleading was the description of Royal Roads University as a "hybrid institution" when the term had only been used in the article to describe university colleges. While the land where we are located has been many things in its history, our institution has been a university ever since it was created on June 21, 1995.

Angelo Belcastro
VP, Academic
Royal Roads University

Reworking Millstream Road interchange

There are too many traffic lights around Millstream interchange. The worst is the way from Victoria to big-box stores and from big-box stores to Victoria. If you take this way, you

have to wait at the traffic light then go across Millstream road then again wait at traffic light. It's not only inefficient but also dangerous. On return way, there is same problem.

There is a way to solve these problems. From Victoria, take an exit to Langford and without turning right circle, go straight up to Costco parking lot. To attain this, a new overpass bridge from Langford east to Costco parking lot should be constructed. From Costco parking lot, go through between Costco and Sheraton Hotel, then turn right down to another big-box stores. There is no traffic light and cross over.

On the return way, from Costco parking lot go directly to the other side of Trans-Canada Highway then merge to Victoria.

To attain this, a new long round overpass bridge should be constructed. It may cost much to construct two overpass bridges, but it is still cheaper than to construct Spencer interchange.

Moreover, if we can spend more money, this route can extend to Florence Lake Road and to Bear Mountain Parkway.

This is also good for the residents of Florence Lake and Bear Mountain and will eliminate the necessity to construct Spencer interchange.

Tom Suganami
Blechnon

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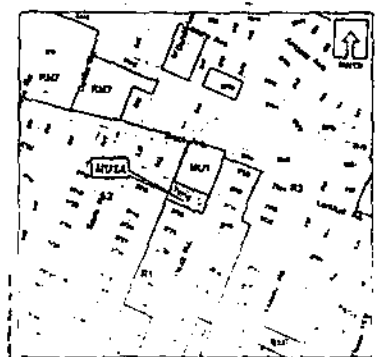
City of Langford Notice of Public Hearing

NOTICE of Public Hearing on Bylaw No. 1157: being the proposed Bylaw to amend Zoning Bylaw No. 300 for the City of Langford, is hereby given. All persons who believe that their interest in property is affected by the proposed Bylaw will be afforded a reasonable opportunity to be heard or to present written submissions respecting matters contained in the Bylaw at the PUBLIC HEARING to be held in the CITY OF LANGFORD COUNCIL CHAMBERS, Third Floor, 677 Goldstream Avenue, Langford, BC, on Tuesday, 20 May 2003, at 7:00 pm. Please note no further submissions will be accepted after the close of the public hearing.

Purpose: The purpose of Bylaw No. 1157 is to amend the City of Langford Zoning Bylaw No. 300 by amending the zoning designation of the land that is the subject of Bylaw No. 1157 from R1 (One-Family Residential) Zone and adding to the MUA (Mixed Use Residential Commercial A) Zone to permit approximately 6 townhouses, two of which may be for live-work.

Applicant: Marie Brotherton

Location: The land that is the subject of Bylaw No. 1157 is 2710 Peatt Road as shown shaded on the plan.



COPIES of the complete proposed Bylaws and other related material may be viewed during office hours 9:30 am to 4:30 pm, Monday to Friday (holidays excluded), from Monday, 5 May 2003 to Tuesday, 20 May 2003, inclusive, at the Langford City Hall, Second Floor, 677 Goldstream Avenue, Langford, BC. Please contact the Planning Department at 474-6919 with any questions in this Bylaw.

Leo Buchan
City Administrator

John Horgan
MLA
Malahat-Juan de Fuca
Community Office

Mon-Fri
10am-4pm

800
Goldstream Ave.
Victoria, BC
V8B 2X7

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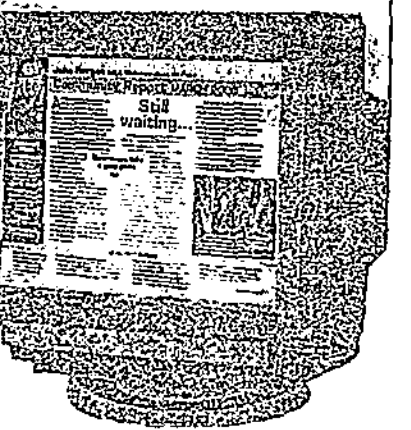
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City of Langford Notice of Public Hearing

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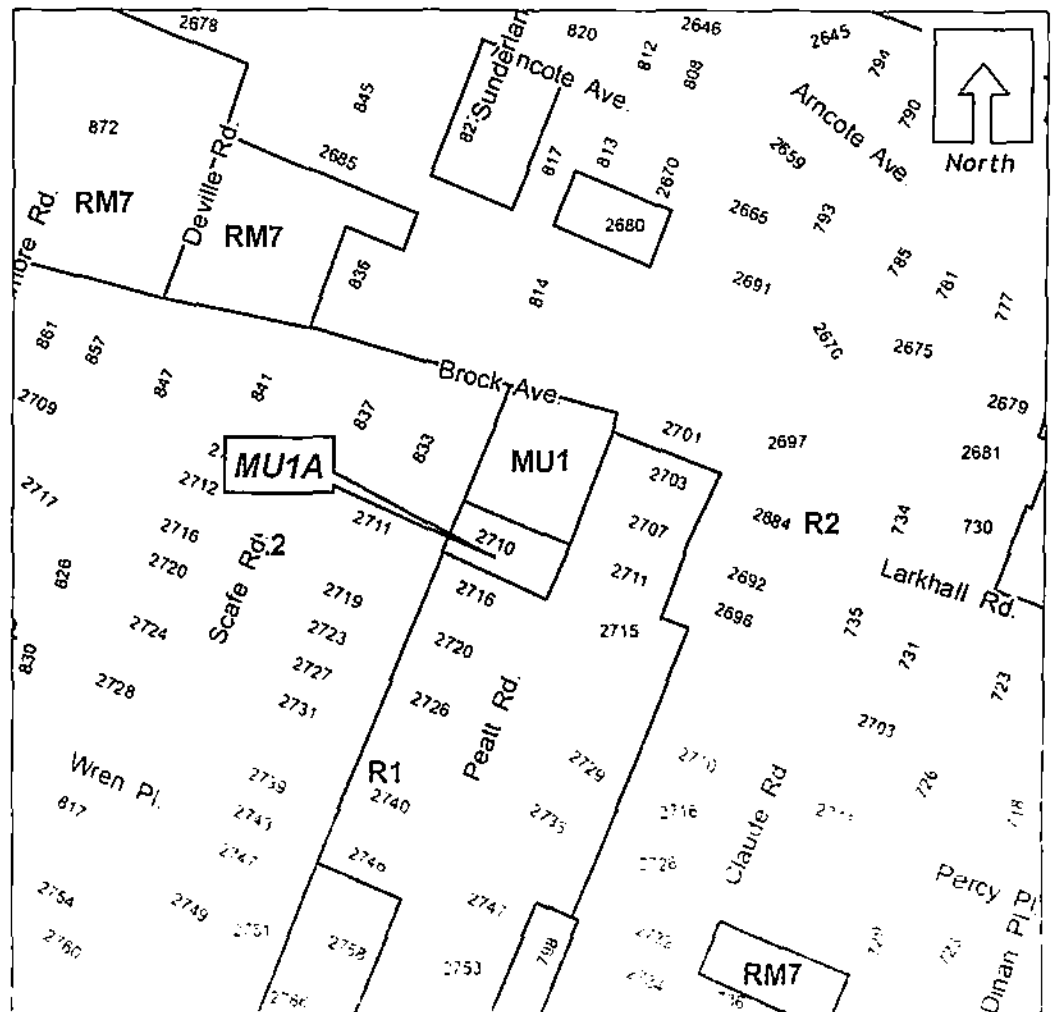
Purpose: The purpose of Bylaw No. 1157 is to amend the City of Langford Zoning Bylaw No. 300 by amending the zoning designation of the land that is the subject of Bylaw No. 1157 from R1 (One-Family Residential) Zone and adding to the MU1A (Mixed Use Residential Commercial A) Zone to permit approximately 6 townhouses, two of which may be for live-work.

Applicant: Marie Brotherston

Location: The land that is the subject of Bylaw No. 1157 is 2710 Peatt Road as shown shaded on the plan.

COPIES of the complete proposed Bylaws and other related material may be viewed during office hours 8:30 am to 4:30 pm, Monday to Friday (holidays excluded), from Monday, 5 May 2008 to Tuesday, 20 May 2008, inclusive, at the Langford City Hall, Second Floor, 877 Goldstream Avenue, Langford, BC. Please contact the Planning Department at 474-6919 with any questions on this Bylaw.

Rob Buchan
Clerk-Administrator





Z-07-43

5 May 2008

Dear Sir/Madam:

City of Langford Council is holding a Public Hearing on Bylaw No. 1157 on Tuesday, 20 May 2008, at 7:00 pm, in the Council Chambers of the City Hall located on the Third Floor, 877 Goldstream Avenue. The owner is proposing to amend the zoning designation of 2710 Peatt Road from R1 (One-Family Residential) to MU1A (Mixed Use Residential Commercial A) to permit the development of approximately 6 townhouses, two of which may be for live-work. To implement this change to the City of Langford Zoning Bylaw, a Public Hearing is required.

The Public Hearing will provide an opportunity for residents and property owners, whose interests may be affected, to make a presentation to Council regarding the proposal. If you are unable to attend the Hearing, Council would welcome your comments in writing, as long as they are received before or at the Public Hearing on Tuesday, 20 May 2008, at 7:00 pm. Please be advised that no comments may be received by Council after the close of the Public Hearing.

Relevant background information is available in the Planning Department for perusal Monday through Friday, except Statutory Holidays, between the hours of 8:30 am and 4:30 pm. Please do not hesitate to call the Planning Department, at 474-6919, with any questions you may have on the proposal.

Sincerely,

A handwritten signature in black ink, appearing to read "Rob Buchan".

Rob Buchan
Clerk-Administrator

:tlc

Please see reverse

4. REPORTS

- a) **Rezoning Application – 2710 Peatt Road**
(Application to Amend the Zoning Designation from R1 [One-Family Residential] to MU1A [Mixed Use Residential Commercial A] to Allow Four Attached Townhouses and Two Live-Work Townhouses)
– Staff Report (File No. Z-07-43)

MOVED BY: M. HALL
SECONDED: N. STEWART

That the Planning and Zoning Committee recommend to Council:
That Council:

1. Direct staff to prepare a bylaw to amend the zoning of the subject property from R1 (One-and Two-Family Residential 1) to MU1A (Mixed Use Residential Commercial), subject to the following terms and conditions:
 - a) That prior to first reading of any bylaw to rezone the subject property the applicant provide the following, as a bonus for increased density:
 - i. \$1,000 per new lot to the General Amenity Fund;
 - ii. \$500 per new lot to the City Park Fund;
 - iii. \$100 per new lot to the Community Archeological Assessment Fund;
 - iv. \$300 per new lot to the Downtown Beautification Fund;
 - v. \$100 per new lot to the Traffic Guards School Fund;
 - vi. \$200 per new lot to the Fire Truck Fund; and
 - vii. \$500 per new lot to the Housing Fund.
 - b) That prior to public hearing:
 - i. The applicant provide assurance to the satisfaction of the City Engineer that storm water can be managed as required by the current bylaws;
 - ii. The applicant provide a new site plan showing the driveway width conforming to the City of Langford's Building Bylaw No. 650; and
 - iii. That the applicant reconfigure the development so that there are 1.4 parking spaces per unit, independent of any parking spaces that are inline or tandem.
 - c) That prior to adoption of any bylaw to rezone the subject property, the applicant provides a Section 219 Covenant, registered on title that agrees to the following:
 - i. That addresses will be posted at the entrance to the development as well as on each home;
 - ii. That the applicant will provide a reciprocal access agreement to provide access to the adjacent lot to the south;
 - iii. That the applicant will post signage in front of the two unsecured on-site parking spaces to designate the spaces for the commercial live-work units;
 - iv. That prior to issuance of a Building Permit, the applicant provide a storm water maintenance plan for the site to the satisfaction of the City Engineer; and
 - v. That any parking for a commercial use become "visitor" parking after normal business hours

- d) That prior to final adoption of any bylaw to rezone the subject property, the applicant provide a road dedication sufficient to account for the sidewalk use and necessary maintenance.
2. That as a condition of building permit, works and services be provided to Subdivision and Servicing Bylaw No. 500 standards, to the satisfaction of the City Engineer.
3. That Council direct staff to amend the MU1A zone to include townhouses as a permitted use on the lot legally described as: Lot 6, Section 5, Esquimalt District, Plan 8120 PID 005-462-312.

CARRIED.

5. ADJOURNMENT

The Chair adjourned the meeting at 7:45pm

CHAIR

CERTIFIED CORRECT
(Clerk-Administrator)

9. COMMITTEE RESOLUTIONS

- a) Planning & Zoning Committee – January 28th, 2008
1. Rezoning Application – 2710 Peatt Road (Application to Amend the Zoning Designation from R1 [One-Family Residential] to MU1A [Mixed Use Residential Commercial A] to Allow Four Attached Townhouses and Two Live-Work Townhouses)(File No. Z-07-43)

MOVED BY: COUNCILLOR SZPAK

SECONDED: COUNCILLOR SIFERT

That Council:

1. Direct staff to prepare a bylaw to amend the zoning of the subject property from R1 (One- and Two-Family Residential 1) to MU1A (Mixed Use Residential Commercial), subject to the following terms and conditions:
 - a) That prior to first reading of any bylaw to rezone the subject property the applicant provide the following, as a bonus for increased density:
 - i. \$1,000 per new lot to the General Amenity Fund;
 - ii. \$500 per new lot to the City Park Fund;
 - iii. \$100 per new lot to the Community Archeological Assessment Fund;
 - iv. \$300 per new lot to the Downtown Beautification Fund;
 - v. \$100 per new lot to the Traffic Guards School Fund;
 - vi. \$200 per new lot to the Fire Truck Fund; and
 - vii. \$500 per new lot to the Housing Fund.
 - b) That prior to public hearing:
 - i. The applicant provide assurance to the satisfaction of the City Engineer that storm water can be managed as required by the current bylaws;
 - ii. The applicant provide a new site plan showing the driveway width conforming to the City of Langford's Building Bylaw No. 650, and
 - iii. That the applicant reconfigure the development so that there are 1.4 parking spaces per unit, independent of any parking spaces that are inline or tandem.
 - c) That prior to adoption of any bylaw to rezone the subject property, the applicant provides a Section 219 Covenant, registered on title that agrees to the following:
 - i. That addresses will be posted at the entrance to the development as well as on each home;
 - ii. That the applicant will provide a reciprocal access agreement to provide access to the adjacent lot to the south;
 - iii. That the applicant will post signage in front of the two unsecured on-site parking spaces to designate the spaces for the commercial live-work units;
 - iv. That prior to issuance of a Building Permit, the applicant provide a storm water maintenance plan for the site to the satisfaction of the City Engineer; and
 - v. That any parking for a commercial use become "visitor" parking after normal business hours.
 - d) That prior to final adoption of any bylaw to rezone the subject property, the applicant provide a road dedication sufficient to account for the sidewalk use and necessary maintenance.
2. That as a condition of building permit, works and services be provided to Subdivision and Servicing Bylaw No 500 standards, to the satisfaction of the City Engineer
3. That Council direct staff to amend the MU1A zone to include townhouses as a permitted use on the lot legally described as: Lot 6 Section 5 Esquimalt District. Plan 8120 PID 005-462-312.

CARRIED.



Staff Report to Planning and Zoning Standing Committee

Date: January 28th, 2008
File: Z-07-43
Subject: Application to Rezone the Property at 2710 Peatt Road from R1 (One-Family Residential) to MU1A (Mixed Use Residential Commercial A) in order to create four attached townhouses and two live-work townhouses

PURPOSE

Marie Brotherston is applying on behalf of Ken Brotherston to amend the zoning designation of the property at 2710 Peatt Road from R1 (One-Family Residential) to MU1A (Mixed Use Residential Commercial A) in order to create four attached townhouses and two live-work townhouses.

BACKGROUND

In 1997, Council rezoned this property along with all of the residential properties along Peatt Road to One Family Residential (then R2, now R1) to prevent further duplexing of existing single-family dwellings. The intent of Council is reflected in its resolution dated December 15, 1997:

*"That Council endorse the statement of intent that after substantial redevelopment (75%) of the existing pedestrian commercial area, it is Council's goal that Peatt Road North of Goldstream Avenue shall be redeveloped as a mixed use Pedestrian Commercial with residential on upper floors; and
That Council indicate that the redevelopment would include high quality landscaping, ornamental lighting, wide red brick paver sidewalks, and rear land access to parking at the rear of the building."*

In 1997, Council amended the OCP designation of the subject property from Multi-Family Low Density to Live-Work Studio. There have been no other previous applications made to the City of Langford Planning Department with respect to the subject property.

APPLICATION DATA

Applicants	Marie Brotherston
Owners	Ken and Marie Brotherston
Location	2710 Peatt Road
Legal	Lot 6, Section 5, Esquimalt District, Plan 8120

Size of Property	770 m ² (8,299 ft ²)	
DP Areas	Multi-Family & Live-Work Studio	
	Current:	Proposed:
Zoning	R1 (One-Family Residential)	MU1A (Mixed Use Residential Commercial A)
OCP Designation	Live-Work Studio	Live-Work Studio

	Permitted by MU1A Zoning	Proposed Development
Height	Maximum = 15m (49 ft)	11m (37ft)
Site Coverage	Maximum = 90%	45.96%
Density of Development	FSR Maximum = 2.0	1.0
Front Yard Setback	Minimum = 2m (6.5 ft)	1.5m (5ft) [0m if Council accepts road Dedication]
Interior Side Yard Setback	Minimum = 0m (0ft)	1.5m (5ft) on South Side & 0m (0ft) on North Side
Exterior Side Yard Setback	Minimum = 2m (6.5ft)	N/A
Rear Yard Setback	Minimum = 7.5m (25 ft)	1.5m (5 ft)
Parking Requirement	Required: 1.4 spaces per unit = 9 spaces	12 on-site spaces

SITE AND SURROUNDING AREAS

The subject property is located on the west side of Peatt Road two properties south of Brock Avenue.

The surrounding land uses are as follows:

	Zoning	Use
North	MU1 (Mixed Use Residential Commercial)	4 storey Condominium
East	MU1 (Mixed Use Residential Commercial) & R1 (One- Family Residential)	4 storey Condominium (currently under construction) & Single Family Residence
South	R1 (One-Family Residential)	Single Family Residence
West	R2 (One- and Two-Family Residential)	Single Family Residence

COUNCIL POLICY

Official Community Plan – Bylaw № 150

A3 *Priority will be given to development with the provision of sanitary sewer service within the Town Centre area.*

A4 *Council shall endeavor to reduce outbound commuting by encouraging an increased supply of housing and work opportunities for people in and nearby the Town Centre, allowing people to both live and work locally.*

A7

d) *Within the Live-Work Studio Area designation a mixed residential, commercial and light industrial uses, which combine home occupations on the ground floor and residences above, will be encouraged to provide a variety of housing and work opportunities and encourage integration of tourism, craftsmanship and selected industry:*

- *Design that displays high ceilings and large windows with an industrial character will be encouraged;*
- *Used with no nuisance impacts and that are compatible with residential living will be encouraged;*
- *On site storage must be enclosed;*
- *Small loading bays may be permitted;*
- *Live-work studios will be encouraged along Peatt Road north and the north side of Orono.*

Affordable Housing, Park and Amenity Contribution Policy № 6930-00-006:

On September 4th, 2007 Council amended the Affordable Housing, Park and Amenity Contribution Policy. The subject property is in the Downtown Revitalization Area and is subject to the following contributions:

Table 2

Contribution	Downtown Revitalization Area
General Amenity Fund	\$1,000 per unit
City Park Fund	\$500 per unit
Archeological Fund	\$100 per unit
Downtown Beautification Fund	\$300 per unit
Traffic Guards School Fund	\$100 per unit
Fire Truck Fund	\$200 per unit
Housing Fund	\$500 per unit
Total Contributions	\$2,700 per unit (\$16,200)

A summary of the applicant's Development Cost Charges are as follows:

Table 3

Development Cost Charges		
• Roads	\$2,184 per lot	\$13,104
• Storm Drainage	\$1.34 per m2 of site area	\$1,031.80
• Park Improvement	\$1,100 per parcel created	\$6,600
• Park Acquisition	\$1,890 per parcel created	\$11,340
• CRD Water	\$2,653.14/unit	\$15,918.84

• School Site Acquisition	\$698/unit	\$4,188
Total DCC's		\$52,182.64

COMMENTS

OFFICIAL COMMUNITY PLAN

As noted above, the subject property is designated as Live Work Studio in the Official Community Plan Bylaw No. 150. The proposed MU1A (Mixed Use Residential Commercial A) zone is consistent with this designation and would permit the subject property to be developed according to the OCP.

ZONING

Apartments and live-work studios are permitted uses in the MU1A zone. The applicant has proposed to construct four attached townhouses at the rear of the site. In order to allow development of this site as proposed, the MU1A zone would need to be revised to include townhouses as a permitted use. Townhouses may relate better to the residential character of the single family dwellings located behind and beside the proposed development. If Council chooses to allow townhouses as a permitted use in the MU1A zone on this site solely, it would not preclude a higher density use of the subject property, such as an apartment building.

VARIANCES

As mentioned above, the site layout currently being considered would require two variances for setbacks. The applicant has requested a variance to the required rear lot line setback from the required 7.5m (25ft) to 1.5m (5ft). If Council chooses to accept the proposed road dedication, the front setback would be reduced to 0m. Variances to all setbacks may be granted through the Development Permit process by the City Planner.

IMPLICATIONS ON NEIGHBOURING LAND USES

The properties to the north and east are occupied as four-storey condominiums, and the properties to the south and west are occupied by single-family dwellings, which are much smaller in height and size than the proposed townhouses. The design of the proposed mixed-use building is consistent with the form and character encouraged within the Town Centre Area both in general and within the Live-Work Studio and Pedestrian Commercial designations. In order to provide privacy between the proposed development and existing single family dwellings, the applicant has proposed to construct a 1.8m (6ft) solid board fence along the side and rear property boundaries in addition to landscaping around the perimeter of the site.

FORM AND CHARACTER

This development proposal, if supported by Council, will be subject to a form and character Development Permit for Multi-Family and Live-Work Studio Design Guidelines. It should also be noted that the plans submitted will require the following modifications to meet the standards of the Multi Family and Live-Work Design Guidelines.

- Increase window area to the maximum allowable (7%), on the south elevation of the live-work unit; and
- On the live-work unit, take the northern façade and replicate the mixed material design (cedar shake on all levels) on the southern elevation;
- To provide additional screening, add trees into rear setback; and

- To minimize the visual impact to the low density residential behind and beside the site, the roof height should be lessened by approximately 50% (irrespective of the pitch).

TRAFFIC AND ACCESS

Access to the proposed development is from Peatt Road. The current plans indicate a driveway width of 3.7m (12ft), which is below the standards of Building Bylaw No. 650. The applicant has proposed to remedy this by lessening the 1.5m (5ft) landscape strip, and will submit a new plan to the standards of Building Bylaw No. 650 prior to public hearing. Additionally, the applicant has agreed to provide a Section 219 Covenant for reciprocal access to the adjacent lot to the south.

The Fire Chief has requested that the addresses of the homes to be built be posted at both the entrance to the development as well as on each home.

COMMUNITY TRANSPORTATION

The BC Transit system is within a five minute walk from the site, in a southbound direction, (Goldstream Avenue). From Monday to Friday, bus frequency ranges from 5 minutes during peak hours to every 15 minutes mid day, and to every 30 minutes into the evening (on weekends and holidays, the waiting time is slightly longer).

PARKING

The parking requirement for the proposed MU1A zone is 1.4 spaces per unit. The applicant has provided 2 spaces per unit. The four townhomes at the rear of the site will each have double (length) stacked car spaces in a single car-width garage. The two live work units at the front of the site will have one space each in a single car garage and will have one space each at the side of the driveway to be reserved for the live-work spaces (the applicant will ensure, by covenant, that signs will be posted). The plans reveal an attempt to create a 13th parking space off site (in front of the proposed development on Peatt Road) should this prove feasible.

ROAD DEDICATION

The applicant has proposed to curve the sidewalk onto private property in order to achieve a better building envelope, parking maximization and landscape design. The Engineering Department has stated that this would be acceptable if the applicant provides the land as road dedication of a sufficient amount to accommodate the sidewalk area. It is requested that this be completed prior to final adoption.

FINANCIAL IMPLICATIONS

Rezoning the property to permit a mixed use development will contribute more to municipal revenue than by redeveloping it under its present R1 zoning. As the applicant will connect the property to municipal sewers, and as the applicant will complete frontage improvements, the direct capital costs to the municipality, associated with this development, will be negligible. However, maintenance costs will offset most or all of the additional revenues.

OPTIONS

That the Planning and Zoning Committee recommend that Council:

1. Direct staff to prepare a bylaw to amend the zoning of the subject property from R1 (One- and Two-Family Residential 1) to MU1A (Mixed Use Residential Commercial), subject to the following terms and conditions:
 - a) **That prior to first reading of any bylaw to rezone the subject property the applicant provide the following, as a bonus for increased density:**
 - i. \$1,000 per new lot to the General Amenity Fund;
 - ii. \$500 per new lot to the City Park Fund;
 - iii. \$100 per new lot to the Community Archeological Assessment Fund;
 - iv. \$300 per new lot to the Downtown Beautification Fund;
 - v. \$100 per new lot to the Traffic Guards School Fund;
 - vi. \$200 per new lot to the Fire Truck Fund; and
 - vii. \$500 per new lot to the Housing Fund.
 - b) **That prior to public hearing:**
 - i. The applicant provide assurance to the satisfaction of the City Engineer that storm water can be managed as required by the current bylaws;
 - ii. The applicant provide a new site plan showing the driveway width conforming to the City of Langford's Building Bylaw No. 650.
 - c) **That prior to adoption of any bylaw to rezone the subject property, the applicant provides a Section 219 Covenant, registered on title that agrees to the following:**
 - i. That addresses will be posted at the entrance to the development as well as on each home;
 - ii. That the applicant will provide a reciprocal access agreement to provide access to the adjacent lot to the south;
 - iii. That the applicant will post signage in front of the two unsecured on-site parking spaces to designate the spaces for the commercial live-work units; and
 - iv. That prior to issuance of a Building Permit, the applicant provide a storm water maintenance plan for the site to the satisfaction of the City Engineer.

- d) That prior to final adoption of any bylaw to rezone the subject property, the applicant provide a road dedication sufficient to account for the sidewalk use and necessary maintenance.

AND

2. That as a condition of building permit, works and services be provided to Subdivision and Servicing Bylaw No. 500 standards, to the satisfaction of the City Engineer.

AND EITHER


3. That Council direct staff to amend the MU1A zone to include townhouses as a permitted use on the lot legally described as: Lot 6, Section 5, Esquimalt District, Plan 8120 PID 005-462-312.

OR

4. That Council direct staff to amend the MU1A zone to allow for townhouses as a generally permitted use.

OR

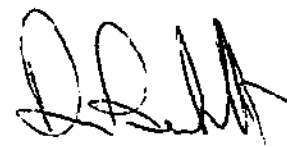
5. Reject this application for rezoning.



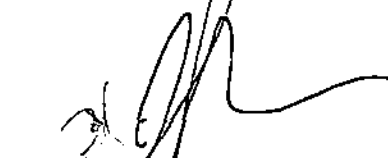
Matthew Baldwin, MCIP
City Planner




Grant Liebscher
Planner




Bob Beckett
Fire Chief



John Manson, P.Eng.
City Engineer



Steve Ternent
Treasurer



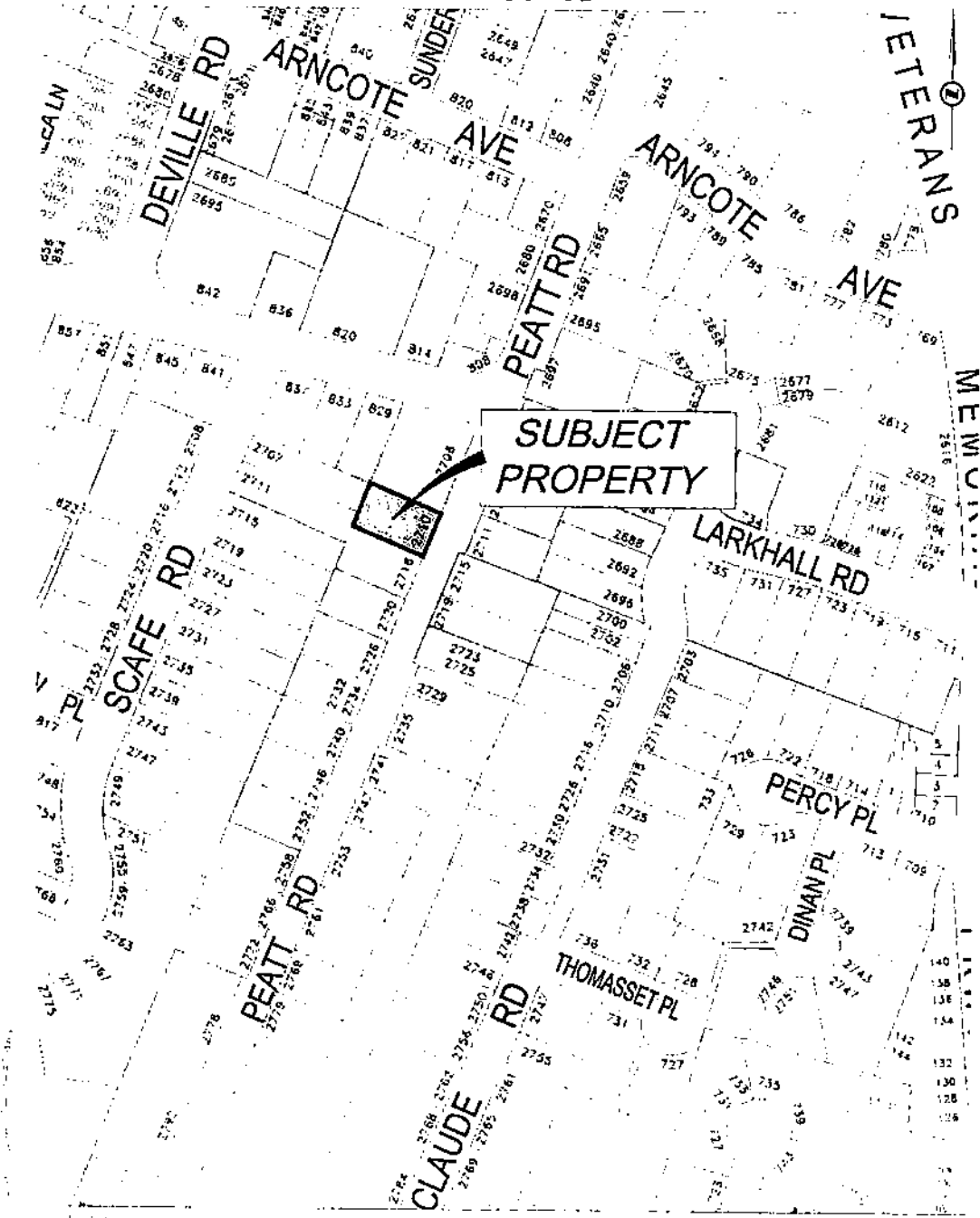
Rob Buchan, MCIP
Clerk-Administrator

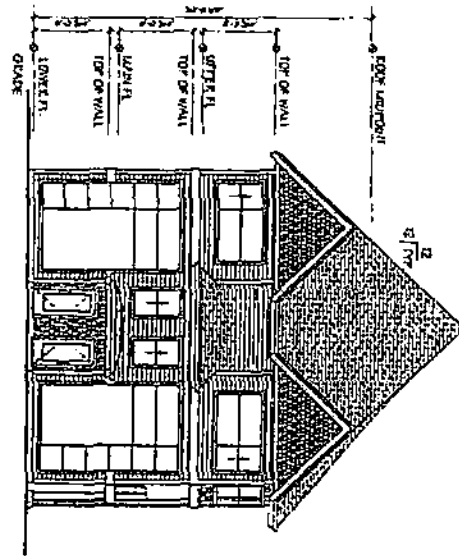
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REZONING BYLAW AMENDMENT 2710 PEATT RD Z-07-43

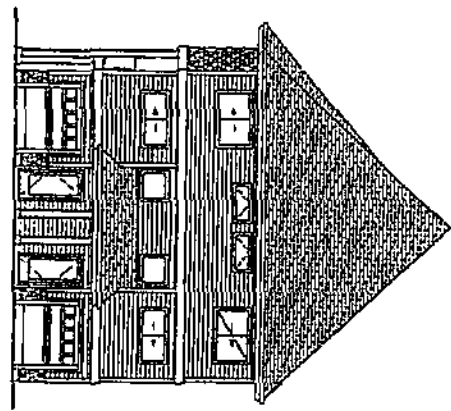


REZONING BYLAW AMENDMENT 2710 PEATT RD Z-07-43

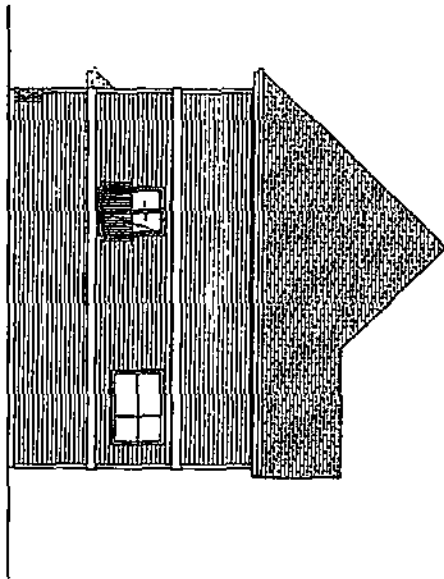




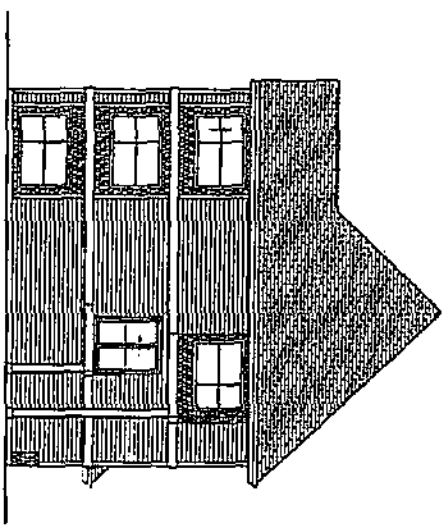
1 FRONT ELEVATION
 Scale: 1/8" = 1'-0"



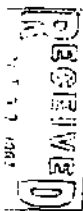
2 REAR ELEVATION
 Scale: 1/8" = 1'-0"



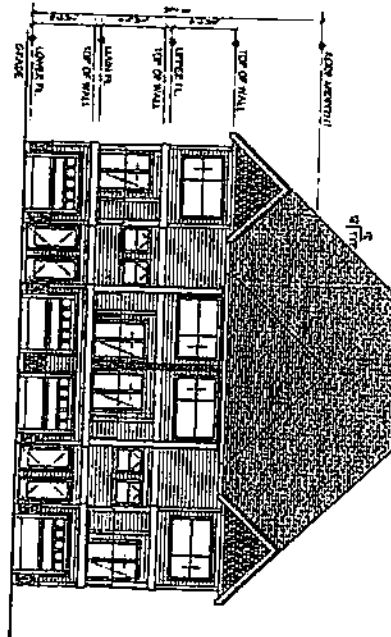
3 SIDE ELEVATION
 Scale: 1/8" = 1'-0"
 (DIMING DISTANCE = 15'51" (4.11' 0.55")



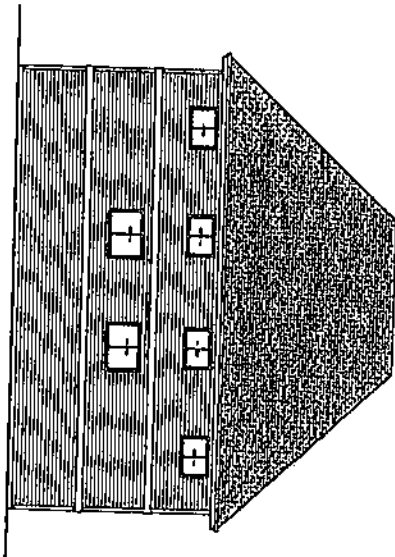
4 SIDE ELEVATION
 Scale: 1/8" = 1'-0"



<p>POLLACK & BROTHIERSTON LULINE REGISTERED ARCHITECTS 10000 W. 10th Street, Suite 100 Fort Worth, Texas 76132 Phone: 817.336.1111 Fax: 817.336.1112</p>		<p>CHRIS SUPREME ARCHITECT 10000 W. 10th Street, Suite 100 Fort Worth, Texas 76132 Phone: 817.336.1111 Fax: 817.336.1112</p>
<p>Project No. 1 Drawing No. 1 Date: 1/28/08</p>	<p>Location: 2710 PEATT RD FORT WORTH, TX 76104</p>	<p>Client: PEATTIER MARSHALL</p>
<p>Scale: 1/8" = 1'-0"</p>	<p>Date: 1/28/08</p>	<p>Author: JAMES W. POLLACK</p>

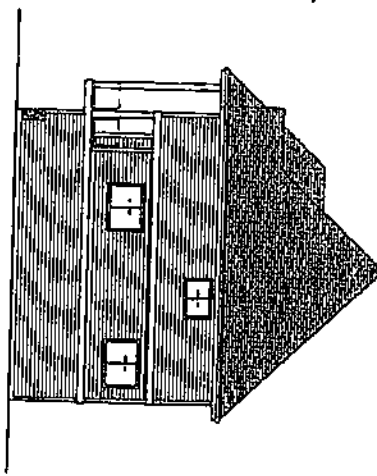


1 FRONT ELEVATION
 Scale: 1/8" = 1'-0"



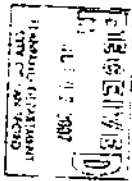
3 REAR ELEVATION
 Scale: 1/8" = 1'-0"

LANDING DISTANCE = 15M (47.10567)
 EXPOSED BUILDING FACE = 882.9 SQ. FT.
 TOTAL UNPROTECTED OPENINGS ALLOWED = 111 479 SQ. FT.
 TOTAL UNPROTECTED OPENINGS = 109 SQ. FT. (8.7823)



2 SIDE ELEVATION (OPPOSITE ELEV. SIM.)
 Scale: 1/8" = 1'-0"

LANDING DISTANCE = 15M (47.10567)
 EXPOSED BUILDING FACE = 821.927 SQ. FT.
 TOTAL UNPROTECTED OPENINGS ALLOWED = 71,595 SQ. FT.
 TOTAL UNPROTECTED OPENINGS = 69 SQ. FT. (8.18593)



PALLOCK & BROTHERS ARCHITECTS 1000 W. 10th St. Anchorage, Alaska 99501 Phone: (907) 562-1111 Fax: (907) 562-1112 Email: info@pallockandbrothers.com	
Project: 2710 Peatt Road	Client: [Redacted]
Architect: C. J. B. B. B.	Date: 1/28/08
Scale: 1/8" = 1'-0"	Sheet: A5



Staff Report
to
Council

Date: April 28th, 2008

File: Z-07-43

Subject: *Rescind 1st reading for Bylaw N^o 1157; Application to Rezone the Property at 2710 Peatt Road from R1 (One-Family Residential) to MU1A (Mixed Use Residential Commercial A) to Allow for Approximately 6 Multi-Family Residential Units Two of Which May be Live-Work.*

BACKGROUND

At their regular meeting of March 17th, 2007, Council gave 1st Reading to Bylaw (N^o 1157) to change the zone of the property at 2710 Peatt Road from R1 (One-Family Residential) to MU1A (Mixed Use Residential Commercial A) to allow for a 6 unit Multi-Family Residential Development, two of which may be live-work. Subsequently, staff noticed the Bylaw required additional modifications thereby necessitating a rescinding of the 1st Reading.

The incorrect Bylaw was on Council's agenda on April 21st, 2008 and the previous 1st Reading was rescinded and Council gave 1st Reading to the incorrect Bylaw.

COMMENTS

The correct Bylaw is attached and Council may wish to rescind 1st reading from April 21st, 2008 and consider giving first reading to Bylaw N^o 1157 as presented.


OPTIONS

That Council:

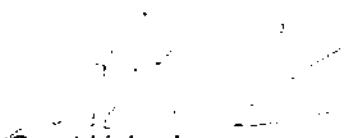
1. Rescind 1st reading of Bylaw N^o 1157 for the property at 2710 Peatt Road, and give 1st reading to Bylaw N^o 1157 as presented;

OR

2. Take no action at this time with respect to Bylaw N^o 1157.

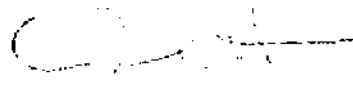


Mathew Baldwin, MCIP
City Planner




Grant Liebscher
Planner

Bob Beckett
Fire Chief



John Manson, P.Eng
City Engineer



Steve Ternent
Treasurer



Rob Buchan, MCIP
Clerk-Administrator

:gl

CITY OF LANGFORD
BYLAW NO. 1157

A BYLAW TO AMEND BYLAW NO. 300, LANGFORD ZONING BYLAW, 1999"

The Council of the City of Langford, in open meeting assembled, hereby enacts as follows:

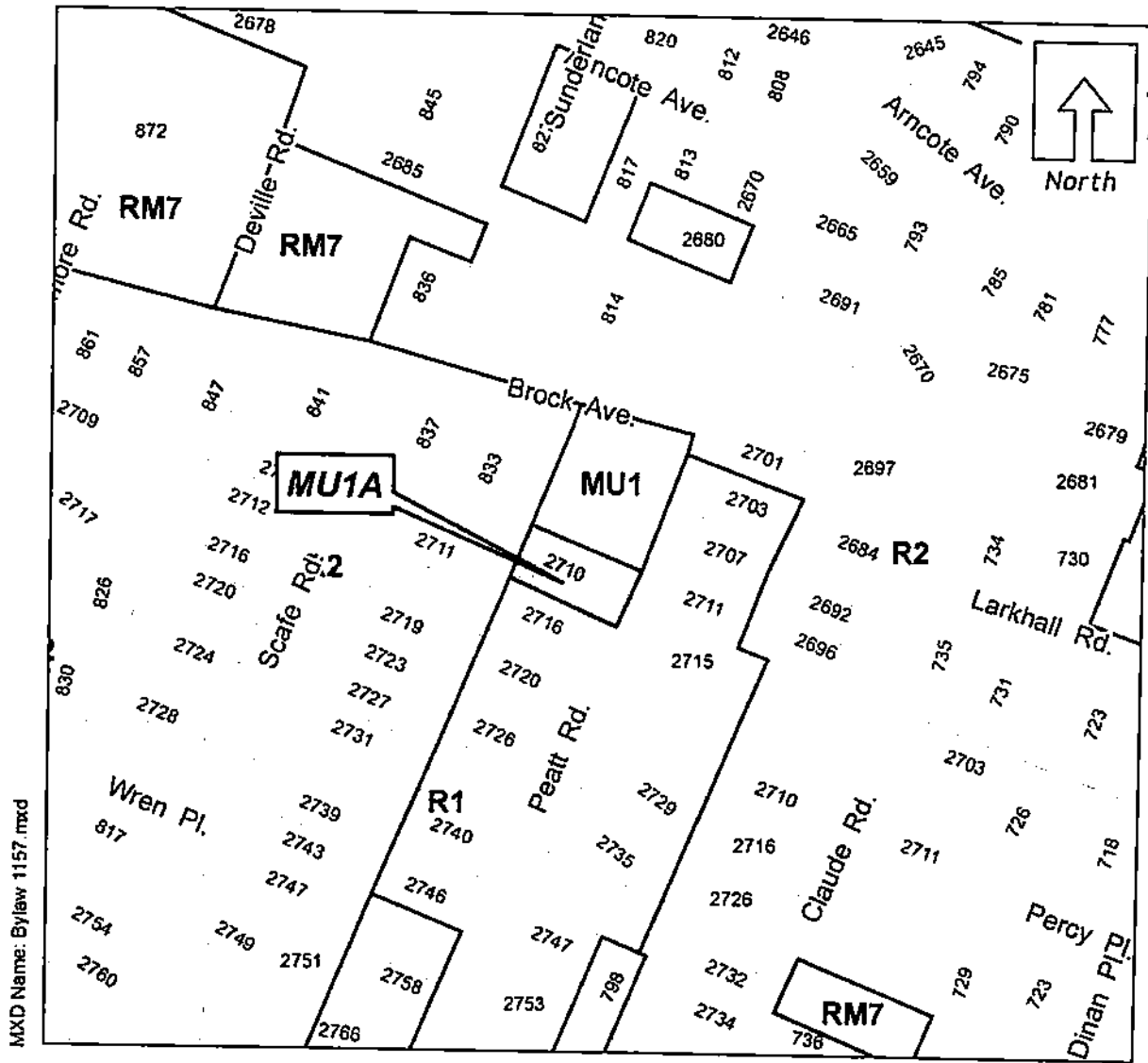
A. Langford Zoning Bylaw No. 300, 1999 is amended as follows:

1. By deleting from the R1 (ONE-FAMILY RESIDENTIAL) ZONE and adding to the MU1A (MIXED USE RESIDENTIAL COMMERCIAL A) ZONE the property legally described as Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.
2. By adding to the list of permitted uses in Section 6.51.01A in appropriate alphabetical order the words "townhouses, on land legally described as Lot 6 Section 5 Esquimalt District Plan 8120" and renumbering the following subsection accordingly.
3. By replacing the words "the floor area ratio may not exceed 2.0" in Section 6.51.04A(1) with the words "not more than one dwelling unit per 550 m² of lot area may be constructed on any lot and the floor area ratio for all uses may not exceed 0.3" and by replacing the words "the minimum floor area ratio may be increased to 2.0" in Section 6.51.04A(3) with the words "the density of development shall be regulated by floor area ratio only and the maximum floor area ratio is 2.0".
4. By adding the following paragraphs to Section 6.51.04A(3) in alphabetical order:
 - (j) pays to the City the amount specified in Column 11 of Table 1 below at time of building permit, to be deposited in the School Crossing Guards Reserve Fund;
 - (k) pays to the City the amount specified in Column 12 of Table 1 below at time of building permit, to be deposited in the Affordable Housing Reserve Fund;
 - (l) pays to the City the amount specified in Column 13 of Table 1 below at the time of building permit, to be deposited in the Equipment Replacement Reserve Fund.

A. By adding to Table 1 in the section 6.51.04(A)

Table 1

1. Legal description	2.	3.	4.	5.	6.	7.	8.	9.	10.	11.	12.	13.
Lot 6, Section 5, Esquimalt District, Plan 8120	\$500 per dwelling unit	\$100 per dwelling unit	\$0	\$1,000 per dwelling unit	\$0	\$300 per dwelling unit	\$0	\$0	\$0	\$100 per dwelling unit	\$500 per dwelling unit	\$200 per dwelling unit



LEGAL DESCRIPTION: Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.

I HERBY CERTIFY THIS TO BE A TRUE COPY
 OF PLAN No. 1 AS DESCRIBED
 IN SECTION ____ OF BYLAW No. 1157

 MAYOR

ADOPTION: _____

 CLERK - ADMINISTRATOR

Scale: N.T.S.

Last Revised: February 08, 2008

March, 18, 2008

Dear Langford Planning and City Council,

As residents of 2711 Scafe Road we have concerns pertaining to the proposed development at 2710? Peatt road. (Bordering our property at the rear). Our concerns relate to the positioning of the proposed townhouses at the development and the potential effect any foundations may have upon the trees in our yard and the neighbours. Since these trees are very large they have extensive root systems, and our concern is that if these roots are damaged/removed they will have a potentially catastrophic effect on the stand of trees. Not only does the potential exist of killing or weakening the closest tree(s) to the development but the cohesiveness of the entire stand as a whole is ruined.

Therefore, unless the developer is willing to accept any current or future liability for the resultant potential tree falls (in writing, and registered with Langford), it is my expectation that the developer removes the existing trees from the rear of my property (at his expense) to ensure no future liability issues.

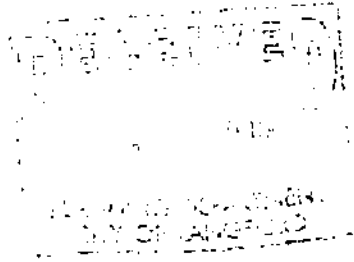
Please note, the developer has approached and introduced himself to my wife pertaining to this issue, however we do not feel he is aware of our offer of solution.

Sincerely
Geoff & Trisha Turner
2711 Scafe Road
Victoria, B.C. V9B 3W6
(250) 478 0956

*RECEIVED
\$250 Trust*

RECEIVED
MAY 13 2008
BUSINESS DEPARTMENT
CITY OF LANGFORD

207-43

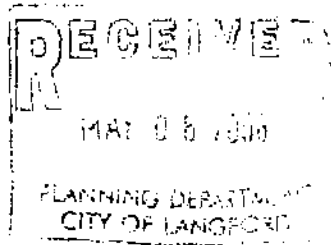


Paul Morris
2707 Scafe Rd.
Langford B.C.
V9B3W6
25 March 2008

Mayor and Council
Langford B.C.

I am very concerned about the plans to build at the site Civic Address 2710 Peatt Rd. The back of my property borders on this site. I have four very large fir trees within a few feet of the property line. Due to the very soft sandy soil they will be required to dig down several feet to create a satisfactory building pad to support these units. They can not do this without cutting through the roots of my trees with only a five foot setback from the property line. This will cause extensive damage to my trees and could easily lead to their failure in the future. I believe that a five foot setback is not adequate to protect my trees. I would require an arborist perform an independent evaluation of this situation. I would not be happy with the removal of these trees to solve this problem. This could to lead to damage of many other trees on my property during a severe wind storm. I wish to have this situation addressed at the next meeting before any approval is given to continue.

Paul Morris



Paul Morris
2707 Scafe Rd.
Langford B.C.
V9B3W6
25 March 2008

Mayor and Council
Langford B.C.

I am very concerned about the plans to build at the site Civic Address 2710 Peatt Rd. The back of my property borders on this site. I have four very large fir trees within a few feet of the property line. Due to the very soft sandy soil they will be required to dig down several feet to create a satisfactory building pad to support these units. They can not do this without cutting through the roots of my trees with only a five foot setback from the property line. This will cause extensive damage to my trees and could easily lead to their failure in the future. I believe that a five foot setback is not adequate to protect my trees. I would require an arborist perform an independent evaluation of this situation. I would not be happy with the removal of these trees to solve this problem. This could lead to damage of many other trees on my property during a severe wind storm. I wish to have this situation addressed at the next meeting before any approval is given to continue.

5 May 2008 Regarding the above previous Memo I sent to you.

Since that time I have received a copy of the independent report from the arborist. In this report he states that three of my trees must be removed before such a development could proceed. As I have no intentions to allow my trees to be removed I do not know how permission to proceed can be granted. I will not be responsible for any damage caused by my trees in the future should they proceed.

Paul Morris

Dear Mr Buchan:

I am writing to respond to a notice I recently received regarding a Public Hearing on Bylaw No. 1157 on Tuesday, May 20 regarding the property at 2710 Peatt Road.

In the past 3 years I have twice addressed city council regarding neighbourhood development and concerns over traffic, parking and lack of green space. I live at 837 Brock Avenue. Because of a prior commitment I am unable to attend the meeting but wish to voice my concern. I have not heard what is proposed for this small city lot but the city needs to be aware of the ongoing parking problem in our neighbourhood. Cars from the local townhouse and condominium projects park along Brock Avenue, blocking the street to one-way traffic only. There are also so many parked beside our property on Scafe Road that there is no parking available for clients to our home bookkeeping business. The businesses on Peatt are already using all the spaces that have been set aside by insets in the road. Will the development address the parking problem adequately?

The amount of traffic on Peatt is another concern. It is difficult at peak times to cross the street on foot. There are no crosswalks near Peatt and Brock. The traffic is sometimes backed up from the Veterans Memorial Parkway lights to the roundabout on Brock. The city needs to address a better traffic flow before more development is encouraged in the downtown area.

I know in the new community plan trees and green spaces in the downtown core are important. I see too often, as in this case, the developer taking down all the trees and generally letting the property deteriorate before applying for a building permit. I know the developer originally wanted part of the neighbour's property to build. Can he still go ahead on such a small lot, with no street parking and no green space? Are we in the downtown area to see only concrete and cars?

Yours sincerely

Rosemary Tarnowski

PUBLIC HEARING

Tuesday, May 20, 2008

Bylaw Nos. 1200 and 1201

The purpose of Bylaw No. 1200 is to establish a new Official Community Plan for the City of Langford. This new plan sets policy and strategic objectives that reflect the most current thinking on sustainability, providing a strong rationale for how and where development and land use changes are to occur. The new Official Community Plan integrates policy on land use, economy, environment, transportation, community facilities and services. The plan also designates development permit areas within which most types of development will require a development permit issued in accordance with design guidelines. Every parcel of land within the City of Langford is affected by Bylaw No. 1200.

The purpose of Bylaw No. 1201 is to amend the Zoning Bylaw No. 300 by incorporating all of the current design guidelines from Official Community Plan Bylaw No. 150 into the Zoning Bylaw No. 300.



Staff Report
to
Council

Inserted into Public
Hearing Package
May 9, 2008.
J. Plouffe

Date: May 20, 2008
File: OCP-06-04
Subject: Langford 2008 Official Community Plan (OCP) – Bylaw 1200

Purpose

This report outlines the proposal from staff to change the OCP maps after the Public Hearing, to address a few mapping errors and to expand development permit areas in light of new information that has been provided to the municipality.

This report also serves to discuss comments on the OCP which were received by the public and referral agencies since first reading of the OCP.

Background

Council has given first reading to Bylaw 1200 on April 21, 2008. Public Hearing for this bylaw is being held in Council Chambers on May 20, 2008.

The City of Langford's 2006-2008 Official Community Plan review was executed jointly with the City of Colwood. This sub-regional planning exercise was a first in BC, and allowed numerous opportunities for creative integration, public feedback and genuine input.

An estimated 650 people from the Cities of Colwood and Langford provided input at six public events held between June 23rd, 2007 and Mar. 8th, 2008.

Each public event not only provided written feedback opportunities, but also focused on dialogue-based work groups where participants were provided the opportunity to shape the agenda, choose the topics they discussed and provide recommendations on those topics for the OCP review. An OCP Steering Committee was also struck that included representatives from Council, committees and citizens at large. Findings and public feedback summaries were provided online for public review for each event. The public was also presented with an extensive OCP Policy Options report (that contained a review of public concerns, analysis results and policy recommendations) and numerous draft OCPs online and at public events.

Additionally, the Langford public was invited to comment on the draft OCP at the Planning and Zoning Committee meeting held on March 31, 2008.

Comments

Mapping Errors:

Council may wish to amend the following mapping errors, which do not constitute a change in land use, and are permissible amendments following the close of Public Hearing:

1. Adding missing ALR parcels to 'Agricultural Strategy Lands' designation on Maps 2, 5 and 6:

Despite that Map 10: Agricultural Land Reserve (ALR) Inventory is correct, Map 2: Growth Management & Land Use Strategy, Map 5: Natural Setting, Parks & Open Spaces Inventory, and Map 6: Natural Setting, Parks & Open Spaces Integration Strategy all contain the same mapping error, such that not all ALR parcels are shown highlighted on the map. Council may wish to amend this mapping error for the sake of clarity, while noting that amending this mapping error does not affect land use of ALR parcels, as the management of ALR lands is fully regulated by the Province of British Columbia.

2. Adding Bilston Creek to Map 3: Sensitive Ecosystems & Habitat Areas:

Bilston Creek was identified in the former City of Langford (1996) Official Community Plan, and its lack of delineation in Map 3 of the current OCP was a mapping error.

3. Amending Steep Slopes development permit areas on Map 17:

The Steep Slopes development permit areas do not accurately reflect the Steep Slopes DP area mapping that Council adopted in Bylaw 302 (date).

Amended Maps 2, 3, 5, 6 and 17 have been added to this report as appendices, for Council's reference.

Mapping Additions:

Council may also wish to amend the Map 15: Areas with Potential Habitat & Biodiversity Values (attached) such that it includes additional Potential Habitat and Biodiversity areas. Council had created this new category of Development Permit area in 2005 to address the need to professional assess rural properties for their habitat and biodiversity value, when development or alteration of the land is under consideration. An outside agency has provided the City with a recently completed mapping inventory of Garry Oak and Associated Ecosystems. As this mapping inventory has not been verified by City staff or consultants, Council may wish to add the small portion of lands that is not already inside one of our Environmental Protection Development Permit areas into the Potential Habitat and Biodiversity area, so that a Registered Professional Biologist or other appropriate professional will be assessing the property's habitat and ecosystem health prior to development.

Comments from the Public:

Staff have received the following five comments from the public, noting that concerns from the public have been addressed where possible, and might be the result of misunderstanding in other instances.

1. Additional protection for lakeside neighbourhoods and riparian values

The new OCP expands Council policy around protection of shoreline areas.

Resident concerns raised in two separate petitions are also being addressed with a proposed rezoning of the Subdivision Lot Requirements section of the R1 (One-Family Residential) and R2 (One- and Two-Family Residential) zones, with respect only to lakefront parcels. Consideration of this report follows the Public Hearing OCP, at the 20 May 2008 Council meeting.

Consideration is also being given to amending the design guidelines for shoreline development in the Environmental Protection Area Development Permit area guidelines, which will occur concurrently with a broader review of design guidelines in the coming months.

2. Adequacy of timeline to review OCP

OCP policies, which derived from public meetings at the 2-day Futures Forum in June 2007, were first presented to stakeholder agencies and non-profit organizations at October 2007 meetings and to the general public at four (4) days of public events in November 2007. Changes to OCP policies have all been in keeping with the general themes of sustainability and smart growth, and have been in response to stakeholder and public feedback and recommendations. A Policy Options Report that fully outlines OCP policy directions was publically released in January 2008, and the introduction of Bylaw 1200 follows two previous public drafts of the OCP which were available for review on the City of Langford website, at the 8 March 2008 Open House, and the 31 March 2008 Planning and Zoning Committee meeting.

OCP policies on encouragement of mixed use where appropriate is in keeping with sustainability principles espoused by provincial ministries, regional authorities, and non-profit organizations such as Smart Growth BC; as well as with all drafts of OCP documents. As a high-level strategy or goal, this policy will inform the staff, public and decision-making bodies, without hampering their ability to weigh each rezoning proposal on its own merits.

3. ALR parcels mapping error

This mapping error was brought to the attention of the City of Langford by a concerned resident. Staff have instructed OCP planning consultants to amend the mapping error, for Council's consideration following Public Hearing.

4. Notification requirements

A couple of City of Langford residents have noted a concern that the new OCP will allow new permitted uses in their neighbourhoods, without notification. Council will note that full

notification and public engagement is required by the British Columbia Local Government Act and by City of Langford procedures policy, whether or not a rezoning application is accompanied by an OCP amendment. No new uses can be permitted in any zone under the jurisdiction of the City of Langford without a rezoning process.

Council may also wish to note that the City of Langford does not have the issue of "pre-zoned" lands, in which development occurs with no public notification or review process, which has been an issue of concern in some other BC municipalities.

5. Advertising of Public Hearing

In addition to all of the requirements of the British Columbia Local Government Act and by City of Langford procedures policies, a Public Hearing notice and separate "notice board" (including copies of OCP) have been made available for review on the City of Langford website; extra and larger notices have been placed in the paper; and notice signs have been erected throughout the community.

Comments from Referral Agencies:

Some agencies have applauded the City of Langford OCP for its focus on sustainability and the measures that have been taken to protect environmentally sensitive areas. Some comments relate to Council policy which may be required, as a follow-up to the completion of the OCP review process, that addresses how agencies can work together to implement OCP policies.

Some small housekeeping items and requested additions (such as CRD's request that we add descriptions of regional parks and describe their benefits to residents) can occur when City of Langford amends the OCP in future to meet the coming Provincial requirements to add GHG emissions targets to the OCP (Bill 27).

Other referral agency responses are addressed below:

1. Town of View Royal:

While a Core Municipality, the Town of View Royal looks forward to working closely with the City of Langford on integrated transportation, economic development, and other initiatives.

2. Tsawout First Nation:

The Tsawout First Nation has a concern that the five Saanich First Nations, which have Indian Reserve #13 in common, have not been consulted in regards to the designation of their lands in the Official Community Plan. Council will note that none of the maps or policies in the new OCP change or in any way address the land use of I.R. #13. As neighbouring governing bodies, the Tsawout, Tsartlip, Pauquachin, Tseycum and Malahat First Nations have received Referrals on the draft and final OCP's, at the same time as the District of Metchosing, the Town of View Royal, the City of Colwood, the District of the Highlands, and the Capital Regional District.

3. District of Metchosin:

The mapping error with regard to the inclusion of the Bilston Creek in OCP mapping has been addressed.

Edge planning has been incorporated into the Official Community Plan as a guiding principle, but will not affect current zoning and land use, nor the ability of the City of Langford to require clustering of housing density, where appropriate, when the goal of ecological preservation can be achieved.

4. Capital Regional District (CRD):

The recommendations from the CRD to make some small changes to the labeling on maps has been incorporated where possible and other small suggestions can be easily incorporated with other small housekeeping items when the City of Langford amends the OCP in future to meet the coming Provincial requirements of Bill 27, introduced recently to the legislature by Ida Chong.

5. BC Transit

All of BC Transit's suggestions have been incorporated into the OCP in Bylaw 1200.

6. Garry Oak Ecosystem and Recovery Team (GOERT)

Council may wish to include Garry Oak and associated ecosystems mapping which has been provided to the City into its Potential Habitat and Biodiversity development permit area.

Other suggestions from GOERT's consultant are being incorporated into the draft amended design guidelines where possible. The amended design guidelines will be brought before the Planning and Zoning Committee and Council the near future, after completion of an internal staff review.

Options

That Council:

1. Give Bylaw No. 1200 amended first reading, second reading and third reading;

OR

2. Take no action with respect to the Official Community Plan at this time.




Matthew Baldwin, MCIP
City Planner



Emilie K. Adin
Deputy City Planner

Bob Beckett
Fire Chief

John Manson, P.Eng.
City Engineer



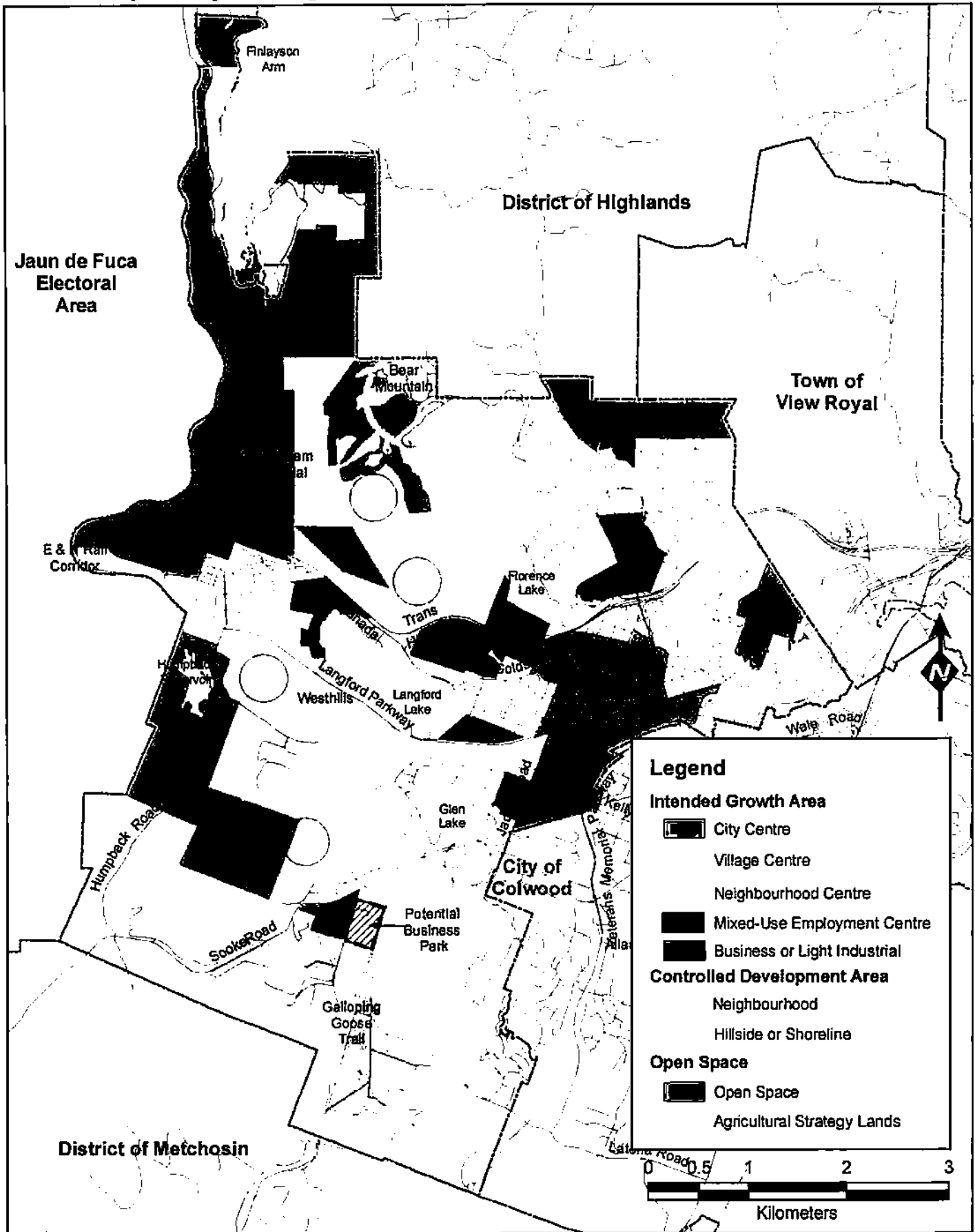
Steve Ternent
Treasurer



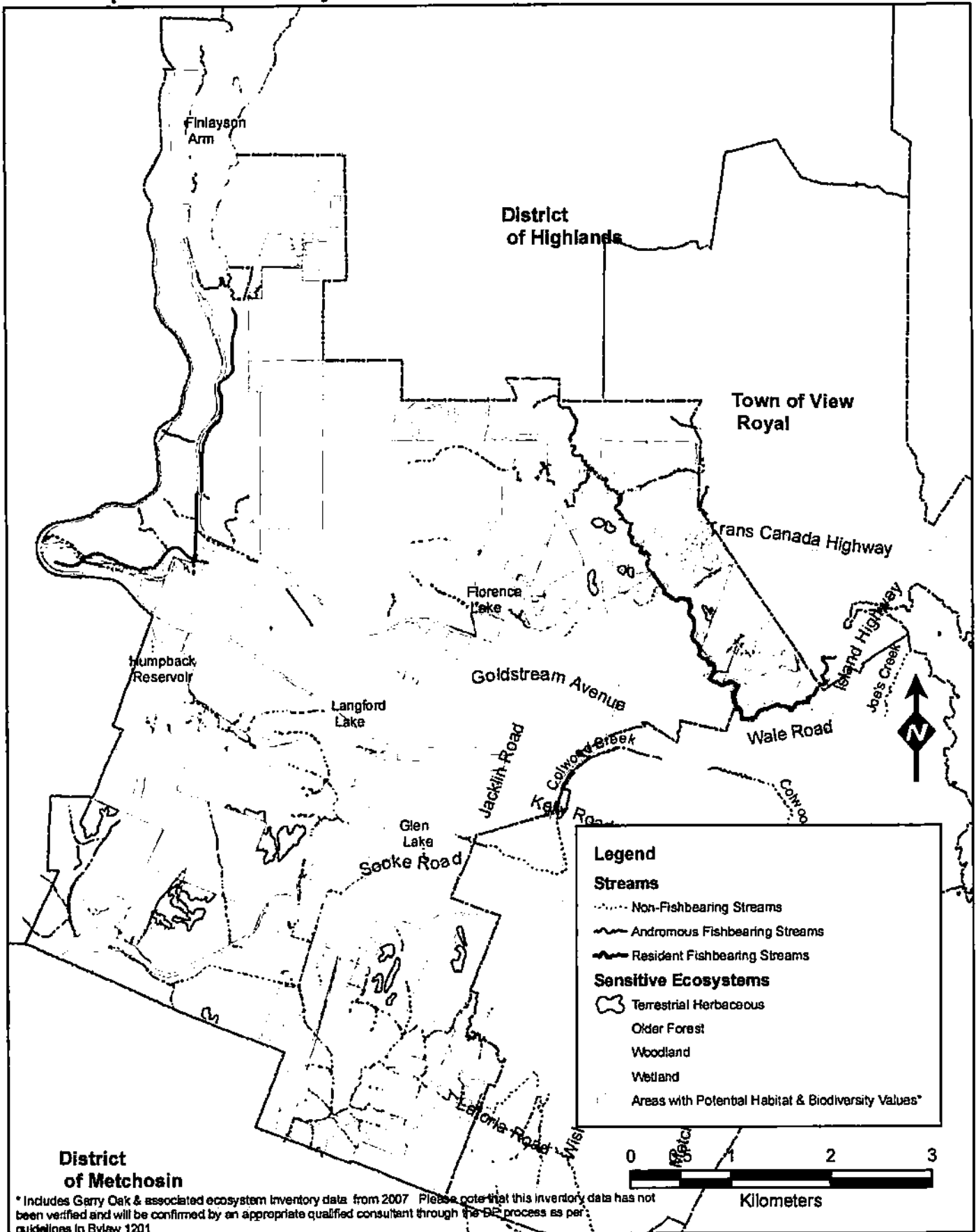
Rob Buchan
Clerk-Administrator

eka

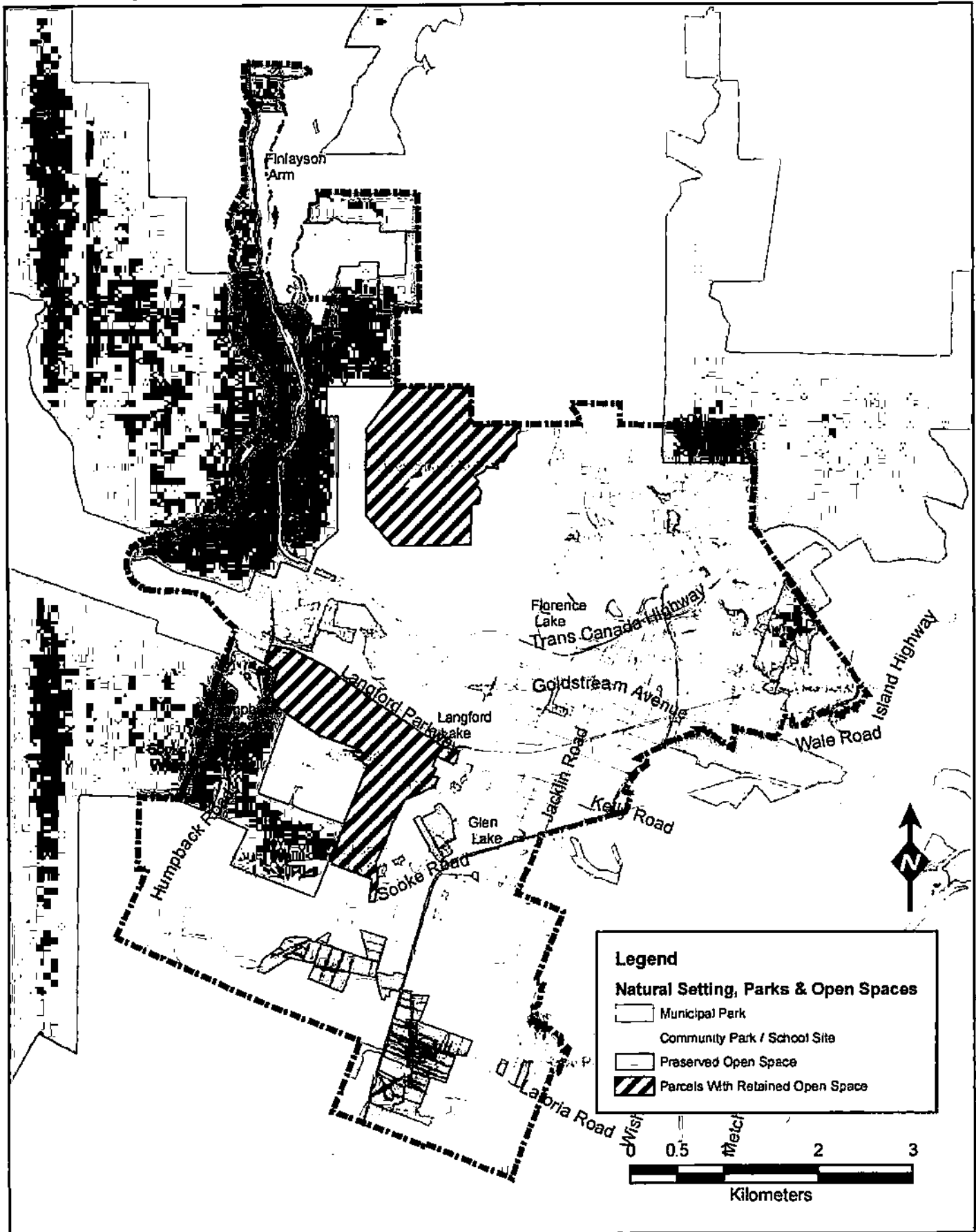
Map 2: City of Langford – Growth Management & Land Use Strategy



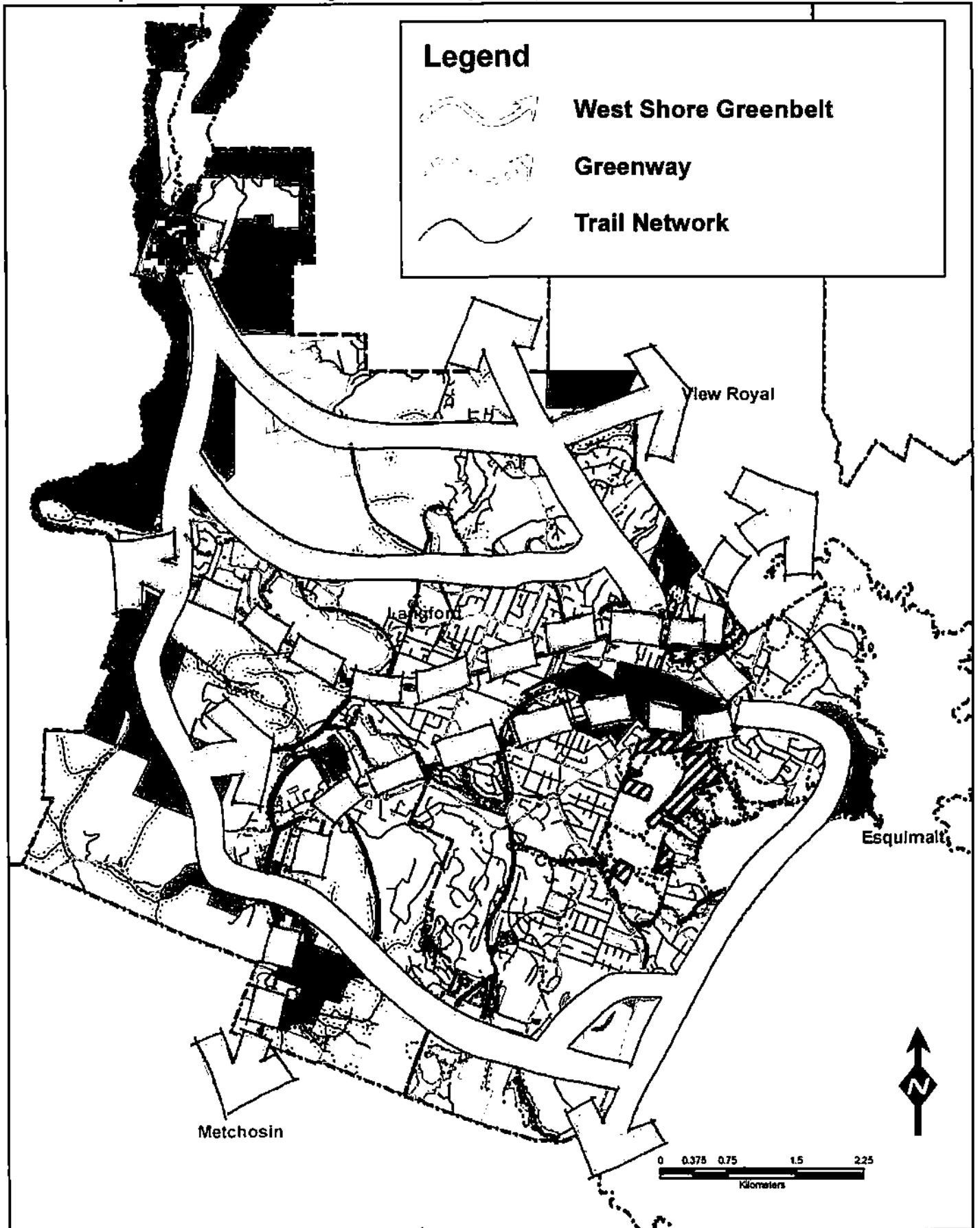
Map 3: Sensitive Ecosystems & Habitat Areas



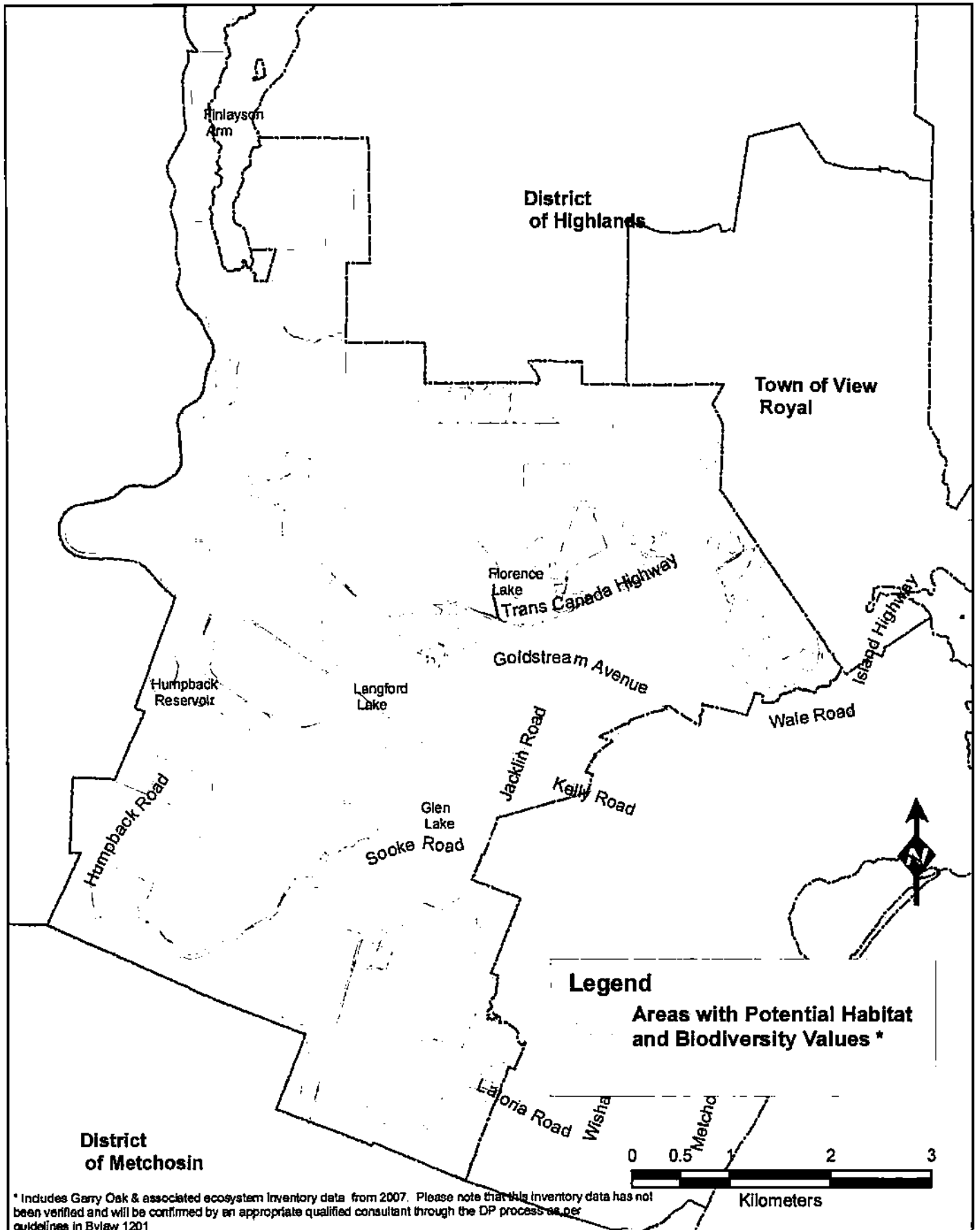
Map 5: Natural Setting, Parks & Open Spaces Inventory



Map 6: Natural Setting, Parks & Open Spaces Integration Strategy

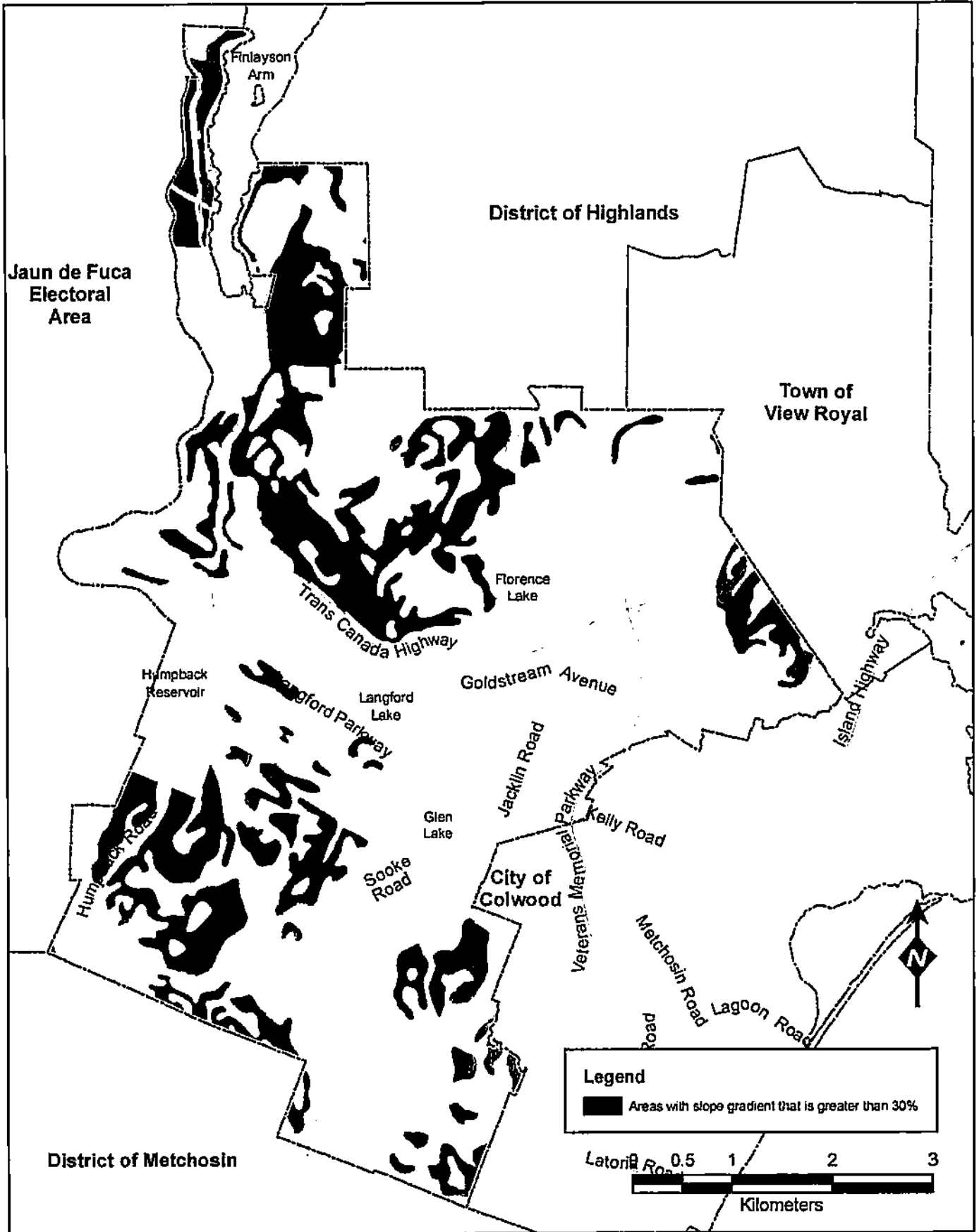


Map 15: Areas with Potential Habitat & Biodiversity Values



* Includes Garry Oak & associated ecosystem inventory data from 2007. Please note that this inventory data has not been verified and will be confirmed by an appropriate qualified consultant through the DP process as per guidelines in Bylaw 1201

Map 17: Steep Slopes





Tsawout First Nation
 Administration
 PO Box 121
 7725 Tetayut Road,
 Saanichton, BC V8M 2C3
 Tel: 250-652-9101 Fax: 250-652-9114

FACSIMILE TRANSMITTAL SHEET

TO:

Amilie Adinco
 Deputy City Planner

FROM ONE OF THE FOLLWING LISTED BELOW:

- Marc Jinnouchi, Director of Operations
- Karen Harry, Office Manager
- Russel Harder, Controller
- Sherri Wowehuk, Finance Clerk
- Maryanne Sam, Tax Administrator
- Rhonda Underwood, Social Development Coordinator
- Tuesday LaFortune, Social Development Clerk
- Ken Cossey, Lands Manager
- Gwen Underwood, Land Code Coordinator
- Eric Pelkey, Sencoten Alliance
- Dan Claxton, Tsawout Fisheries
- Receptionist
- Tsawout After School Program
- Other: _____

COMPANY:

City of Langford

DATE:

April 23/2008

FAX NUMBER:

TOTAL NO. OF PAGES INCLUDING COVER:

02

PHONE NUMBER:

RE:

LANGFORD OCP

URGENT

FOR REVIEW

PLEASE COMMENT

PLEASE REPLY

ORIGINAL IN MAIL

NOTES/COMMENTS:

CITY OF LANGFORD
Planning and Zoning
 1st Floor, 877 Goldstream Avenue
 Langford, BC V9B 2X8



e-mail: planning@cityoflangford.ca
 website: <http://cityoflangford.ca>
 phone: (250) 474-6819
 fax: (250) 391-3436

Referral Form

Application No. ALR-08- OP-08- DVP-08- FLR-08- L-08- OCP-08-04 TUP-08- Z-08-	Date: 21 April 2008
	Respond by: 5 May 2008
	Respond to: Emilia Adin, Deputy City Planner
	Referral checked by: Matthew Baldwin, City Planner
	Applicant: City of Langford
	PUBLIC HEARING DATE - 5 MAY 2008
Referrals sent to: Archaeology Branch BC Assessment Authority X BC Hydro - X BC Transit - emailed X Building Department - emailed X Bylaw Enforcement - emailed X Capital Health Region - emailed X City of Colwood - emailed X CRD Parks - emailed X CRD Planning - emailed X CRD Water - emailed X Crown Lands (Land & Water BC Inc.) - emailed X Department of Fisheries and Oceans - X District of Highlands - emailed X District of Melchosis - emailed X Energy & Minerals Division - emailed X Engineering Department - emailed X Finance Department - emailed X Fire Department - emailed X Parks Department (Jason Parks) - emailed X Forests - emailed X Heritage Branch - emailed X First Nations Reserve #13 - emailed X Land Commission (ALC) - emailed X Ministry of Transportation (Ross Deveau) - emailed X Ministry of Environment (Marlene Caskey) - emailed X RCMP (Constable Chippierfeld) - emailed X School District No. 62 - emailed X Songhees Indian Band - emailed X Town of View Royal - emailed X West Shore Environmental Services - emailed	Present Zoning: Proposed Zoning: Present OCP: Proposed OCP: DP Area: Fees Paid: Proposal: The public hearing has been scheduled for Monday, 5 May 2008. Please have any comments delivered to us by that date. A link to the most recent draft Bylaw is attached for your perusal. Paper copies can be made available at Langford City Hall. http://www.cityoflangford.ca/OCP/Langford%20Final%20Draft%204-17-08%20OCP.pdf Tentative Development Review Meeting: Tentative Planning and Zoning Committee Meeting: Name: <i>Chris Palkey</i> Title: <i>Tsawout Member's Duty Office</i> Phone: 250-544-0962 Date: <i>April 21/2008</i> Email: <i>chpalkey@tsawout.ca</i> <input type="checkbox"/> No objection <input type="checkbox"/> No objection subject to conditions below <input checked="" type="checkbox"/> Approval NOT recommended due to reasons outlined below Comments: <i>Infringes on the Aboriginal Rights and Title of the Seawich Tribes, includes Indian Reserve #13 owned in common by Tsawout, Tsat'is, Squamish, Tsaywan and Malahat First Nations; all members of the Seawich Tribes. This has been done without consultation or inclusion of the First Nations listed above</i>
<input checked="" type="checkbox"/> Entered in CityView	<input type="checkbox"/> Entered in Report

October 3, 2007

Langford City Hall
2nd Floor – 877 Goldstream Avenue
Langford, British Columbia
V9B 2X8

To: Langford Mayor and Council

Re: Lakefront Zoning

Please accept this letter as a request to consider the following regarding lakefront zoning in Langford. This is in addition to the 1) petition and 2) letter already received from Glen Lake residents. We understand that the Planning and Zoning Committee will be reviewing these items on October 9, 2007 and will forward a recommendation to Mayor and Council for the October 15, 2007 council meeting.

We live on Glen Lake and have noticed that two developments have been approved that has brought to light the allowed zoning on lakefront properties. The current zoning allows for multiple homes on the shorefront by the changes incorporated in 1998 where the minimum lot size changed from 850 square metres to 550 square metres. This type of development had not been utilized that we were aware of, and therefore had not come to the attention of most lakefront residents. Generally, most homes on Glen Lake are one home on the lakefront and if subdivided the home(s) are behind.

We respectfully request that the Planning and Zoning committee and Mayor and Council consider all the lakes residents' concerns and change the existing zoning as it applies to lakefronts in Langford. We understand that the Official Community Plan (OCP) could be the forum to incorporate these changes. In the interim, we respectfully request that Mayor and Council not consider any rezoning requests that increase the current density around lake front properties until the OCP consultant has allowed for public input and put forward policy changes.

We have spoken with many residents of Glen Lake, Florence Lake and Langford Lake and found that many citizens are enthusiastic to participate in the OCP process in order to assist with guiding the future of our community. Our understanding is that the next OCP sessions are November 7th and 10th, 2007. Additionally, some of the Glen Lake residents are interested in working together to form a Neighbourhood Plan. Ideally, a way to work together to have the unique needs of each area considered would be best. I understand there is a tool that can be utilized called Cityview that is an aerial photograph and allows for the overlay of property lines, riparian set backs and other land features. The usage of this tool would enable all residents to know exactly how the rules/laws/zoning and land features affect their property and to know exactly what requests residents want to put forward to Council. We look forward to working with municipal staff to learn these processes.

Currently, the letter mentioned in the first paragraph states a "request to increase the riparian setback by 15 metres from 43 to 60 and to not allow any development to receive a variance for side, rear or front setbacks. Also, request the minimum size of lakefront properties be increased from 550m² to 750m²." We propose that the request to increase the riparian setback be removed or disregarded from this request. We have spoken with many Glen Lake residents including some that signed the letter and verbal agreement has been given to amend this part of the request of the letter. Ideally, if a stay on hearing any more requests for lakefront rezoning is favourably decided effective now by Mayor and Council, pending a full discussion with residents has taken place either within the OCP mandate, a Neighbourhood Plan or lakefront citizens' meeting, then the urgency to make the requested changes can be alleviated.

We feel that the integrity of our lakes is worth special attention and citizen involvement, many residents feel that they wish to leave the legacy of our lakes to the future generations in the same beauty and quality that we have enjoyed for many years.

In order to help us with that vision, we respectfully ask that the Planning and Zoning Committee and Mayor and Council consider the message from the majority of lakefront residents.

Thank you for your consideration.

Yours truly,

A handwritten signature in black ink, appearing to read "Martin Sabell" and "Kelly McNulty Sabell". The signature is written in a cursive, flowing style.

Martin Sabell and
Kelly McNulty Sabell

/kms

207-42

I have lived on Florence Lake for more than 35 years, and have always cared a lot about the Lake. About 35 years ago a neighbour and I started the Florence Lake Involvement Association to bring residents around the Lake together in what was then unorganised territory, and formed working parties cleaning up the Lake and make it safer for children. We removed about a dozen of big trees that had toppled into the Lake, become water-logged and partly sunk hazards, local businesses volunteered dumptruck time during weekends. We raised more than \$1,200. to build a public float in front of the beach, replacing an old raft of water-logged logs with big spikes sticking out, on which numerous kids injured themselves. One or two years later we found that the lake had become polluted with high coliform counts and swimmers developing nasty infections where their skin was broken. We forced a number of septic fields to be checked and repaired and a non-existing field to be actually built, and the trailer court at the north end of the Lake was obliged to build a new septic field a further distance from the Lake. I am relating this to you, not to beat my drum, but to illustrate that I have always had strong parental urges about the Lake, and I still do. I think that with zoning issues it is important to establish people's motives, and mine are for the public good.

Now I feel strongly (and have met, and continue to meet, many who feel likewise) that Florence Lake, Langford Lake, and Glen Lake — the three so-called jewels of Langford — need to be protected against overpoweringly dense development of individual lots on their immediate shores. The provision of sewers around the 3 lakes frees up space required for septic fields, and sewer trigger the existing zoning to allow much denser residential development which in turn diminishes the natural character & ecosystems around the lake shores. This has started over the past year and points toward inadequate Official Community Plan & zoning protection of the 3 jewels. If unchecked, the city will end up with 3 ponds surrounded by concrete. Over the past four months this has been on my mind and I have been mulling this over in my head. And now that a number of Glen Lake residents have come on board, I believe that there is support among a number lakeside property owners on all 3 lakes in this city to downzone their lakefront properties to a new one-family lakefront residential zone LFR, the intent of which is to preserve through less density as much as possible the characters & natural surroundings of the 3 lakes, lakes that belong to ALL the residents of the city.

Just like the consultant on the Agricultural Land Reserve review in this city last week recommended what he calls "Edge Planning", so too must the city put in place policies that plan for responsible residential development of lakefront properties, in order to preserve as much as possible the characters & natural surroundings of the 3 lakes, lakes that belong to ALL the residents of the city. *First of all, that the full lakefront lots be included in the existing Development Permit Areas around the lakes, not just the first 43m from the lakeshores, and also that the city draw up & establish general, natural landscape plan criteria for lakefront lots. As part of Development Permit applications for lakefront lots landscape plans must be submitted.

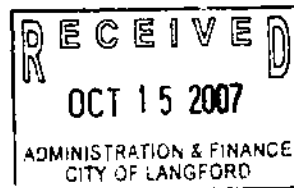
*Secondly, that a general city policy must be established to, in the public interest, very severely limit zoning requirement variances in Development Permit Areas on lakefront properties to between 0% and 7% of requirements. *Thirdly, that the city require alternate building envelope space be reserved on lots where non-conforming dwellings are situated within the 30m riparian setback, so that if these should burn to the ground or otherwise go to ruin they can be rebuilt in conformance with the riparian setback zones. *Fourthly, that non-conforming single-family dwellings may not be changed into duplexes in the R2 zoning, while all or part is non-conforming within the 30m riparian setback from the lakes. *Fifthly, that parking requirements be established for lakefront lots in the R1 & R2 zones, taking account of secondary suites and possibly four boarders permitted in these zones.

Planning staff have reassured me that it is not an enormous big deal to establish a new zone such as the proposed lakefront residential LFR zone. The following regulation suggestions would be instrumental to the purpose of this LFR zone: ° one one-family dwelling of a maximum size of 360 m² and of a maximum height of 7.5 m (the size is just under 4,000 sq ft — a number of neighbourhoods in Oak Bay now have the 360 m² cap.) ° subdivision requirements: -minimum lot area: 1,000 m²; -minimum lot width: 20 m; -minimum lot line setback: 4 m. ° lot coverage not to exceed 30% ° not more than 2 boarders. ° if in exceptional circumstances variances are applied for that are in excess of 7%, then that could be considered through a public hearing process, so that all the residents of the city can have input on what seriously affects THEIR lakes. Council may consider carrots for property owners acting for the common good and applying to downzone to LFR and by making it simple and painless. Similarly that could apply to covenants, but these are much more restrictive because they are forever and cannot evolve in time.

Earlier this year Council passed by-law 1040 amending Developments Permit Area Guidelines and which in Clause VI directs applicants to retain at their expense a qualified environmental professional who certifies that no negatives occur with activities in the riparian zone (paraphrased). Well, there is a Cumberland adage that says: "The Piper Plays For Who Pays". Just like in legal cases there are highly qualified experts for the defence and ditto for the prosecution, who usually arrive at very different conclusions. It makes much more sense and inspires much more confidence if this Clause VI were amended so that the city would hire the environmental professional and bill the applicant for the assessment. The city needs to have a policy so that experts are not restricted in what they can report on, but instead they should be encouraged to include in their report any concerns as they become aware of them, and also that these assessments can often only be done properly at the right time of year, or, when necessary, are spread out over all seasons of a year.

Wherever lakes are strongly conserved and protected from intrusive and inappropriately dense development, property owners are rewarded for this security by high property values. Property values on Prospect Lake, where Saanich aggressively protects the riparian areas and the character & environmental qualities of that lake, are very high because of the security that is provided to their environment. I request urgently that Council ensures that the jewellike qualities of the 3 lakes in the city are well stated, regulated, and protected by the new Official Community Plan update, and that sessions will be scheduled with the co-ordinator and the 3-lakes residents and all city residents for THEIR lakes.

Finally, I am pleased and somewhat amazed that the City of Langford is able to attract and keep such pleasant, helpful, and professional staff. They were that, and more even, when taking on extra duties during the summer holiday period. Thank you all!



[Handwritten signature]
Axel Brøck-Miller,
2621 Savory Road,
Langford.

Aug 24/09

We the undersigned are against this small, narrow lot lake front subdivision located at 3177 Glen Lake Rd. One lot with existing house on the lake front, and two house lots where lot 2 and lot 1 are, located on the present plan is sufficient.

Barbara Rose

Tom & Dana Rose 3171 Glen Lake Rd. Tom Rose -
Zuel ~~Murphy~~ 3167 " " "

E. Ashworth 3165 GLEN LAKE RD.
ROSS ASHWORTH " " "

TIM CORDNER 3182 GLEN LAKE RD.

Kathy Lohm 3182 Glen Lake Rd.

BRADLEY FENN 3152 MORNINGTON PLACE

Kay Fenn 3152 Mornington Place

Don Andrews 3150 Mornington Pl

Dale Bonner 3155 Mornington Pl.

~~James Moman~~ 3181 GLEN LAKE ROAD

3181 Glen Lake Road

AK Man 3167 Glen Lake Rd.

Andrea Martin 3167 Glen Lake Rd.

Melvin Durie 3163 Glen Lake Rd.

Earl Dumas 3167 Glen Lake Rd.

J. Bonner 3151 GLEN LAKE RD.

FRANK HADSON 3150 GLEN LAKE RD. ~~Frank Hadson~~

~~James Moman~~

~~Anette Moman~~



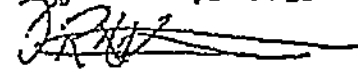
~~Andrea Martin~~

James Moman

Anette Moman

Andrea Martin


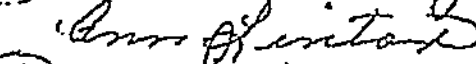
MARGARET D WHELDON	3030 GLENNAN RD	
KEITH MYERS	1015 Loch Glen Pl.	
Donna Robertson	1019 Loch Glen Pl	
Zoyla Leon Rojas	Mc Nulty 2965 Glen Lake Rd,	
Candace Galloway	2975 Glen Lk.	
Brad Galloway	2975 Glen Lk.	
Neta Sabell	2979 Glen Lake	
Stan Sabell	2979 Glen Lake	S. Sabell
SANDRA PETERSEN	3177 GLEN Lake Rd	
Geoff Peterson	3179 Glen Lake Rd	
TERRY KEAN	956 Page Ave. B.C.	(M. Kean)
Pat + John Zeman	948 Page Ave BC	
Charles E. (Ted) Mills	940 — " —	
Jared Sigmund	3180 GLEN LAKE	
GINA REID	991 Mary Ellen Place Reid.	
	991 Mary Ellen Place	


 3143 Glen Lake Rd.
 Julie Kujala 3143 GLEN LAKE RD.
 LAURA JENKINS 3153 Glen Lake Rd. } CO-OWNERS
 Dorothy Jenkins DOROTHY JENKINS 3153 GLEN LAKE RD }
 Roger Plus 3170 Glen Lake Rd
 Zoe Indur 3170 Glen Lake Rd.
 3076 Shoreview Drive

Jim + Jim London 3121 Glen Lake Rd Victoria
 Lou Ann 3119 " " " " " "

SEAN RANDALL 1198 WATER LILY LANE
 TOWER PLOZ 1190 Waterlily Lane

Phil Kovaly 3127 Glen Lake Rd
 Judith A. Krause 3133 Glen Lake Rd.

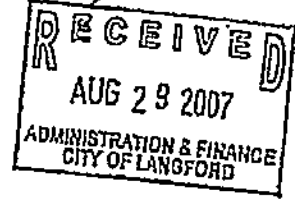
 3063 Glen Lake Ave.
 3147 Glen Lake Rd

Donna Hupod 3159 Glen Lake Rd.
 Kim Rose 3141 Glen Lake Rd.

Jan Belandson 3137 Glen Lake Rd
 Jackie Bayles 356 Glen Lake Rd
 H S. BORTOLANCO 999 GLEN LAKE RD.

Kuce Petera Gray Lee 1003 Loch Glen Pl.
 Lynn Mage 1023 Loch Glen Pl
 Chi Bui 1023 Loch Glen Pl
 Glenn Whitney 2920 Glen Lake Rd
 Vince Smythe 1035 Loch Glen Pl.
 R & J ARLETT 1031 ✓ ✓ ✓
 Daphne Knize 3034 Glen Lake Rd.

File # 3220-20-SUB-07-23 3177 Glen Lake.



August 29, 2007

Dear Mayor and Council,

On behalf of the following Glen Lake residents I would like to request that with respect to development around the lake perimeter council increase the riparian setback by 15 meters from 43 to 60 meters and to not allow any development to receive a variance for side, rear or front yard setbacks. I would also like to request that the minimum size of lakefront properties be increased from 550m² to 750 m².

Sincerely,

Geoff Reid
Geoff Reid
Tri Lakes Committee

991 MARY ELLEN PLACE.

Candace Galloway 2975 Glen Lk. *Ch Galloway*
Lanna Rose 3171 Glen Lake.
Tom Rose 3171 Glen Lake Rd.

Jim Galloway 2925 GLENWIRE *Jim*
RUSS HATHWORTH 3165 GLEN LAKE RD
Ellen Carwood " "
Mama 3163 Glen Lake Rd.



OCP-06-04

Garry Oak Ecosystems Recovery Team (GOERT)
301-1205 Broad Street
Victoria, BC V8W 2A4
Canada
Attention: Louise Goulet, Executive Director

November 8, 2006

Dear Ms. Goulet,

Re: Partnership of City of Langford with City of Colwood and GOERT

This letter is to confirm that the City of Langford has agreed to work together in partnership with Colwood and GOERT to identify Garry Oak sites. We understand that GOERT also proposes to assist in developing policies and practices that protect these endangered ecosystems; the City of Langford is interested in considering all options towards this goal.

We hope that this project will inform both our upcoming review of Langford's Official Community Plan and our planned update to our Master Parks Plan. As such, we agree to support Colwood and GOERT in their efforts to finalize a work plan and funding submission.

Sincerely yours,

A handwritten signature in black ink, appearing to read "Matthew Baldwin".

Matthew Baldwin, MCIP, RPP

cc. Jon Munn, City of Colwood

eka



Referral Form

Application No. ALR-08- OP-08- DVP-08- FLR-08- L-08- OCP-06-04 TUP-08- Z-08-	Date: 10 March 2008 Respond by: 17 March 2008 Respond to: Emilie Adin, Deputy City Planner Referral checked by: Matthew Baldwin, City Planner Applicant: City of Langford Owners: Address: Legal:
Referrals sent to: Archaeology Branch BC Assessment Authority X BC Hydro - mailed X BC Transit - emailed X Building Department - emailed X Bylaw Enforcement - emailed X Capital Health Region - emailed X City of Colwood - emailed X CRD Parks - emailed X CRD Planning - emailed X CRD Water - emailed X Crown Lands (Land & Water BC Inc.) - mailed X Department of Fisheries and Oceans - mailed X District of Highlands - emailed X District of Metchosin - emailed X Energy & Minerals Division - emailed X Engineering Department - emailed X Finance Department - emailed X Fire Department - emailed X Parks Department (Jason Parks) - emailed X Forests - mailed X Heritage Branch - mailed X First Nations Reserve #13 - mailed X Land Commission (ALC) - emailed X Ministry of Transportation (Ross Devaau) - emailed X Ministry of Environment (Marlene Caskey) - emailed X RCMP (Constable Chipperfield) - emailed X School District No. 62 - emailed X Songhees Indian Band - mailed X Town of View Royal - mailed X West Shore Environmental Services - emailed	Present Zoning: Proposed Zoning: Present OCP: Proposed OCP: DP Area: Fees Paid: Proposal: Proposal to update the City of Langford Official Community Plan. A paper copy can be obtained at our City Hall, or you may download a copy from our website, at: http://www.cityoflangford.ca/newsarticle.asp?TopicID=424 Tentative Development Review Meeting: Tentative Planning and Zoning Committee Meeting: Name: <i>M. Riefman</i> Michael Riefman, C.P.H.I. (C) Title: Registered Environmental Health Officer Phone: 478-5110 Date: MAR 12/08 Email: MICHAEL.RIEFMAN@VIHA.CA <input type="checkbox"/> No objection <input checked="" type="checkbox"/> No objection subject to conditions below <input type="checkbox"/> Approval NOT recommended due to reasons outlined below Comments: RE POLICY 7.2.2 (SECONDARY SUITES PERMITTING): A REFERRAL PROCESS SHOULD BE IN PLACE WITH VIHA TO ENSURE THAT ON-SITE SEWERAGE SYSTEMS ARE ADEQUATE OR REQUIRE UPGRADING RE POLICY 12.5.1 (GREYWATER REUSE) GREYWATER MUST BE TREATED TO MINISTRY OF HEALTH STANDARDS <input checked="" type="checkbox"/> Entered in CityView <input type="checkbox"/> Entered in Report

Dianna Plouffe

From: Larry Parkinson [Larry.Parkinson@corix.com]
Sent: Monday, March 10, 2008 4:01 PM
To: Dianna Plouffe
Subject: RE: Referral OCP-06-04
Attachments: OCP-06-04 City of Langford OCP.pdf

West Shore Environmental Services has no objection to Referral OCP-06-04.

Regards,
Larry Parkinson, CTech
Coordinator, Engineering & Construction
West Shore Environmental Services
103 - 859 Orono Avenue, Victoria, BC, V9B 2T9
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-----Original Message-----

From: Dianna Plouffe [mailto:dplouffe@cityoflangford.ca]
Sent: Monday, March 10, 2008 3:48 PM
To: Dianna Plouffe
Subject: Referral OCP-06-04

Good Afternoon,

Please see the attached referral for your review and response.

****Please note**** - if you received this referral earlier today, the due date for responses has been changed to March 25th.

Dianna Plouffe
Planning Secretary
City of Langford
Office: (250) 474-6919
Fax: (250) 391-3436
2nd Floor, 877 Goldstream Avenue
Victoria BC, V9B 2N8
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website: www.cityoflangford.ca



Please consider the environment before printing this email

March 20, 2008

Matthew Baldwin
City Planner
City of Langford
Langford BC.
V9B 2XB

Dear Matthew Baldwin

BC Transit's comments on the proposed Langford Official Community Plan:

Thank you for the opportunity to comment on your proposed OCP. In general, the OCP and policies support improved transit service and more sustainable transportation options.

The proposed alignment for LRT outside the municipal boundaries does not appear to reflect the direction established by the Capital Regional District's Regional Growth Strategy and the Travel Choices Study.

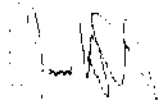
BC Transit recommends that instead of identifying one corridor that Langford identify the need to review and establish a corridor for future LRT. This review should involve other municipalities, the region and regional transportation agencies

Other policies which would be considered to strengthen the links between transit and the development process and to assist in the provision of transit service and transit amenities in new neighborhoods include:

- A policy in regards to that design of road networks in new developments which are conducive to fast and direct transit.
- A policy that encourages pedestrian pathways in areas which are difficult to link to transit through the road network such as cul-de-sacs.
- A policy that improves multi-modal connections with transit for pedestrians and cyclist by installing amenities such as shelters, accessible features, pedestrian lighting and bike lockers at high activity stops.
- A policy that when higher densities rezoning occurs that applicants are required to upgrade the local bus stop (within 200-400m walking distance) with amenities such as shelters, accessible features or pedestrian lighting
- A land use policy that encourages higher densities and commercial development on existing transit corridors
- A land use policy that addresses the needs of an aging community such as locating seniors' services and housing to close proximately to transit.

Thank you for the opportunity to comment on the City of Langford's Official Community Plan if you have any questions in regards to BC Transit's comments do not hesitate to call me. We look forward to working with Langford on improving transit service in your community.

Sincerely,



James Wadsworth,
Transit Planner,
Phone: 385-2551,
Email: James.Wadsworth@bctransit.com.



Garry Oak Ecosystems Recovery Team

Dear Emilie,

The Garry Oak Ecosystems Recovery Team (GOERT) is pleased to have participated in the OCP review process for Colwood and Langford over the past year through public meetings, information sessions, design charrettes, as well as providing technical information (i.e. mapping) related to Garry oak and associated ecosystems in both municipalities. In addition, GOERT contracted Deborah Curran, a local expert with 10 years experience providing legal advice to local governments for implementing smart growth strategies, land development and urban sustainability, and the primary author of the Green Bylaws Toolkit, to review and provide comments on the recently released draft OCP for Langford. Ms Curran's comments and suggestions are provided here in PDF format (attached). We sincerely hope that you will find this information useful for developing an OCP that offers protection of environmentally sensitive ecosystems including species at risk, while undergoing significant growth and development within the community of Langford. Also attached to this message is one of the maps that GOERT produced as part of the review process that indicates the areas of Garry oak ecosystems in Colwood and Langford, and I will follow this email with one more that includes a similar map but with species at risk locations indicated.

Please feel free to contact myself or Chris Junck (chris.junck@goert.ca / 383-3445) at GOERT, or Deborah Curran (info@dcurranandco.ca / 882-0642) should you have any questions about this message or the attached document.

Kind regards,

Deanna Mathewson, M.Sc.
Program Coordinator
Garry Oak Ecosystems Recovery Team (GOERT)
301-1205 Broad Street, Victoria, BC V8W 2A4
Tel: 250.383.3224 / Fax: 250.479.0546
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MEMO

To: Garry Oak Ecosystems Recovery Team

Attention: Deanna Mathewson and Chris Junck

From: Deborah Curran

Date: April 11, 2008

Re: **Comments on Draft City of Langford Official Community Plan**

The purpose of this memo is to provide you with comments on and suggestions for the City of Langford Official Community Plan Draft #2 dated March 25, 2008 (Draft OCP). The intent is to provide assistance with best practices in the areas of protection of environmentally sensitive areas, particularly Garry Oak and associated ecosystems, and species at risk. I would be pleased to discuss any of these comments and suggestions with you.

I make two different types of comments. The first suggests modifications or additions to the draft policy to strengthen environmental protection. The second asks questions and gives options where policy is absent or unclear.

My comments are arranged to reflect the order of the sections in the Draft OCP. Where I deal with a specific policy I list the page number or section in the Draft OCP where it can be found. Please note that the heading numbering in this document reflects the heading numbering found in the Draft OCP.

Overall, it is refreshing to read an OCP that is based on sustainability principles and attempts to integrate the natural, built and cultural environments with significant attention paid to density and urban design. It is also positive to see several statements that encourage the use of native vegetation in landscaping. However, the natural environment, being the foundation for ecological function and sustainable communities, unfortunately does not receive the level of detail that it warrants. I have focused particularly in my comments below on situating the Langford Draft OCP in the regional context, strengthening the draft policies in Part II, Chapter 3 (Chapter 3 in the Draft OCP but listed as Chapter 4 in the Table of Contents), and making better use of development permit areas for protection of the natural environment.

One more general comment that relates to measuring the success of this OCP over time. There are few measurable targets in this Draft OCP. While I appreciate that council and staff would like to retain significant flexibility when assessing applications for development to respond to opportunities, measurable goals reflect the community's values and tell applicants what will likely be expected of them. It assists applicants to



craft the type of project that the community and council will likely support. Targets for biodiversity, natural space, park space per capita (which the Draft OCP makes reference to on p.40), affordable housing, and transportation options are becoming the norm for comprehensive OCPs in British Columbia.

Part I OCP Overview

1.0 Introduction

The final text on p.6 states that after adoption all works and bylaws must be consistent with the OCP. A regional perspective is warranted here as well, that the OCP must be consistent with the RGS, to alert readers to the larger context within which the OCP is developed.

2.0 Sustainability Goals

I applaud Langford's sustainability goals, and this is a good model for OCPs in the province. I note that the first sustainability goal relates to ecosystems and biodiversity, the foundation upon which a healthy economy and society rest. It states, "Maximize the ecological value of natural areas. Where development occurs, maintain ecosystem values."

It is this point to which I will return several times in my comments below. To clarify, the Draft OCP seeks to allow development to occur AND to maintain ecosystem function and biodiversity in the face of that development.

3.0 Our Sustainable City

It is in this section that terminology about the green infrastructure becomes somewhat unclear and challenges the reader throughout the rest of the Draft OCP to understand how biodiversity and ecosystem function will be integrated throughout the municipality. The terms greenway, open space, park, natural area ecological value and biodiversity appear to be used somewhat interchangeably, and are not defined in the Glossary of Terms. The wording throughout the Draft OCP requires the reader to infer that wherever the term park, open space, natural area, greenway, etc. are used, biodiversity and ecological function will be preserved. However, it is not clear how biodiversity goals are met using a greenway that is a sidewalk with grass and trees on either side, or a park designed for active recreation such as playing fields (versus passive recreation of natural areas). Integrating parks, recreation and open space does not equal supporting biodiversity.

The Draft OCP would be strengthened considerably from the perspective of biodiversity and ecosystem values if these terms were differentiated and more specific terminology used. I also suggest measurable biodiversity targets and provide more detail below in suggested monitoring indicators.

It is important to note that the resilience and redundancy of biodiversity is a key aspect of sustainability. Successful adaptation to climate change in each region will depend to a large degree on the ability of the natural environment to adjust. Maintaining adequate ecosystem function pre-climate change impacts is a precondition for adaptation over time.

Growth Management & Land Use Strategy

The Growth Management and Land Use Strategy is the fundamental macro statement of where growth will occur and how much of it will happen in each area. The Strategy as presented is very clear, makes sense and enables useful densities for achieving other sustainability goals.

If I understand the overall growth management and land use strategy from an ecosystem perspective, biodiversity values will primarily be supported on greenways corridors identified in the Natural Setting, Parks & Open Space Integration Strategy (p.43 of Part II, Chapter 3, hereafter referred to as the Integration Strategy). This is predominantly on lands in the Hillside or Shoreline Mixed Use, Agriculture Strategy Lands, and Open Space designations.

The Draft OCP would be significantly strengthened if the Integration Strategy was the foundation for this section on Growth Management and Land Use. As the building block for sustainability, greenways and biodiversity is the underpinning within which development occurs. A sustainable city requires the Growth Management & Land Use Strategy to build upon or be integrated into the Integration Strategy. Indeed, the Draft OCP recognizes this with the opening words of Part II Chapter 3 (p.36): "A community's natural settings, parks and open spaces are form-makers for creating sustainable communities."

Finally, the Draft OCP is very strong on urban design, density, growth management (locating growth in the right places), mixed-use and creating compact complete communities. Attention to the natural environment beyond "parks and open space integrated throughout" is warranted in each land use category and demonstrates a sophisticated understanding of Sustainability Goal number one. The natural environment and ecosystem function is a land use in each land use designation, like residential, commercial and industrial uses.

Part II: Community Objectives & Policies

3.0 Our Natural Settings and Open Spaces

I commend the focus throughout this Chapter and the Draft OCP on connecting the patchwork of open spaces and natural settings. This approach is significantly more valuable from a biodiversity perspective than are large unconnected parks. However, this statement depends on actual biodiversity function throughout the open spaces and natural setting.

The title for this section, “Natural Settings and Open Spaces” is somewhat of a misnomer as the section deals with the natural environment. As the foundation for sustainability, the natural environment is not a natural setting or open space, it is functioning ecological systems that maintain human health and the economy of a region. The title is somewhat weak given the importance of the natural environment and sensitive ecosystems in an OCP based on sustainability principles, and in light of Sustainability Goal number one.

Strategy

If it is assumed that environmentally sensitive areas (ESAs) include species at risk, this should be stated.

Policy 3.1.2 Update mapping of ESAs

It is always appropriate to update mapping when applications for development come forward, however the City of Langford has the benefit of significant existing mapping resources that it does not appear to be using to their fullest. The Garry Oak Ecosystems Recovery Team has provided maps of both the extent of Garry oaks and associated ecosystems, and species at risk. The Sensitive Ecosystems Inventory for East Vancouver Island and the Gulf Islands (http://www.env.gov.bc.ca/sci/van_gulf/index.html) and the CRD Natural Areas Atlas (<http://www.crd.bc.ca/maps/natural/atlas.htm>) also provide important baseline information that can be included in Map 3 (p.38). All of this mapping can be used as a baseline with a commitment to obtaining better and smaller scale mapping through the use of Development Approval Information for all rezoning and development permit areas for protection of the natural environment (EDPAs) applications.

Policy 3.1.3 Expand policies for protecting etc. ESAs

What does this policy mean? Expand what policies? If OCP policies are meant, it appears that now would be the time to include them in the Draft OCP. If EDPA is meant, then more detailed policies should be developed as part of this process rather than as an amendment in the future. I suggest the following “expanded” policies for this OCP, and while some of the objectives and policies listed below are similar to those found in the Draft OCP, they are worded more strongly and are more precise in their guidance. I have divided them into topic areas for clarity, and to expand this existing chapter. Surprisingly this chapter is lacking in policies on all aspects of the natural environment, such as water. Again, this might be a problem of terminology but protecting ESAs and preserving natural spaces does not necessarily mean water cycles or quality are maintained:

Objectives –

- Preserve sensitive ecosystem areas, their living resources, and connections between them in a natural condition and maintain these areas free of development and human activity to the maximum extent possible.

Policies –

- Development design must reflect the objectives and guidelines of Best Management Practices, including those detailed in:
 - Develop with Care: Environmental Guidelines for Urban and Rural Land Development in British Columbia
http://www.env.gov.bc.ca/wld/documents/bmp/devwithcare2006/develop_with_care_intro.html;
 - Best Management Practices for Amphibians and Reptiles in Urban and Rural Environments in British Columbia
<http://www.env.gov.bc.ca/wld/BMP/herptile/bmpherptile.html>;
 - Best Management Practices for Raptor Conservation During Urban and Rural Land Development in British Columbia
http://www.env.gov.bc.ca/wld/documents/bmp/raptor_bmp_final.pdf;
 - Instream Flow Guidelines for British Columbia
http://www.env.gov.bc.ca/wld/BMP/instreamflow_wkgdrft.html;
 - Standards and Best Management Practices for Instream Works
<http://wlapwww.gov.bc.ca/wld/documents/bmp/iswstdsbpsmarch2004.pdf>;
 - Riparian Areas Regulation Assessment Methods
http://www.env.gov.bc.ca/habitat/fish_protection_act/riparian/riparian_areas.html;
 - Best Management Practices for Lakeshore Stabilization
http://www.env.gov.bc.ca/wld/documents/bmp/BMPLakeshoreStabilization_WorkingDraft.pdf;
 - Environmental Objectives and Best Management Practices for Aggregate Extraction <http://wlapwww.gov.bc.ca/vir/pa/aggregate.pdf>;
 - Stream Stewardship: A Guide for Planners and Developers
http://dev.stewardshipcanada.ca/sc_bc/stew_series/NSCbc_stewseries.asp?sProv=bc&siteLoc=scnBC&lang=en#ssg;
 - Access Near Aquatic Areas: A Guide to Sensitive Planning, Design and Management
http://dev.stewardshipcanada.ca/sc_bc/stew_series/NSCbc_stewseries.asp?sProv=bc&siteLoc=scnBC&lang=en#access;
 - Stormwater Planning: A Guidebook for British Columbia
<http://www.env.gov.bc.ca/epd/epdpa/mpp/stormwater/stormwater.html>; and
 - Community Green Ways Linking Communities to Country and People to Nature
http://dev.stewardshipcanada.ca/sc_bc/stew_series/NSCbc_stewseries.asp?sProv=bc&siteLoc=scnBC&lang=en#cg.
- It is the policy of the City to develop a sound information base about all sensitive ecosystems to inform land use plans, regulatory processes and other priorities for protecting sensitive ecosystems. The City will map ESAs, including Garry oak and associated ecosystems and species at risk, and create a comprehensive Sensitive Ecosystems Inventory that describes all sensitive ecosystems. In addition, the City will require applicants for development to obtain and present all available information about the site from the Conservation Data Centre, Sensitive Ecosystems Inventories, natural areas atlases and other relevant inventories.

- The City will undertake, or assist other government agencies and community organizations in undertaking, to provide information through brochures, seminars, presentations, and other educational activities, to landowners of sensitive ecosystem lands and all residents of the City on the importance of aquatic habitat and other sensitive ecosystems, and ways in which they can help to preserve these important resources.
- The City will support efforts of senior agencies and community organizations to restore damaged habitat and sensitive ecosystems.

Covenants and Conservation Zoning

- The City will work with landowners to protect and preserve sensitive ecosystems using one or more of the following measures, where appropriate:
 - a. dedication as a City park or trailway component, where the area complements the goals and objectives of the City's park or trailway systems. Sensitive ecosystems acquired as parks or trailways will be managed to protect their sensitive features from public use;
 - b. dedication to a private land trust or non-government organization, which are eligible to receive donations of land under the Federal Ecological Gifts Program for conservation purposes;
 - c. use of conservation covenants to preserve the natural values of sensitive ecosystems. The covenants may be held by the City, the Province and/or a nongovernmental organization eligible to hold conservation covenants;
 - d. registration of a statutory right-of-way under the *Land Title Act*;
 - e. adoption of bylaws to exempt eligible riparian property from property taxes where a property is subject to a conservation covenant under section 219 of the *Land Title Act*;
 - f. density bonusing, cluster housing, or other development incentives to facilitate the protection of all or a significant portion of sensitive ecosystems;
 - g. amalgamating lots to achieve greenways and ESA goals outside of urban containment boundaries; and/or
 - h. encouraging the amalgamation of lots outside the urban containment boundary.

Note that measures a. to c. may involve an ecological gift as defined by Environment Canada and may have tax benefit for the donor. See http://www.cws-scf.ec.gc.ca/ecogifts/intro_e.cfm. Where measures i. and ii. involve subdivision, the dedication may qualify for an expedited subdivision if it meets the requirements of s.99 of the *Land Title Act*.
- The Local government will develop and implement a system for keeping track of covenants related to protecting sensitive ecosystems, and of informing residents of their presence and significance.

Integrated Watershed Management

- Develop an integrated Greenways/Watershed Plan that:
 - coordinates land use activities:

- ensures the maintenance of ecosystem functioning;
 - includes integrated stormwater/rainwater management planning;
 - identifies a network of ecosystems that exist within the watershed;
 - identifies isolated ecosystems and establishes or enhances corridors, connections, and linkages with larger ecosystem networks; and
 - promotes connectivity between, and discourages fragmentation of, contiguous ecosystems and ecosystem components to preserve landscape diversity, and allow wildlife use, movement, and dispersal.
- Encourage and codify in City bylaws alternative design standards and best management practices for new developments that maintain ecosystem functioning and decrease impervious surfaces.
 - Encourage the narrowing of road widths to decrease the land area required for roads and minimize municipal maintenance costs.
 - Design sites so that the natural hydrological cycles (hydrographs) are maintained during and after development.
 - Minimize the amount of impervious surface and encourage groundwater recharge using rainwater management based on infiltration, narrower road widths, vegetated swales and pervious paving material.
 - Prohibit the discharge of unmanaged rainwater into watercourses.
 - Design buildings, infrastructure and other development so that established native vegetation, particularly trees, can be retained, with enough distance to protect the root system.

Incentives for Environmental Protection

- Encourage voluntary placement of conservation covenants, dedication of land or voluntary changes in zoning to protect sensitive ecosystems, by considering increased density on the balance of the subject property, an amenity bonus for another property, trading land, purchasing land, or granting tax exemptions.
- Exempt eligible riparian property from property taxes where a property is subject to a conservation covenant registered under section 219 of the *Land Title Act*.
- Allow the owner(s) of land affected by dedications for environmental protection to use the original site area in computing density and floor area ratios and minimum area for development or subdivision purposes, where appropriate.
- Support conservation organizations to secure important habitat by means of acquisition, conservation covenants or other stewardship agreements for conservation purposes.

Cluster Development

- Encourage cluster forms of development to reduce the amount of land affected by residential growth, where the permitted number of units is clustered on part of the site, protecting the remaining area in its natural state, or protecting adjacent important habitat, and consider alternatives such as comprehensive development zones, density averaging or other methods to achieve this purpose. A proposal for cluster development should clearly demonstrate and articulate how

environmental, economic and social development sustainability principles are satisfied, and meet the following conditions:

- a. the total area of land to be subdivided excluding undevelopable land, such as land in the Agricultural Land Reserve, watercourses and leave areas, sensitive ecosystems slopes in excess of 3:1 (30%), divided by the number of lots to be created is no greater than the density permitted under the Zoning Bylaw,
- b. the parcel configuration and sizes are adequate to accommodate buildings and structures appropriate to the intended use and in compliance with the Zoning Bylaw,
- c. a restrictive covenant is registered in the name of the City against the title to the land at the time of registration of the subdivision, prohibiting the further subdivision of the original parcel(s) under covenant,
- d. a long term management plan, including responsibilities and actions, for the future management of the remaining protected area is approved by the City. [Where additional conservation measures or provision of amenities are included in the cluster development proposal, the Director of Planning may recommend to the City that a comprehensive development zoning bylaw be considered.]

Public Use of Sensitive ecosystems

- Prohibit or severely limit recreational access into sensitive ecosystems to minimize impacts.
- Encourage development of appropriate interpretive media to explain why access is restricted and the ecological significance of the sensitive ecosystems.
- Limit public trails and public access points in watercourse leave strips and other sensitive ecosystems to locations where their presence will not compromise the habitat and ecological function of these areas.
- Protect sensitive ecosystems within City parks by building public trails and access points so as not to compromise the ecological functions of these areas.

Zoning

- Review and amend permitted uses in zones near sensitive ecosystems to prohibit or regulate uses that would have adverse impacts on the ecological function of the sensitive ecosystem.
- Review and amend density, lot size, and site coverage regulations on a watershed basis to ensure that they maintain or enhance ecosystem functions, specifically hydrologic functioning.
- Review and amend regulations for the siting, size, and dimensions of uses and buildings in zones adjacent to sensitive ecosystems to ensure the uses will not compromise the sensitive ecosystem.
- Create cluster housing zones for residential areas adjacent to sensitive ecosystems to allow a tighter grouping of houses or multiple-unit buildings on the most buildable portions of a site in exchange for retaining a large portion of the land, such as a sensitive ecosystem, in a natural state.

- Establish comprehensive development zones for complex sites within the Urban Growth Area and Future Growth Areas to enable careful site planning for conservation of sensitive ecosystems.

Watercourse/Wetlands Conservation

- Ensure that any uses, activities and development in a watershed which are connected to wetlands by hydrology or habitat do not negatively impact the health of wetlands and their functions.
- Establish integrated rainwater management policies that maintain the natural hydrology and natural environment of watersheds, groundwater, streams, and other waterbodies, including provisions that ensure the maintenance of minimum base watercourse flows.
- Enact or amend a watercourse protection or environmental bylaw that prohibits or restricts anyone from polluting or obstructing or impeding the flow of a stream, creek, waterway, watercourse, wetland, waterworks, ditch, drain, or sewer, and imposes penalties for contravention of the bylaw.
- All streams must be maintained in an open state (not enclosed in a culvert or other engineered material).
- Adopt a policy that limits the crossing of watercourses.
- Establish a program to remove obstacles impeding movement of fish such as inappropriately designed culverts and watercourse crossings.

Water Quality

- Protect water quality through best management practices for land development.
- Use engineered wetlands or other water detention facilities to filter pollutants before they can enter streams or creeks.
- Require the use of vegetated waterways and swales or other measures to prevent the movement of road salts and other contaminants into sensitive habitats.
- In areas of significant pavement ensure that pollutants such as oil and other hydrocarbons are removed by oil/water separators before they enter the groundwater or streams.
- Enact or amend watercourse protection provisions in bylaw format that:
 - a. Restrict the polluting or obstructing or impeding the flow of a stream, creek, waterway, watercourse, water body (including wetlands), waterworks, ditch, drain, or sewer, and imposes penalties for contravention of the prohibition;
 - b. establish a maximum percentage of lot or watershed areas that can be covered by impermeable material, particularly adjacent to Sensitive ecosystems;
 - c. establish standards for drainage works for the ongoing disposal of surface runoff and stormwater from paved areas and roof areas during and after construction to maintain natural runoff volumes and water quality
- Require erosion and sediment control plans before construction begins.
- Require the construction and stabilization of runoff management systems at the beginning of site disturbance and construction activities.

- Minimize disturbed areas and the stripping of vegetation and soils, particularly on steep slopes.

Partnerships

- The City will provide leadership in the development and implementation of a long-term strategy to acquire priority sensitive ecosystems, including:
 - a. acquiring and preserving sensitive ecosystems as part of local parks programs;
 - b. identifying acquisition priorities in co-operation with non-government and government conservation organizations;
 - c. identifying priorities for protection through development permit, rezoning, subdivision, and other regulations; and
 - d. acquiring additional lands that focus and limit the spatial growth of communities and provide a natural landscape setting for a community.
- Support and encourage individuals and community organizations to be involved in managing sensitive ecosystems, restoring and enhancing native habitats, planting appropriate native vegetation, preventing erosion, and installing signs to inform and educate the public.

Environmental Impact Assessment

- All development proposals that involve a change in zoning, subdivision or amendment to a plan must undergo the environmental impact assessment process (development approval information) before development approvals are granted.
- The City will require an Environmental Impact Assessment (EIA) to be undertaken for areas in DPAs for protection of the environment and areas subject to an application for rezoning for high impact uses prior to issuing development approvals. The purpose of the assessment is to review impacts on the environment of proposed uses and to identify or recommend any necessary development monitoring and mitigation measures.
- The applicant will conduct inventories for sensitive ecosystems and species at risk (plants, vertebrate and invertebrate animals) in appropriate seasons by registered professional biologists with species at risk identification expertise.
- Some of the key considerations to be addressed through the EIA process include:
 - Protection of watercourses, including ephemeral and permanent water courses. Note the principal watercourses are designated in Map [], however this only represents a landscape level of designation. More detailed on the ground assessment of the actual protection area is still required.
 - Preservation of other sensitive habitats including Garry oak meadows, grasslands, mature and old growth forests, seasonal (vernal) pools and seeps, ecologically sensitive rock outcrops, and connections between habitats.
 - Preservation of functioning ecosystems including conservation areas, buffers and wildlife movement corridors.
 - Appropriate mitigation measures to minimize impacts on habitat loss.

- Use of covenants, riparian area or park dedication, private amenity area designation or other appropriate measures to address the preservation of ecologically sensitive areas within the development blocks.
- The EIA must meet the development permit requirements for mitigation, compensation, protection or replacement to ensure the maintenance of ecological features and ecosystem functioning.
- Require as part of the development permit process funding through bonding to ensure the completion of landscaping and environmental rehabilitation (including invasive species management), and to address damage to the environment caused by development activity.

Policy 3.2.1 Set targets for canopy coverage

This type of target allows the City to monitor success over time and sets a bar for development applications. It is most appropriate to include the target in the OCP as part of this process. The Urban Forest Stewardship Initiative can assist with this policy http://www.hat.bc.ca/projects/ufsi/ufsi_q2.htm.

Policy 3.2.4 Enhance wildlife biodiversity

This appears to be the most direct reference in the Draft OCP to operations for biodiversity. Absent a commitment to developing a landscape strategy or a landscaping chapter in a Greenways Strategy (see below), this policy provides little direction or measurable standards for enhancing wildlife biodiversity and protecting species at risk.

Objective 3.3 Ensure natural areas etc. are connected with the Integration Strategy

If I understand correctly, the Integration Strategy shows the network of parks, natural areas and open spaces, as depicted in Map 6 on p.42. The West Shore Greenbelt (Policy 3.3.3) will include all lands that have biodiversity values. The creation of a comprehensive West Shore Greenbelt Strategy is, therefore, the key underpinning to maintaining biodiversity in this part of the region. Elaboration on what the West Shore Greenbelt Strategy or Agreement would contain is warranted, for example acquisition priorities for ESAs, the development of policies to secure elements of the Greenbelt and protect ESAs, programs for working with landowners to protect ecosystem function, watershed quality standards, biodiversity measures, and monitoring.

Policy 3.3.4 Density bonus

Density bonus is a key tool to achieve biodiversity and ecological values, and the City of Langford already has experience in this area. It is important to note that density bonus can be a controversial tool in small and medium sized communities if residents perceive that the local government is awarding much more density without any benefit to the community.

I recommend clear definition of three factors to promote understanding in the community of this approach. The first is to define the maximum uplift that the City will allow in each neighbourhood, e.g. 50 percent increase in density over base zoning. This allows

the amount of the bonus to be discussed beforehand with the community. The second is the list of priority amenities on a neighbourhood-by-neighbourhood basis so that each neighbourhood is receiving the appropriate amenity contributions. Thirdly, a clear formula for calculating the value of the uplift in density and the value of the amenities is required. Developers who opt into the density bonus program should be providing 50-60 percent of the increase in land value to the community in the form of amenities. The City may consider including in the list of community amenities "extraordinary environmental protection measures." The City will also want to consider whether green buildings qualify as a community amenity under the density bonus program. Many communities do not recognize green buildings as eligible under density bonus because that building form arguably provides more private benefit to the developers and purchasers than to the public.

I include here some additional policies to clarify a density bonus program:

- In this Plan amenity bonus and density bonus means allowing owners to develop land at an increased density or bonus over existing zoning in exchange for the owner providing a priority amenity to the community.
- Applications for Density bonus should propose a density level that does not exceed the amenity bonus density levels outlined in this Plan, as depicted in Map []. The amenity bonus target density levels are not more than a [e.g. 40%] increase over base zoning levels depicted in the land use Map []. Note that density bonus is not available for some properties or areas where increased density would not be appropriate.
- In only very limited circumstances will the City allow density bonuses on land that is outside existing residential areas [or the growth areas]. Land that is outside residential areas may be acquired and protected as the amenity part of the density bonus, but the development will generally occur inside residential areas. Density bonuses for land outside residential areas will only be considered where:
 - i) there are significant ecological benefits to entering into a density bonus scheme; and
 - ii) the development is clustered, maintains the rural character of the area, and has no significant environmental impact.
- Applications for density bonus should show that one of the eligible community amenities listed in Section [] will be provided in exchange for the higher density level being requested. Eligible amenities are listed in order of importance. The City will give the highest priority to applications that offer protection of the sensitive ecosystems identified in Map []. However, this priority list should not prevent the City from considering applications that provide amenities below the number one priority if a unique opportunity to do so arises.
- Density bonus applications should be consistent with other policies of this Plan regarding rezoning.
- Density bonus applications should be accompanied by a site plan that shows how additional building sites and accesses will be designed to minimize the impact on the surrounding neighbourhood.
- The City will consider density bonus applications that provide the following eligible community amenities:

1. Dedication of sensitive ecosystems to a public body or private conservation organization;
2. Restoration of degraded habitat and ESAs;
3. Registration of conservation covenants on significant ESAs;
4. Land for affordable housing provided to a public body or non-profit housing provider.
5. etc.

(Note: the amenities within this list are in order of priority)

- The City will consider applications that would provide either a maintenance annuity or funds in trust for the purchase or development of all or part of an eligible community amenity.
- The dollar value of the community amenity provided should usually approximate 60% of the net appraised value that accrues to the property owner due to the increased density.
- Applications to exchange higher density levels for community amenities are to be made as a rezoning application.
- Detailed specifications of the community amenity to be provided are to be included in the rezoning application.
- Where a community amenity is to be provided to a third party for operation and maintenance, the application should be accompanied by a written agreement from that party to accept and maintain the amenity for the intended use. Restrictive covenants will be required to ensure the amenity is used as intended. Parties chosen to hold an amenity should be public bodies or well-established non-profit groups with a mandate consistent with the amenity provided.
- Applications should be accompanied by an appraisal that shows the net increase in value expected to accrue to the property owner as a result of the increased density level being requested.
- When a community amenity is provided in exchange for extra density, the amenity must be provided or legally guaranteed before or at the time of development of the extra density.

Objective 3.5 Develop waterfront parks

Equally important to providing public access to ocean, lakes and creek is to ensure that the ecological integrity of these waterbodies is quantified and preserved before allowing access. Limiting human access is a key biodiversity strategy.

Other Comments

The lack of attention to policies pointing to the preservation of ecological function of watercourses and waterbodies is striking in the Draft OCP (and not dealt with any further in Chapter 11 – Our Infrastructure). Water quality and the hydrological cycle are macro ecosystem elements that usually warrant a section of OCP policies.

There is also a lack of attention to certain significant ecosystem elements, for example the Langford and Glen Lakes systems. Special ecological features often deserve

particular attention, if not at the level of a management plan, then tailored guidelines in an EDPA designation.

10.0 Our Food System

I commend this focus on building the local food system and supporting the existing farmland in the City. This land is also key component of the West Shore Greenbelt, and a policy in this chapter should discuss working with the farming community to integrate farming and biodiversity values.

11.0 Our Community Infrastructure

I commend the focus on green infrastructure and the use of that term from a variety of perspectives (energy, water, waste). The Draft OCP would be strengthened by expanding on Objective 11.8. Quantifying and making reference to the natural (not just engineered) green infrastructure and the role it plays in the City's infrastructure inventory will greatly integrate ecological function into the fabric of the existing City of Langford.

13.0 Interjurisdictional Cooperation

The regional context statement is somewhat unsettling where conflicts between the Regional Growth Strategy (RGS) and OCP are "acknowledged." What is acknowledged is that there will be development in natural areas without any concurring commitment to maintaining biodiversity or ecological function. Section 866(1)(b) of the Local Government Act require a municipality to state, in the regional context statement, how the OCP will be made consistent with the RGS over time. "Reducing the inconsistency" is not equivalent to "making consistent with."

It is important to note that the City of Langford is recognized in the RGS as both a growth centre, and also as an integral part of the green infrastructure for the region, containing lands in the agricultural land reserve and also significant ecological features such as Langford Lake. Langford has important roles in both urbanizing and protecting sensitive ecosystems.

On a more positive note, with the City's support of lands in the agricultural land reserve for their green infrastructure and food security values, the City can take the opportunity to amend its regional urban containment and servicing policy area boundary (urban growth boundary) to exclude these lands from urban-type development [please note that each municipality in the CRD established its own urban containment boundary based on OCP maps at the time the regional board adopted the RGS]. Removing some lands from urban designations allows Langford to more fully realize its greenways connectivity and ecosystem protection goals.

This connection between the RGS and Draft OCP is particularly relevant when presenting the Growth Management and Land Use Strategy on page 16, which better supports the RGS than the existing Langford OCP.

Part VI.1: Development Permit Guidelines: Environmentally Sensitive Areas

Objectives

An integral part of sensitive ecosystems are species at risk. If the wording of the objectives intends to provide some measure of protection or consideration for species at risk as part of sensitive ecosystems, it should state this explicitly.

Definition of ESAs

The trend for local governments in B.C. is to define ESAs and provide EDPA guidelines based on the provincial government's approved sensitive and other ecosystem map codes and descriptions (found in Appendix D, *Standard for Mapping Ecosystems At Risk In British Columbia: An Approach to Mapping Ecosystems at Risk and Other Sensitive Ecosystems* (Prepared by the Ministry of Environment, Ecosystems Branch for the Resource Information Standards Committee, December 2006 at http://filmbwww.gov.B.C.ca/risc/pubs/teecolo/habitat/assets/standards_for_mapping_ea_version1.pdf).

This standard provides a province-wide definition for different ESAs, and allows local governments to tailor environmental DPA (EDPA) guidelines to the specific needs of each particular ecosystem-type. I recommend individual subsections on riparian or watercourse protection, wetlands, woodland, mature forest, Garry oak and associated ecosystems (woodlands, meadows/grasslands, transitional forests, vernal (seasonal) pools and seeps, rocky outcrops), and other ecosystem types unique to this region (and note that Map 3 on p.38 lists several ecosystem types as sensitive ecosystems and stream habitat areas). There can also be general guidelines that apply to all ecosystem types to deal with water and water quality, air and air quality, species at risk, and agriculture and ESAs. The specific subsections should list the sensitive ecosystem subclasses and their description covered, e.g. "grasslands" would cover Grasslands, Grasslands:disturbed, Grasslands:gentle slope, Grasslands:grasslands, Grasslands:shrublands, Grasslands:steep slope shallow soils, Grasslands:steep slope deep soils. I recommend that the City revise these draft EDPA guidelines to reflect the province's standard, and create more fine-grained EDPA guidelines.

In the case of the West Shore, I also recommend including greenway corridor guidelines that specifically attend to promoting connectivity of ecosystems to keep core areas free from development by, for example, clustering development.

Recital of Prohibitions

It is helpful to restate what cannot occur before a development permit is obtained (this is done somewhat in the guidelines for riparian areas, but not for other ecosystem types):

- land within the area must not be subdivided
- construction of, addition to or alteration of a building or other structure must not be started
- land must not be altered.

Development Permit Area Guidelines

General

The direction these general guidelines give to applicants is vague.

I provide more strongly worded guidelines below as an example that assists applicants to craft a project proposal that will meet the standards established by the community:

The overarching purpose of designating EDPAs is to preserve sensitive ecosystems and ecosystem function. The best way to do this is to discourage applications for development within EDPAs:

- Development within an EDPA will generally only be considered where historical subdivision or construction of structures has occurred prior to the designation of EDPAs and:
 - a) an EDPA takes up so much of a pre-existing lot that it makes the lot undevelopable for the use permitted under its existing zoning; or
 - b) due to topographic, natural hazard or other environmental constraints on the lot, there is no acceptable building site outside the EDPA; and
 - c) all opportunities to relax other development requirements (such as yard setbacks, minimum lot size, parking, etc.) have been exhausted.
- The onus lies with the applicant to demonstrate that encroaching into an EDPA is necessary due to the above circumstances, in order to allow the use of a site as otherwise permitted under existing zoning.
- To determine whether a proposed development is inside an EDPA, two things need to be done:
 - a) Locate the EDPA boundaries on the ground: On any given site, this means:
 - i) locating the watercourse or sensitive ecosystem relative to the property lines;
 - ii) locating the top of bank (for creeks, streams and rivers) or natural boundary (for wetlands, ponds, lakes, and terrestrial sensitive ecosystems); and
 - iii) measuring the applicable leave strip or distance from that top of bank or natural boundary.Unless all development activities will be clearly outside the EDPA, these determinations generally need to be made by a B.C. Land Surveyor (BCLS). However, they can be incorporated into the BCLS-certified site plan that is a standard requirement of any development proposal.
 - b) Locate the proposed development relative to the EDPA boundaries: This means locating where proposed structures will be built and where soil or vegetation will be disturbed for yards, driveways, patios, walkways, etc. relative to the EDPA boundaries.
- In considering how much encroachment into an EDPA should be allowed, the City will weigh the applicant's need to encroach upon the watercourse leave strip or sensitive ecosystem against the potential impacts of the encroachment on the habitat.

- The applicant and City will seek to vary other land use requirements under the Zoning Bylaw before or, where necessary, along with encroaching into the leave strip in order to minimize the encroachment. One or more of the following variances from existing Zoning Bylaw requirements may be applied:
 - i) front and/or rear yard setback reductions.
 - ii) site coverage increased by up to 50% of maximum.
 - iii) maximum height increased by up to 3 metres.
 - iv) parking requirement reductions.
- Where the EDPA occupies more than 50% of a lot, the EDPA area may be reduced to occupy the equivalent of 50% of the lot. Variances of other Zoning Bylaw requirements indicated in the above guideline may also be applied.

Riparian

I commend guideline R4 that seeks to exceed the Riparian Areas Regulation.

In guidelines R5, where Qualified Environmental Professionals (QEP) are assessing riparian areas, it is important that they assess the site with a view to maintaining all ecological features, not just fisheries values. I recommend you require QEPs to assess for other ecological values, or, as some local governments are doing, reviewing the QEP reports to ensure they have adequately taken into account all ecological values.

Security

I recommend that the City require applicants to post security where mitigation and replanting is required, or where potential disturbance to ESAs may occur to ensure that the restoration work is completed as required:

- The City may require the applicant to submit to the City a cost estimate, prepared by a qualified professional, of the total cost of restoring/revegetating (with appropriate native plants) the sensitive ecosystem after the development is completed.
- The applicant will provide adequate financial security, as determined by the City, prior to beginning the construction of any building or disturbance of a site located within an EDPA.
- The value of the financial security should be equal to the amount required to pay for:
 - the cost of repairing damage caused by construction or site disturbance;
 - the cost of restoring/revegetating the sensitive ecosystem that has been disturbed by the development and/or construction, as determined by the [Local Government], in the event that the sensitive ecosystem is damaged as a consequence of a contravention of a condition contained in the development permit;
 - the cost of restoring fish habitat that is damaged during the development process.

Sensitive Ecosystems and Potential Habitat and Biodiversity

The guidelines for sensitive ecosystems and potential habitat and biodiversity are weak. They do not set strong standards or direction for staff for ecosystem protection.

The potential habitat and biodiversity map and guidelines are the most important EDPA designation. It is through this designation that actual quality of biodiversity and implementation of greenways can be achieved. The guidelines in this area are deficient for meeting these objectives.

I recommend the following guidelines in this section:

Guidelines –

- Manage rainwater in accordance with the most recent integrated watershed management or rainwater policy and design manual from the B.C. Ministry of Environment. This includes managing rainwater on site and maintaining pre-development drainage flows.
- Should damage occur to an environmentally sensitive area during development, the City may require a professional assessment of the damage and a report on recommendations for rehabilitation.

Aquatic Ecosystems

- Encroachment into the EDPA by all development activities will not exceed that indicated in the site plan approved in the development permit. All development activities will avoid or minimize disturbance in the EDPA beyond the building footprint. This may mean adjusting conventional practices with respect to locating machinery and stockpiles relative to excavations, use of hand labour as opposed to machinery, etc.
- Prior to any development activity, boundaries of the EDPA and the extent of encroachment allowed by the development permit will be clearly marked with bright orange or other highly visible temporary fence with a minimum height of 1.2 m (3.94 ft) and supported by poles a maximum distance from one another of 2.5 m (8.2 ft). This fence will remain in place throughout clearing, site preparation, construction, or any other form of disturbance.
- The applicant must provide an erosion and sediment control plan that reflects measures prescribed in the “Land Development Guidelines for the Protection of Aquatic Habitat” (1992: note Section 3), “Stream Stewardship: a Guide for Planners and Developers” (1994: note pages 30-34), or other standards or guidelines adopted or approved by the City. This plan will form part of the development permit.
- Sediment containment and erosion control measures will be installed prior to development activity.
- Existing trees and vegetation within the EDPA will not be disturbed except where allowed under this development permit.
- To ensure their long-term health, all existing trees that are retained will be clearly marked prior to development, and temporary fencing installed at the drip line to protect them during clearing, grading and other development activities.
- Where existing trees and vegetation are retained, the following are allowed:

- a) pruning or removing of hazardous trees (as determined by the City arborist, or a qualified arborist), but leaving wildlife trees and snags (dead, upright trees or stumps) if safe.
- b) pruning of undergrowth within 1 meter of existing or proposed public trails to avoid injury to users, but no disturbance of vegetation within 3 meters of the natural boundary of the watercourse.
- c) supplementing existing vegetation with planted stock as needed to landscape bare or thin areas, following specifications of the following Guideline.
- Replanting of disturbed areas or supplementing existing vegetation with planted stock in thin or bare areas of a leave strip will be required in accordance with the following:
 - a) replanting will use trees, shrubs and ground cover native to the area and selected to suit soil, light and groundwater conditions of the site, and to promote habitat or erosion control functions as necessary.
 - b) for wooded areas, clearing should not exceed 10% of the EDPA, should be confined to the outer portions of the EDPA, and must not be on slopes greater than 50% (27°). The same replacement ratio, average tree density and site features as in the previous Guideline apply.
 - c) a shrub layer will be provided for a minimum of 33% of the restoration area; shrubs will be planted at an average density of 1.0 meters apart and a minimum #2 pot size at time of planting.
 - d) groundcover may be substituted for shrubs; if used, groundcover will consist of brush layers or planted groundcover species at a maximum average spacing of 0.5 meters with plants of minimum 10 cm pot size at time of planting.
 - e) areas not covered by trees, shrubs or groundcover will be seeded with native herbaceous plants, grasses, or legumes.
 - f) all vegetation will be protected from intrusion by motor vehicles with a curb or other suitable protective barrier if roads, driveways or parking areas abut the leave strip.
 - g) all planted stock will be maintained for a minimum of two years; within that time, any unsuccessful stock will be replaced at the owner's expense.
- To replace portions of the leave strip that are permanently removed, remaining portions may be enhanced by supplementing existing vegetation, re-vegetating bare or thin areas, or by adding to (widening) the leave strip in other portions of the site not affected by the development.
- (Primarily for larger developments) EDPA or habitat enhancement in another portion of the same watercourse that is in need of restoration may be considered as compensation for habitat that is permanently displaced on a given site, but only as a last resort when options to avoid, mitigate, restore or enhance on-site habitats are exhausted.
- EDPA boundaries will be indicated on the property, and information will be provided to purchasers of the property on the importance of watercourse leave strips and activities that are not permitted within a leave strip without a development permit.
- Fencing to restrict access of livestock to watercourses will be installed where needed.

Terrestrial Ecosystems

- Prior to any alteration of land within the DPA, non-disturbance areas must be identified and fenced with a continuous barrier not less than 1.5 metres in height sufficient to protect the non-disturbance area from construction and development activity.
- Within a non-disturbance area, trees and vegetation must not be cut, pruned, altered, removed or damaged in any way other than minor damage incidental to the construction of the barrier under paragraph [] above.
- Within the EDPA, development must not either increase or decrease the amount of surface and /or groundwater or affect the quality of water available:
 - within the non-disturbance area; or
 - within the buffer area, other than development expressly permitted within the buffer area within the development permit.
- Within a non-disturbance area, gravel, sand, soils and peat must not be removed; and soil, other fill, or building materials must not be deposited.
- Within a non-disturbance area, vegetation that is not indigenous to the City must not be planted.
- Within a buffer area, the alteration of land or the construction of structures approved through a development permit will be limited to those that are compatible with the characteristics of the non-aquatic ESA:
 - insulate the ecosystem from uses that would cause adverse effects;
 - avoid disturbance and removal of native vegetation by people;
 - emphasize native vegetation species compatible with the ecosystem;
 - deter grazing by livestock in sensitive ecosystem areas.
- Within a buffer area, upon development approval, hard surfacing such as driveways and parking areas and soil deposits must be limited in order to be compatible with the characteristics of the non-disturbance area.
- Settlement, construction, land disturbance, and other development within or directly adjacent to sensitive terrestrial ecosystems will be discouraged.
- Conserve snags and standing dead trees where safe to do so.
- Locate settlements, drives, construction and other development away from existing large, old trees and snags. Artificial snags can be located in safe areas to help improve habitat.
- Conserve trees in communities (groups of trees along with their associated understory) rather than isolating individual specimens.
- Replanting of disturbed areas or supplementing existing vegetation with planted stock in thin or bare areas will be required in accordance with the following:
 - replanting will use trees, shrubs and ground cover native to the area and selected to suit soil, light and groundwater conditions of the site, and to promote habitat or erosion control functions as necessary;
 - for wooded areas, clearing should not exceed 10% of the EDPA, should be confined to the outer portions of the EDPA, and must not be on slopes greater than 50% (27°);

- a shrub layer will be provided for a minimum of 33% of the restoration area; shrubs will be planted at an average density of 1.0 meters apart and a minimum #2 pot size at time of planting;
- groundcover may be substituted for shrubs; if used, groundcover will consist of brush layers or planted groundcover species at a maximum average spacing of 0.5 meters with plants of minimum 10 cm pot size at time of planting;
- areas not covered by trees, shrubs or groundcover will be seeded with native herbaceous plants, grasses, or legumes;
- all vegetation will be protected from intrusion by motor vehicles with a curb or other suitable protective barrier if roads, driveways or parking areas abut the leave strip;
- all planted stock will be maintained for a minimum of two years; within that time, any unsuccessful stock will be replaced at the owner's expense.
- To replace portions of the leave strip that are permanently removed, remaining portions may be enhanced by supplementing existing vegetation, re-vegetating bare or thin areas, or by adding to (widening) the leave strip in other portions of the site not affected by the development.

Garry Oak and Associated Ecosystems

- Obtain inventory of Garry oak and associated ecosystem elements (woodlands, meadows/grasslands, transitional forests, vernal (seasonal) pools and seeps, rocky outcrops) on the site
- Cluster development away from Garry oak and associated ecosystems
- Encourage applicants to dedicate Garry oak and associated ecosystems as parkland
- Design the lot layout to avoid roads, trails and utility corridors that bisect natural areas.
- Retain natural buffers around Garry oak and associated ecosystems.
- Protect nesting and denning sites that were identified on site through an initial reconnaissance or in the ecological inventory.
- Manage access to minimize vehicular access.
- Protect large old trees (and their root systems) and snags.
- Remove encroaching trees.
- Minimize soil disturbance.
- Restore degraded ecosystems and remove invasive species.
- Encourage the maintenance of natural sites and the planting of gardens with native, dry land species.
- Design linear corridors such as roads, driveways, or trails to be as narrow as possible, create as little disturbance as possible and configure them to allow for wildlife crossings.
- When choosing trees to thin or remove, maintain the high canopy layer of the forest and its filtered sunlight affect.
- Before issuing a development permit the City will require the applicant to fence off all Garry oaks ecosystems that will be preserved.

- The development permit conditions will contain an acceptable erosion and sediment control plan that includes construction practices to control the use and disposal of liquid construction waste (such as concrete wash) and to ensure that surface and sub-surface water flows are unchanged.

Implementation

I applaud the City for designating its EDPAs as Development Approval Information Areas. This gives the City considerable flexibility to request information about development that will assist in decision making processes.

To better understand how municipal decisions and changes in land use are actually meeting the goals of the OCP, many communities are adopting monitoring programs. The programs choose a few key or many indicators that will assist the local government and community to evaluate whether land use and other decisions are meeting their long term goals as set out in community plans. The changes in the indicators over time show whether the community is succeeding in meeting its vision and, if not, provide valuable information on what conditions may be affecting this outcome.

I recommend including the following environmental indicators (and comparable indicators may be developed for each substantive chapter of the Draft OCP):

- Species at risk occurrence protected or lost
- Hectares of representative sensitive ecosystem such Garry oak and associated ecosystems protected, remaining unprotected, and lost
- Amount of land included in or taken out of the Agricultural Land Reserve
- Number of trips taken on foot, by bicycle, or by other non-motorized means
- Kilometres of trails, bicycle paths, sidewalks and roads per capita
- Water quality at specific sites in Langford Lake, Glen Lake, and a variety of creeks and wetlands (fecal coliform, phosphorus, and turbidity)
- Total imperviousness in specific watersheds or areas
- Kilometres of healthy riparian ecosystems
- Percentage of residents within 500 metres of a neighbourhood shopping
- Average density of residential, commercial and industrial uses in the City; average density by neighbourhood
- Decrease/increase in per capita solid waste disposal rate

Indicators are most useful when they have a clear benchmark towards which they are working. These can be set out in an OCP or in documents created as part of the monitoring program. For the indicators under an environment category it is important to link changes in the indicator to a scientific measure (e.g. for water quality) or based on per capita targets. For example, many local governments set a target of 120 square metres of green space per capita so that as the population grows so must the amount of parkland. Likewise, if the City adopts a water demand management program it may set a target, e.g. 25 percent, for a decrease in per capital water consumption over the next decade as a measure of the success of its program.

The programs can be inexpensive to run when undertaken in partnership with community organizations. For example, some of the monitoring for these indicators can be undertaken by community organizations.

Conclusion

I have provided comments in this memo on Draft #2 of the Langford Official Community Plan from the perspective of protecting sensitive ecosystem, including Garry oak and associated ecosystems and species at risk. My focus has been primarily on recognizing and valuing the natural green infrastructure before allowing development to occur that does not compromise the ecological systems upon which the City is built.

If you have any questions about these suggestions, please do not hesitate to contact me.

Dianna Plouffe

From: Larry Parkinson [Larry.Parkinson@corix.com]
Sent: Wednesday, April 23, 2008 7:50 AM
To: Dianna Plouffe
Subject: RE: Referral - OCP-06-04
Attachments: OCP-06-04 Langford OCP.pdf

West Shore Environmental Services has no objection to OCP-06-04.

Regards,
Larry Parkinson, CTech
Coordinator, Engineering & Construction
West Shore Environmental Services
103 - 859 Orono Avenue, Victoria, BC, V9B 2T9
P.250-478-2187 F.250-478-2163 M. 250-213-2949
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-----Original Message-----

From: Dianna Plouffe [mailto:dplouffe@cityoflangford.ca]
Sent: Tuesday, April 22, 2008 2:47 PM
To: Dianna Plouffe
Cc: james_wadsworth@bcstransit.com; Dan Reynolds; Lorne Fletcher; michael.riefman@viha.ca; planning@city.colwood.bc.ca; cbaker@crd.bc.ca; tcorbett@crd.bc.ca; darthur@crd.bc.ca; christine.zotzman@gov.bc.ca; terry.chow@gov.bc.ca; lbeckett@district.highlands.bc.ca; District planning staff; Eric.Partridge@gov.bc.ca; Al Fontes; Brent Molnar; George Henshall; John Manson; Raphiel Mattson; Sharon Johnston; Suzette Chapman; Susan Lai; Renee Seginowich; Victor Chen; Steve Ternent; Bob Beckett; Jason Parks; heritage@gov.bc.ca; Gordon.Bednard@gems2.gov.bc.ca; marlene.caskey@gov.bc.ca; beth.chipperfield@rcmp-grc.gc.ca; gwhiteley@sd62.bc.ca; landsmanager@shawbiz.ca; info@town.viewroyal.bc.ca; Larry Parkinson
Subject: Referral - OCP-06-04

Hi Everyone,
Please find the documents attached for your review and response.
Thanks,

Dianna Plouffe
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Please consider the environment before printing this email

Emilie Adin

From: Laura Beckett [lbeckett@highlands.ca]
Sent: April-29-08 8:52 AM
To: Emilie Adin
Cc: Chris Coates
Subject: Highlands Referral - Langford OCP

Hi Emilie,

Thanks for our conversation yesterday. Unfortunately, I will not be able to provide you with a Highlands' Council resolution regarding Langford's proposed OCP update in time for its public hearing May 5. Council will consider Langford's referral at their Council meeting, which occurs the same night.

However, I would like to supply you with a previous Highlands' Council resolution, from their June 4, 2007 meeting. It was in response to a Langford OCP/Zoning amendment for the Bear Mountain area, specifically the Langford area that is adjacent to Highlands. (I think the public hearing for those bylaws occurred on June 4, 2007.) As you indicated, the proposed land use designation in this OCP update is not different from Langford's current OCP, so I would expect that these comments would still be applicable.

I am supplying it to you as staff to staff information, for you to use as you see fit to inform Langford's OCP process, in time before the May 5 public hearing.

THAT Council advise the City of Langford that obtaining a buffer through a restrictive covenant on the subject property, to the satisfaction of The Land Conservancy, adjacent at all points of contact between the conservation land and the subject property would be appreciated for the long term protection of the conservation land.

FURTHER THAT the City of Langford be requested to provide the District adequate time for responses to be formulated.

THAT the City of Langford be requested to minimize view scape impacts on the subject property with height restrictions

Sincerely,
Laura

Laura Beckett, M.U.R.P., MCIP
Planner
District of Highlands
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Tel: (250) 474-1773
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****Please note my new email address: LBeckett@highlands.ca**

Lindy Kaercher

From: Langford Coalition [langfordcoalition@gmail.com]
sent: May 1, 2008 12:32 AM
To: Lindy Kaercher
Cc: islandnews@achannel.ca; news@theq.fm; Newsroom@globeandmail.ca; charlie@straight.com; nwnews@cknw.com; radionews@vancouver.cbc.ca; globalbc.newstips@globaltv.ca; bcnews@ctv.ca; sunnewstips@png.canwest.com; tabtips@png.canwest.com; nvanderklippe@nationalpost.com; localnews@tc.canwest.com; editorial@publiceyeonline.com; lisa_cordasco@cbc.ca; philippe_murat@radio-canada.ca; cfaxnews@cfax1070.com; jeff_davies@cbc.ca; Chris Cook; eternity@islandnet.com; Radio Ecoshock; rlowrie@chtv.ca; bcassign@ctv.ca; chrisbr@citytv.com; victoria@cbc.ca; Dave.Biro@chumtv.com; editorial@lookoutnewspaper.com; dnews@island.net; redevye@coopradio.org; Andrew MacLeod; edit@martlet.ca; editor@saanichnews.com; editor@goldstreamgazette.com; editor@oakbaynews.com; editor@sookenewsmirror.com; editor@vicnews.com; forbesandmarshall@ocean985.com; jbbeacon@shaw.ca; robert.linden@news1130.rogers.com; editor@atlanticfreepress.com; cfuvpsa@uvic.ca; director@uvic.ca; Jason Youmans; Richard Habgood; Yvonne Macnab
Subject: Citizens Group Calls on Langford Council to Delay May 5th Public Hearing of New Official Community Plan

Citizens Group Calls on Langford Council to Delay May 5th Public Hearing of New Official Community Plan

Langford Coalition argues that 10 working days is not long enough to consider newly-finalised documents.

May 1st, 2008
Langford

The Langford Coalition, a group of concerned Langford citizens formed to advocate for good public policy and greater accountability in Langford, B.C., want residents to have sufficient time to access, read and understand the extensive documents of Langford's new Official Community Plan (OCP). Coalition members feel people need more time to be able to compare Langford's proposition with what exists today, to understand what 'mixed use' is, to study the newly-presented colour maps, and time to understand the different wording used. Many terms, such as 'Open Space', are different than the corresponding terminology in the current OCP, or have no clear equivalent.

"I am concerned whenever I am finding out new information at the eleventh hour," said Langford resident Kristina Vanlierop. The Official Community Plan has gone through numerous revisions since originally being made public, and the final draft of the OCP was just posted to Langford's website April 21st; the public hearing is a mere 10 business days later, on May 5th.

Langford's website refers to the "Big moves of everything mixed use" in the Official Community Plan. These "big moves" and the broadbased, city-wide nature of encouraging mixed use in every neighbourhood are heightening concerns about the breakneck speed at which the plan is progressing to Public Hearing on Monday, May 5th, 2008.

Area resident Bea McKenzie, as well as many others, would like to see the public hearing put off to allow for neighbourhood study sessions to occur prior to the hearing, so citizens can first know exactly how their neighbourhood will be affected. "Although City Hall has involved members of the community in many ways to produce this document, we have not had the proper opportunity to get together for a review of the final product, the final draft."

Other questions involve the lack of public input the new process brings with it to future development proposals. While the OCP encourages certain types of mixed use in some neighbourhoods, it appears to greenlight every kind of mixed-use everywhere in all of Langford. This will mean the new Official Community Plan will eliminate the need for OCP amendments and, therefore, much of the notification and public comment requirement that at present inform residents and give them opportunity to speak to a proposed development in their neighbourhood, dramatically reducing the opportunities for public input.

" 'Mixed use everything' is a huge issue which brings with it an unparalleled increase in density in both new and established neighbourhoods," says Vanlierop. "I believe many people will be in for a surprise about how this could affect them personally. For example, in 'Neighbourhood mixed use areas' the density objective is for 16 units per acre. Designating everything 'mixed use' is like pressing the EASY button; avoiding further public input in the future and giving politicians and residents both less time to consider development proposals."

Still others wonder about how the inevitable complaints and flashpoints will be dealt with as residents attempt to adjust to the inevitable changes to the make-up of their neighbourhoods. "Recently we've seen conflicts over a drywall business in a residential area and a concrete business in a commercial area," noted Langford resident Steven Hurdle. "Under the current rules City Hall has been finding itself unable to resolve these mixed-use conflicts. One such conflict, after going unresolved by City Hall for a reported four year span, recently took a dramatic turn when a local resident successfully petitioned for the BC Assessment Authority's review board to reduce his entire neighbourhood's property assessments due to the activities of a home-based drywall business hurting the property values of local residents. City Hall has not proposed a new dispute resolution mechanism for the current, and still unresolved, conflicts, let alone the likely increase in them as mixed-use is encouraged Langford-wide. We need to assess where we are and how our current mixed-use can be done better before we open the floodgates to more of it."

City Hall has indicated that they plan to have the OCP passed later this month. Langford Coalition members request that Langford Council give more time to residents to question and decide whether to accept the dramatic changes, proposed in the new Official Community Plan. Langford Councillor Lillian Spzak was quoted in the media recently as stating (regarding the direction of the new OCP) that "It's hard for people to deal with change," so surely more time for residents to become familiar with the final version of the OCP would be prudent in alleviating the stress Councillor Spzak is concerned about residents suffering under this new document. Noting that the proposed timeframe leaves only four more days to consider the final version of a document meant to guide the growth of our community for decades to come, the Langford Coalition encourages Langford Council to give this issue the importance it deserves and allow a lengthier review of the just-finalised documents.

For more information, contact:

The Langford Coalition
250-884-0575
langfordcoalition@gmail.com

-30-



TOWN OF VIEW ROYAL

45 View Royal Avenue, Victoria, B.C., Canada V9B 1A6

Tel: (250) 479-6800 • Fax: (250) 727-9551

e-mail: info@town.viewroyal.bc.ca

April 25, 2008

City of Langford
2nd Floor, 877 Goldstream Avenue
Victoria, BC
V9B 2X8

Attention: Emilie Adin, Deputy Planner

Dear Ms. Adin:

Re: Draft Langford Official Community Plan

Thank you for the opportunity to provide comments on your draft Official Community Plan. Staff and Council have reviewed the document and have the following observations:

- There are a number of references to View Royal as part of the greater Westshore community. We note that we consider the Town of View Royal as a Core Municipality and we look forward to working with all of our neighbours and the CRD on topics of mutual interest such as integrated transportation planning and economic development.

We look forward to the successful conclusion of your Official Community Plan process and commend you on your forward thinking sustainability and housing policies.

If you have any questions, please contact the undersigned.

Yours truly,

Lindsay Chase, MCIP
Director of Development Services





DISTRICT OF METCHOSIN

May 2, 2008

File No. 6420-20-01

Langford City Hall
2nd Floor, 877 Goldstream Avenue
Langford, British Columbia, Canada
V9B 2X8

Attention: Planning Department

Re: Comment on Langford's Draft Official Community Plan

Thank you for the referral of your draft OCP and the opportunity for the District to comment.

While we appreciate the need and value of concentrating housing and urban densities within the region's Urban Containment Boundary, which encompasses the District of Langford, we request that the City of Langford respect the boundary it shares with the District of Metchosin, and recognize the rural lifestyle that is the most basic and fundamental value of our community. We reiterate that the Regional Growth Strategy requires that member municipalities "negotiate" agreements regarding buffering and land use transition where the Regional Urban Containment and Servicing boundary coincides with a municipal jurisdictional boundary." Once again we implore the City to consider land use transitions that respect the rural nature of its neighbour, the District of Metchosin.

Accordingly, we are concerned about the change in status of lands adjacent to Metchosin from "rural" with a requirement of 12-hectare parcels, as well as some of the "large-lot residential" to "neighbourhood" and "hillside and shoreline" which may allow a range of clustered medium and even high density uses. As a general rule, and given the laudable sustainable aims in the draft OCP, it seems inappropriate at best to locate high density uses at the very edge of the urban containment boundary, in locations far from the region's core services. The District emphasizes the need for edge planning to address not only the transition between higher densities and Metchosin's rural uses, but to recognize the cross-border nature of the watersheds, ecosystems, and the shared responsibility to maintain the area's critical ecological functions.

While we were pleased to see the recognition that much of the Metchosin/Langford border was designated as "Areas Suitable for Wildlife Habitat" (Map 3) and "Areas With Potential Habitat and Biodiversity Values" (Map 15), we were extremely concerned that Map 3: Sensitive Ecosystems & Habitat Areas does not recognize the existence of Bilston Creek as it crosses the Langford/Metchosin border alongside Happy Valley Road, as well

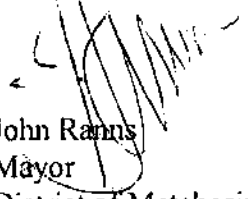
RECEIVED
Peter Tustev

as Veitch creek alongside Sooke Road. Both these creeks are easily identified on the CRD Natural Areas Atlas, and are both depicted on the current OCP. Notably, Bilston Creek is encompassed in Map 13, Riparian Areas, but Veitch creek alongside Sooke Road is not.

The District commends the City for the inclusion of policies intended to support agricultural activities in Section 11, including support for community agriculture activities, for commercial agriculture as a viable business venture, for local food production, and for farmers' markets and region-wide initiatives that support locally grown food. Finally, we wholeheartedly support policy 11.3.1 which requires an edge planning policy for properties adjacent to ALR properties as a means to ensure new development will not jeopardize the long term use of the land for agricultural purposes. We would encourage similar principles apply to rural lands not designated in the ALR but where agricultural activity occurs.

We thank you for providing an opportunity for the District to comment, and thank you in advance for addressing the issues discussed herein.

Sincerely,



John Rams
Mayor
District of Metchosin



Making a difference...together

Regional Parks

2000 Hwy 10, Victoria

BC V8W 2E5

Phone: 250-363-7200

Fax: 250-363-7201

Website: www.crd.bc.ca

May 2, 2008

Emillie Adin, Deputy City Planner
City of Langford
2nd Floor, 877 Goldstream Avenue
Victoria BC V9B 2X8

Dear Emillie Adin:

Re: Draft 3-7-08 City of Langford Official Community Plan 2008

Thank you for the opportunity to review and comment on the draft Official Community Plan for the City of Langford. Our Regional Parks' comments are organized by the page number to which they refer.

Page 16 – Suggest that:

- Make a distinction between provincial and regional parkland and private open space.
- Add the Galloping Goose Regional Trail as it is an important green space corridor.
- Show regional parks immediately on the Langford boundaries (Thetis Lake and Sooke Hills Wilderness regional parks) as they add to the overall feeling of green space in Langford.

Page 30 – Suggest that:

- In the Challenges and Opportunities section include information about the effect that regional parks immediately adjacent to the boundary of the City of Langford have on the setting of the city and the quality of life for residents (e.g., Sooke Hills Wilderness Regional Park provides backdrop to the entire community; Thetis Lake Regional Park provides recreational space for hiking).
- In the Challenges and Opportunities section include information about the existing regional parks in the City of Langford (e.g., Mill Hill Regional Parks has a nationally significant Garry oak ecosystem and provides great opportunities for hiking and nature study; Mount Wells Regional Park provides some of the best views of Langford and is an important green space backdrop; the Galloping Goose Regional Trail is an important green space, connects regional communities and communities within the City of Langford and is part of the Trans Canada Trail).
- Reference be made to the proposed E&N Regional Trail and that it will provide a trail for commuters and recreationalists and connect regional communities and communities within Langford.

2008 MAY 2 10:20 AM
Emillie Adin

Page 35 – Suggest that:

- Provincial and regional parks be distinguished from cemeteries and agricultural land as they are serving different purposes and this be noted in the legend.

Page 36 – Suggest that:

- The Greenway designation also be noted as Regional Trail and part of the Trans Canada Trail system.

Page 66 – Suggest that:

- The trail extending up the Saanich Peninsula as the Lochside Regional Trail be shown.

Sincerely,

A handwritten signature in black ink, appearing to read "Lloyd Rushton", with a long horizontal flourish extending to the right.

Lloyd Rushton, General Manager
Parks and Community Services

2008-05-05 10:10:10
2008-05-05 10:10:10
2008-05-05 10:10:10

May 5,2008

City of Langford

Dear Mayor and Council

I wish to be recorded as opposing the draft OCP bylaw 1200, and bylaw 1201 because my concerns are, that not enough time has been available for the residents of Langford to review these changes that will dramatically affect our existing neighborhoods in particular around the lakes and surrounding wet lands.

Grace E Kasper
Sandra Petersen

3179 Glen Lake Road
Langford BC
V9B 4B6



WEST SHORE ECONOMIC DEVELOPMENT ASSOCIATION
1110 HIGHWAY 147, SUITE 203
MIDFIELD, B.C. V9A 1V7

2008-05-07 10:00 AM

MAY 7 2008

May 7, 2008

Mayor and Council
City of Langford
2nd Floor - 877 Goldstream Avenue
Langford, British Columbia
Canada, V9B 2X8

Mayor and Council
City of Colwood
3300 Wishart Road
Victoria, B.C. V9C 1R1

Re: Official Community Plan Drafts

Dear Mayor Young, Mayor Twa and Councillors,

Please be advised that at our regular meeting of April 1st, 2008, the WEDA Board of Directors passed the following motion:

"Due to the shortage of light and medium industrially zoned areas in the OCP draft Document, WEDA strongly suggests the City of Langford and the City of Colwood councils include light and medium industrial zoned areas to provide a sustainable future".

We urge you to consider this issue as you look to the future of our community. A diverse industry base is critical to a strong local economy and will ensure jobs for a wide range of our citizens in the future.

Sincerely,

A handwritten signature in black ink, appearing to read "Mike Adamek", with a long horizontal line extending to the right.

Mike Adamek
President
West Shore Economic Development Association

1147 Lindsay Place
Victoria BC
V9B 5N9

May 12, 2008

To: Langford Mayor and City Council

Re: Draft OCP Bylaw 1200

I would like to voice my opposition to the proposed OCP, though I acknowledge the thought and intentions in your efforts to date.

My opposition to the proposed OCP is based in large part on 3 concerns:

The emphasis on sustainability is laudable and necessary, but it seems in developing a vision for our community 20 years hence, those of us who are currently residents are being asked to make many decisions based on what is best for people who are not yet here. Moreover, we are the ones to bear any inconvenience of noise, pollution and degradation of our environment in preparing the city for these "yet to arrive" newcomers. Development projects with 20 year or longer build ups sentence current residents to 20 years of noise, dust and congestion if they are unfortunate enough to be neighbours of these developments.

Objective 3.12 (page 35) states : Consider view impacts from the valley or from water bodies and from hillside developments. The supporting Policy statements 3.12.1 and 3.12.2 seem rather toothless when viewed in conjunction with the Development Permit Areas on page 118.

The ability to obtain variances results in reality (see attached photos) far different from the idyllic portrayal the photos in the OCP (p. 34, 35, 42) depict of Langford Lake, which is my third concern.

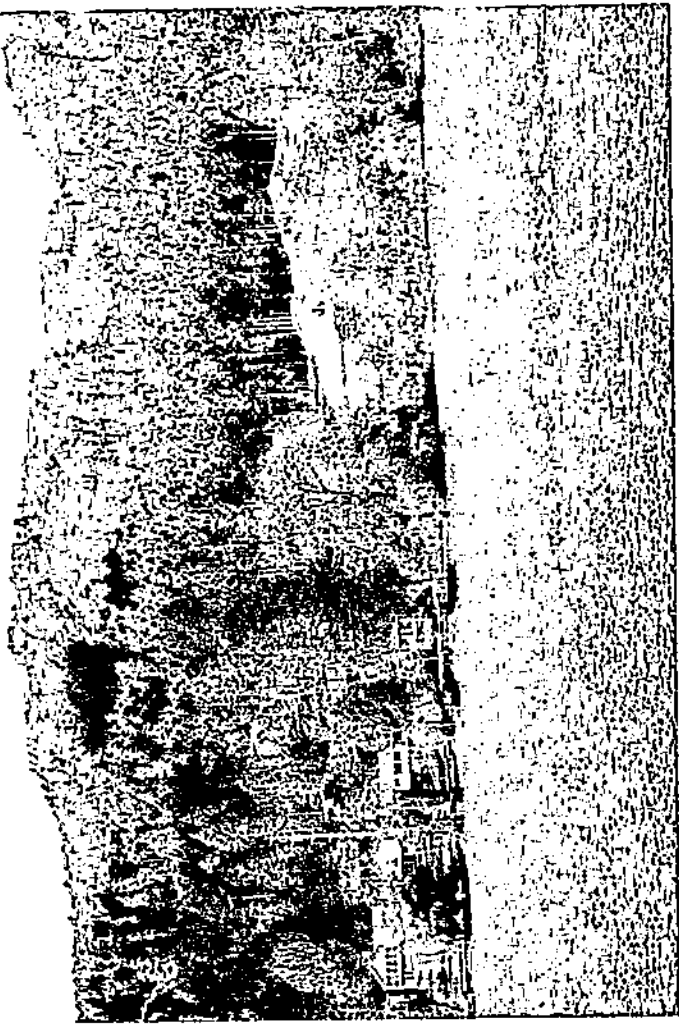
A final question for Mayor and council: how does the inclusion of a lakeside pub/restaurant on the Westhills development fit with the OCP's stated goal : "Preserve the character of Langford's lakes" ? Will the proposed OCP signal the end to this proposed land use?

Thank you for your consideration of my comments.

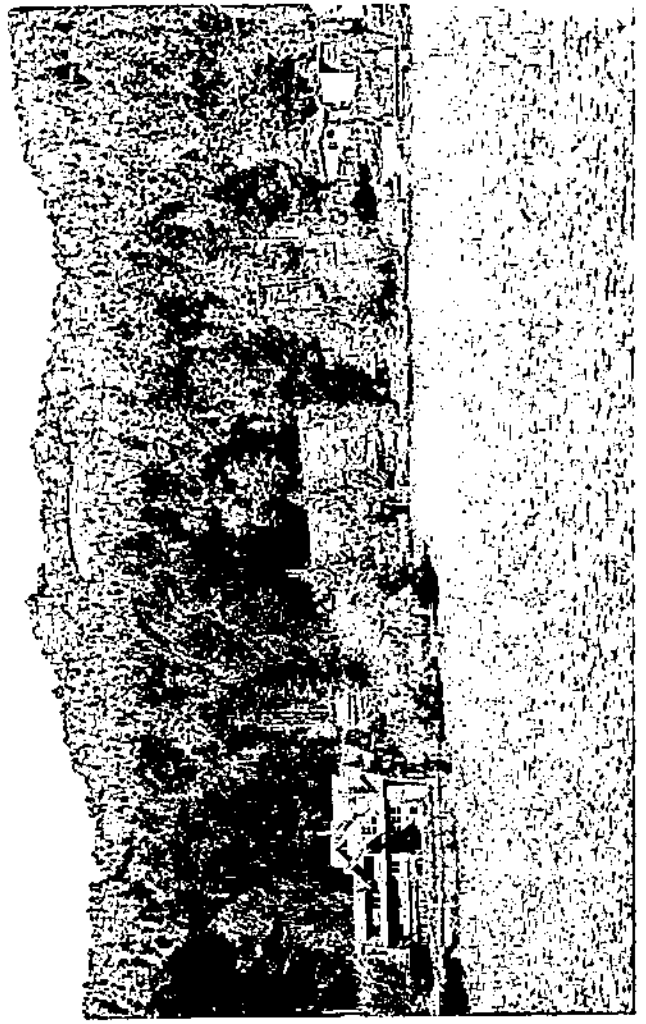
R. Partridge



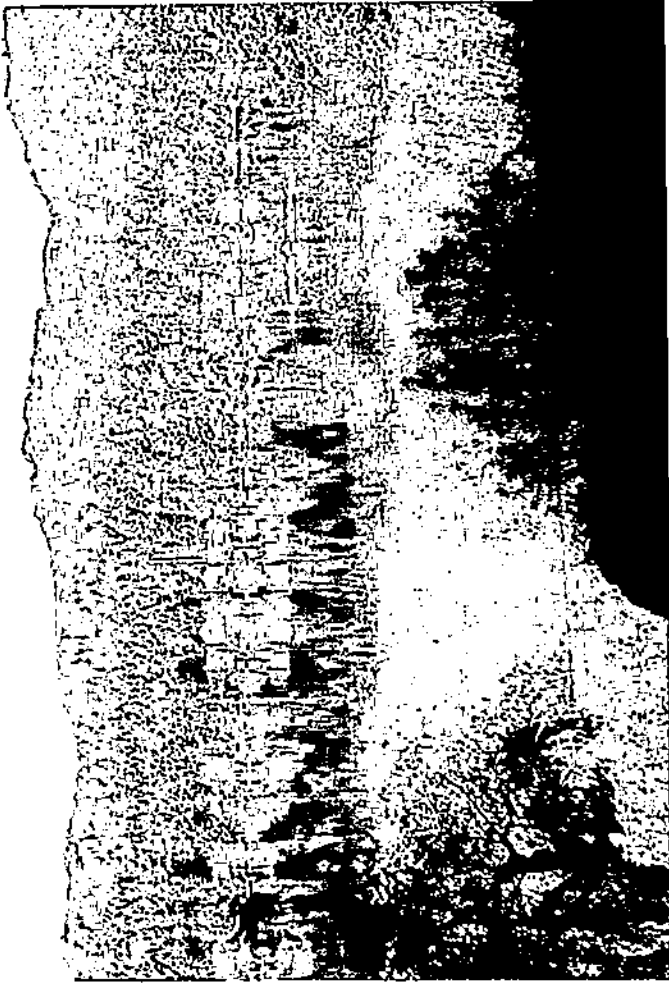
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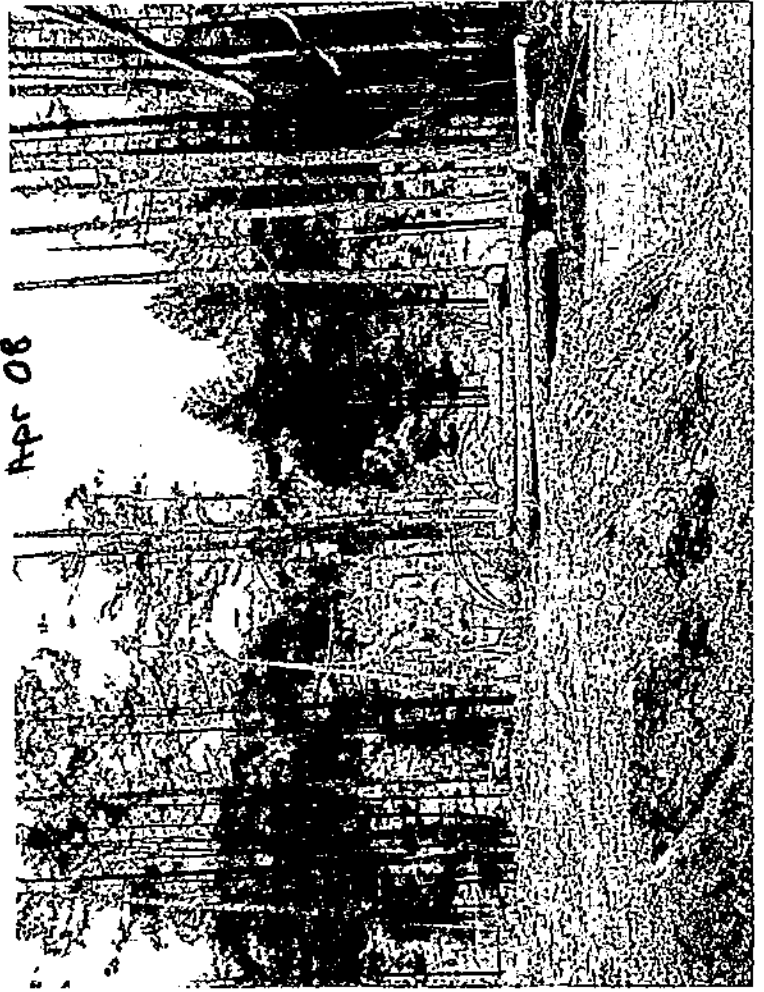
May 2008



May 2008



Apr 08



John Crook
2868 Knotty Pine Rd.
Victoria, BC
May 10, 2008

City of Langford

Mayor, Council & Staff

RE: OCP

So you're coming home one day and suddenly there's a sign on two properties a few doors down from your house. The sign says there will be a public hearing regarding rezoning these properties for condominiums. What the heck you say, you thought your neighbourhood was for houses only. Upon checking with City Hall, you find out that planning meetings had already taken place. Why weren't you told? Because you're not an immediate neighbour to the properties. It had already been decided that this was probably a good use for the land. Is your neighbourhood being changed? How is this going to affect your property value? What about the \$ 75,000 you just borrowed for the renovations you did on your house? Should you invest any more?

Under the previous OCP, expected property use in the future was laid out on a map. when you bought a house, you could look at the OCP map and have a reasonable expectation of the future of the neighbourhood. You could make a choice as to how much you were willing to pay, how much you should spent on improvements and how long you might live there.

To be fair, for a young developing community, a somewhat rigid OCP however, means the chances of the crystal ball future actually developing exactly as foreseen are probably slim. A case in point is the McCallum Rd. area. In the OCP this was originally designated as future business park. We expected that there would be demand for these properties in the near future, but the expectation was that this would not be for five to ten years. Langford took off and the "future" arrived within two years with the development of Costco. The new OCP as proposed is objective based, meaning that planning and development is set out as a series of objectives and policies to create complementary development with green space, transportation, open space and streetscapes. This will work well in all new areas and major re-developments of the core areas. But what about existing neighbourhoods?

The proposed OCP says that existing neighbourhoods should have infill development, some "neighbourhood centres" and higher densities. While some density could be achieved with secondary suites and "coach housing", some of it can be achieved with the building of multi-family housing. Therein lies the rub.

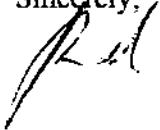
The proposed plan calls for planning staff and developers to essentially agree that a development proposal in an existing housing area seems like a good idea. The proposal would then move forward through planning & zoning, with a limited number of adjacent properties notified, and then a sign and notice essentially letting the public know the process is halfway done. At no time will the *existing residents* be consulted about their overall view of *their existing neighbourhood*. There is a strong probability that once the process starts, it continues to creep as the next properties are proposed for re-development and the "adjacent" residents are now the residents of the 40 unit condominium two doors down. Although the new OCP says that any such development shall be sensitive to and shall mitigate the impact on existing neighbours, a six foot fence does not lessen the impact of a forty foot wall beside you.

If a neighbourhood becomes uncertain of the future of their homes, there is little incentive to upgrade or even maintain their homes. The result can be a decline in appearance and property values until someone wants to tear down the existing homes and rebuild higher density.

While re-development within an existing residential community is not necessarily a bad thing, the rights of residents who are living in what they consider to be a quiet neighbourhood of single family or duplex homes, must be respected. Existing neighbourhoods belong to the existing residents and the OCP must respect their views and wishes.

While the proposed OCP is laudable in many ways, it needs to include consultative neighbourhood plans showing what each community is willing to see in their area. Changes to existing neighbourhoods must involve the neighbourhood as a whole, so notification must be expanded beyond the immediately adjacent property owners for the new process to work fairly and effectively.

Sincerely;

A handwritten signature in black ink, appearing to read "John Crook", written over the typed name below.

John Crook

Dianna Plouffe

From: Lindy Kaercher
Sent: Tuesday, May 13, 2008 10:45 AM
To: 'Cheryl McLachlan'
Cc: Dianna Plouffe
Subject: RE: opposition to Draft OCP Bylaw 1200 & draft zoning ByLaw 1201

Good morning Cheryl. This is to acknowledge receipt of your email and to advise you your email has been forwarded to the Planning Department for the Public Hearing on Bylaw Nos. 1200 & 1201. Regards Lindy

Lindy Kaercher, Deputy Clerk
City of Langford
2nd floor, 877 Goldstream Avenue
Langford BC V9B 2X8
phone: (250) 478-7882 fax: (250) 391-3407 lkaercher@cityoflangford.ca www.cityoflangford.ca

-----Original Message-----

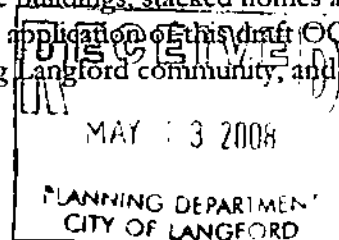
From: Cheryl McLachlan [<mailto:cherylmclachlan@shaw.ca>]
Sent: May 5, 2008 10:53 PM
To: Lindy Kaercher
Subject: opposition to Draft OCP Bylaw 1200 & draft zoning ByLaw 1201

Re: Draft OCP Bylaw 1200 and Draft Zoning Bylaw 1201

I wish to express my strong opposition to the draft OCP Bylaw 1200 and draft zoning Bylaw 1201. I attended the information sessions held at both Royal Roads and the Legion on the joint Langford-Colwood OCP. I gave input and tried to be informed about the content of the OCP throughout the drafting process. However, the draft OCP (1200) and associated draft Design Guidelines and Development Permit Areas (1201) are extensive and unwieldy documents to study. I was only able to download them this weekend, leaving me practically no time to study such a large amount of material, prior to the Public Hearing this Monday May 5th at 7PM. For that reason I am in opposition because I feel I and many, many others, who have actively tried to study the documents still have many sections unread and other sections read but not understood.

I am also in opposition to the both Bylaw 1200 and Bylaw 1201 since much of what little I have read I strongly disagree with. While mixed-use can be a very good neighbourhood and city design, there needs to be guidelines, which I have not found, on what type of use, what density of building and what noise, water and air pollution will be brought into an area so as to not radically alter existing areas. I find no limits to growth, or where that growth occurs. I also do not see linkage between population and city centre growth, as also noted in BC Transit's March 20th letter to City of Langford re: OCP. I also do not see any language protecting existing natural areas or existing neighbourhoods in their current density and character. The District of Saanich has gone to great lengths to preserve parts of Saanich in their OCP so that existing residents get to keep the communities they helped develop.

There can not be unending growth everywhere in Langford otherwise the charm of wooded neighbourhoods sandwiched between Langford's many lakes and ponds, interlinked by a trail system within a short distance to travel to major retail and commercial services will be lost to acres of highrise buildings, stacked homes and businesses surrounded by blacktop and concrete. I have strong concerns that the application of this draft OCP and associated bylaws do not plan out growth to maintain the integrity of the existing Langford community, and environment they exist within, but simply attempts to replace what does exist.



There are already concerns in Langford about how we all co-exist. There are dense housing developments being zoned into neighbourhoods that are unwanted because they do not fit into the existing community they are to be built in. We already have unwanted industrial activity disturbing and disrupting pre-existing residential and light commercial businesses in neighbourhoods. Therefore proposing a mixed-use type of zoning means that we are already intimately aware of the need for guidelines and limits to that mixed use that are simply not found in these draft documents. If the OCP is to guide Langford through its next phase of development then it needs guidelines.

I also strongly object to the lack of time given to the citizens of Langford to acquire and study the extensive amount of information contained in the Final Draft of the OCP. The draft OCP alone is 139 pages, while the accompanying Bylaw 1201 is 185 pages. The inability of many people to get the electronic version downloaded, and the lack of available printed copies has irreparably damaged the integrity of the Public Hearing process. Thus I respectfully ask Langford Mayor and Council to rescind from Monday, May 5, Council Meeting's agenda the Public Hearing on Bylaws 1200 and 1201. Prior to rescheduling of the Public Hearing on Bylaws 1200 and 1201 I respectfully submit there is a very great need for more community discussion, study and questioning of Langford's future path and draft OCP and associated bylaws among the citizens of Langford.

Thank you for taking the time to consider my letter,

Sincerely,

Cheryl McLachlan
2719 Winster Road
Langford BC V9B 3P4

MAY 13 2008

DEPOSITION TO THE PUBLIC HEARING ON BY-LAWS # 1200 & # 1201, BEING THE
NEW OFFICIAL COMMUNITY PLAN & ZONING BY-LAW FOR CITY OF LANGFORD,
TO BE HELD ON TUESDAY, MAY 20, 2008, @ 19:00

In the new Official Community Plan By-law 1200 there is a blanket designation of Mixed Use in almost all zones. I am opposed to that. Mixed Use has its place in higher density areas, such as the city core, the 2 village centres, and the 7 neighbourhood centres, and also in other areas where appropriate and/or accepted. But there are many areas, some of them of lower density, where Mixed Use is inappropriate and some uses unacceptable to residents. A blanket designation confers a right of Mixed Use to every property owner on almost every parcel of land. Instead of a blanket designation, the preamble of the new Official Community Plan should include the statement that Langford has now adopted a policy to look favourably at Mixed Use in high-density areas (core, village & neighbourhood centres) and other areas where appropriate and/or accepted. Such a policy confers a gift to the property owner, rather than a right, and as such could at times result in certain amenities to that area.

I have not had much of a chance to read, let alone study, the new Official Community Plan, but a first, but cursory, impression from learning about some of the changes is that the new Plan and Zoning will be so comprehensive, so inclusive of any and all possibilities of changes, rather than that important changes in land use will be judged on their merits. I am opposed to that. When the new Plan and Zoning are so comprehensive (and even more so with the blanket Mixed Use designation), then there will be a great reduction in input by, and consultation of, area and City residents on developments and changing uses. So, the result will be a great reduction in public consultation, and a reduction in the inclusion of Langford residents who may be affected by the changes-to-come. Changes-to-come will be much more imposed on residents rather than welcomed or accepted by them, thereby reducing a sense of community spirit and pride. Ask yourself the question: why are developers so proud of their projects? The answer is: because they are involved, they take part in decisions to be made, they are and feel included. This sense will be more and more removed from the residents of Langford, when the new Plan and Zoning are so comprehensive. On the other hand, I want to reiterate here that it is right and proper for an Official Community Plan to be comprehensive about affordable housing and transportation choices and park space per capita and natural space and Environmentally Sensitive Areas.

I spent a little more time and effort looking in the new Official Community Plan for the protection of Environmentally Sensitive Areas and species-at-risk. This new Official Community Plan very admirably says that it is based on sustainability principles, and encourages the use of native vegetation in landscaping. That is great. But the natural environment is the basis for the ecology to function and the basis for sustainable communities, and this is dealt with way too casually. And I am opposed to that. For example, I see not much intention to use Development Permit Areas for the protection of the natural environment. I see no concrete environmental targets that reflect Langford's values, and that are measurable, and that tell developers what our expectations are.

Terms such as park, natural area, open space, greenway, biodiversity, ecological value, etc., have no definition in the Glossary of Terms in this Official Community Plan. Parks can be for sport and exercise, like playing fields, or can be for quiet enjoyment of natural areas. Greenways can even be a concrete or paver sidewalk with grass and Japanese cherry trees on either side, and do nothing for the biodiversity goals; nor do playing fields. And so, integrating all these areas does nothing to support biodiversity. And there are also no concrete biodiversity targets that can be measured. And without a quality of biodiversity there can be no sustainability. The new Official Community Plan is strong on design, density, growth management, etc., but weak on the natural environment, other than the phrase: "parks and open space integrated throughout" which is bandied about. Unfortunately the new Official Community Plan fails to grasp that the natural environment and ecosystem function is a land use itself in each land use designation such as residential, commercial, etc. I like the sound of the Section title: "Natural Settings and Open Spaces", but unfortunately it is meaningless, because that Section deals with the natural environment. As a basis for sustainability the natural environment is not a natural setting, nor an open space, but instead it is a whole bunch of functioning ecological systems that sustain human health and the economy of the area. In other words: the Section title is a non-match with the first Sustainability Goal in this new Official Community Plan.

This new Official Community Plan is not worth its salt unless it requires the speedy development of a plan for integrated watershed and functioning connections, corridors, and links between ecosystems and their components in order to preserve, and allow for, diversity of flora and fauna, and very importantly, co-ordinate all land use activities. This new Official Community Plan fails to obligate Langford City to develop a good inventory and information base about all sensitive ecosystems in order to guide land use plans and regulations that protect sensitive ecosystems. It is essential that the new Official Community Plan requires that Langford maps all Environmentally Sensitive Areas, including Garry oak and Douglas fir and associated ecosystems as well as Species at Risk, and builds a comprehensive Sensitive Ecosystems Inventory, that will describe them all. It is just as essential that the new Official Community Plan demands that developers obtain and present all available information from Sensitive Ecosystems Inventories, natural atlases, and the Conservation Data Centre, and any other relevant inventories.



Axel Brock-Miller
2621 Savory Road
Langford

CITY OF LANGFORD
MINUTES OF THE REGULAR MEETING OF COUNCIL
Monday, May 5th, 2008 @ 7:00 p.m.
Council Chambers, 3rd Floor, 877 Goldstream Avenue

PRESENT

Mayor S. Young, Councillors: D. Blackwell; J. Goudy, L. Seaton, M. Sahlstrom, W. Sifert, and L. Szpak.

ATTENDING

Clerk Administrator, Rob Buchan; Deputy Administrator, Jim Bowden, Deputy Clerk, Lindy Kaercher, City Engineer, John Manson, Treasurer, Steve Ternent and City Planner, Matthew Baldwin.

1. CALL TO ORDER

The Mayor called the meeting to order at 7:00 p.m.

2. APPROVAL OF THE AGENDA

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR GOUDY

That the Agenda be approved as amended: DELETE: Agenda items: 3(a) & (b), 10 (g) & (h).

CARRIED.

4. ADOPTION OF MINUTES

a) Minutes of the Meeting of Council – April 21st , 2008

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council adopt the Minutes of the Council Meeting on April 21st, 2008.

CARRIED.

b) Minutes of the Special Meeting of Council – April 28th, 2008

MOVED BY: COUNCILLOR SEATON
SECONDED: COUNCILLOR GOUDY

That Council adopt the Minutes of the Special Council Meeting on April 28th , 2008.

CARRIED.

5. RECEIPT OF OTHER BOARD AND COMMISSION MINUTES

a) CRD & Hospital Board – April 9th, 2008

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

That Council receive the Minutes of the CRD & Hospital Board of April 9th, 2008.

CARRIED.

6. PUBLIC PARTICIPATION

Ms. Bea McKenzie of 1139 Lippencott Road addressed Council with respect to the Public Hearing process for the Official Community Plan, Bylaw No. 1200.

Mayor Young advised Ms. McKenzie of the Public Hearing process.

Mr. Jim Hartshorne of 2789 Guyton Way addressed Council with respect to the Public Hearing and Official Community Plan process.

Ms. Jennifer Burgess, Federal NDP Representative addressed Council in regard to homelessness and affordable housing issues.

7. COMMITTEE RESOLUTION

a) Parks and Recreation Committee – April 28th, 2008

1. CRD and E&N Rail Trail Project – Alignment Review (File No. 5900-40)

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

That Council direct staff to work with the Capital Regional District to ensure that the new alignment meets all the requirements of the City of Langford and work further to ensure the trail continues along Atkins Road .

CARRIED.

b) Protective Services Committee – April 29th, 2008

1. RCMP Monthly Report – March, 2008 (File No. 0650-20 RCMP)

MOVED BY: COUNCILLOR SZPAK
SECONDED: COUNCILLOR SEATON

That Council receive the RCMP Monthly Report for March, 2008.

CARRIED.

2. Fire Safety Report – January – March, 2008 (File No. 0650-20F)

MOVED BY: COUNCILLOR SZPAK
SECONDED: COUNCILLOR SEATON

That Council receive the Fire Safety Report for January – March, 2008.

CARRIED.

**3. Establishment of a New Regional Service for Emergency Program Support
(File No. 7130-02)**

MOVED BY: COUNCILLOR SZPAK
SECONDED: COUNCILLOR BLACKWELL

That Council support the establishment of a new regional service to assist existing municipal emergency preparedness coordinators.

CARRIED.

4. Review of City of Langford Open Burning Policy (File No. 0345-20)

MOVED BY: COUNCILLOR SZPAK
SECONDED: COUNCILLOR BLACKWELL

That Council direct staff to work with the municipal contractor to investigate options for garden waste.

CARRIED.

c) Committee of the Whole – May 5th, 2008 –

- 1. 2008 – 2012 Five Year Financial Plan (File No. 1715-20-2008)
(TO BE CIRCULATED AT COUNCIL MEETING)**

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR SAHLSTROM

That Council:

1. Approve the class multiples for 2008 be unchanged from the 2007 multiples;
2. Approve the 2008/2012 Five Year Financial Plan in principle as presented in the staff report dated May 5th, 2008; and
3. That the Treasurer prepare the City of Langford 2008-2012 Five Year Financial Plan Bylaw for consideration of initial readings at the May 5th, 2008 Council meeting.

CARRIED.

2. Proposed Joint Langford/Colwood Sewer Study (File No. 5340-05)

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR GOUDY

That Council direct the Clerk Administrator and City Engineer to work with the City of Colwood on the parameters of a joint Langford/Colwood Sewer Study and report back to Council.

CARRIED.

8. REPORTS

- a) 2006 Statement of Financial Information
- Staff Report (File No. 1870-30)**

MOVED BY: COUNCILLOR SAHLSTROM
SECONDED: COUNCILLOR SIFERT

That Council approve the 2006 Statement of Financial Information as presented.

CARRIED.

**b) Parade and Temporary Road Closure Request
- Staff Report (File No. 8100-20 Langford Summer Festival)**

MOVED BY: COUNCILLOR SAHLSTROM
SECONDED: COUNCILLOR SIFERT

That Council:

1. Give permission for the Langford Festival to stage a parade on July 12th along the proposed route following all Bylaw 33 permit requirements;
2. Give permission for the Langford Festival to close Aldwynd Road from the afternoon of Friday July 11th until the evening of Sunday, July 13th, 2008; and
3. that fees, insurance and damage deposit requirement be waived for the Parade and temporary closure of Aldwynd Road.

CARRIED.

**c) Island Corridor Foundation
- Staff Report (File No. 8640-01)**

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR SAHLSTROM

That Council direct staff to:

1. send a letter to the Island Corridor Foundation confirming the City's support for the campaign;
2. that the requested campaign sign-up sheet be placed in the City Hall's office; and
3. direct staff to send a letter to the MLA and the Minister of Transportation in support of the Island Corridor Foundation campaign.

CARRIED.

9. CORRESPONDENCE

**a) Glen Lake Elementary School
Re: Financial Request (File No. 049020 G)**

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

That Council donate \$1,000 from the Mayor's Charity golf tournament to Glen Lake Elementary School to support the closing of Glen Lake Elementary School's 'Big Finish' to be held on June 7th, 2008.

CARRIED.

**b) Trico Homes (Calgary)
Re: Affordable Housing (File No. 6940-01)**

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SZPAK

That Council receive and file the email dated April 29th, 2008 from Trico Homes (Calgary) with regard to Langford's leadership on the affordable housing program.

CARRIED.

c) **Westshore Rotary**
Re: Chili Cook-off (File No. 0230-20W)

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

That Council donate \$200 to the Westshore Rotary chili cook off to be held on June 7th, 2008.

CARRIED.

d) **Mr. Ed Pakos**
Re: Group Health Benefits (File No. 1900-01)

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

That Council direct staff to forward a letter to UBCM advising of their support for the following resolution:

THEREFORE be it resolved that the Provincial Government be requested to increase funding of ½ of 1% of payroll for an account specifically designated for Group Health Benefits to assist Municipal Pension retirees to continue with their efforts to maintain and support good health and that UBCM be requested to consider this resolution.

CARRIED.

10. **BYLAWS**

a) **BYLAW NO. 1094 (File No. 3900-1094)**
"Langford Zoning Bylaw, Amendment No. 206, (891 Isabell Avenue), 2007".
(RESCIND FIRST READING, GIVE FIRST READING AS AMENDED)

MOVED BY: COUNCILLOR SIFERT
SECONDED: COUNCILLOR BLACKWELL

That Council rescind First Reading of Bylaw No. 1094.

CARRIED.

MOVED BY: COUNCILLOR SIFERT
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1094 First Reading as amended.

CARRIED.

- b) **BYLAW NO. 1155 (File No. 3900-1155)**
"City of Langford Development Cost Charge Amendment No. 11, Bylaw No. 1155, 2008."
(ADOPTION)

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council adopt Bylaw No. 1155.

CARRIED.

- c) **BYLAW NO. 1159 (File No. 3900-1159)**
"Langford Zoning Bylaw, Amendment No. 231, (1295 Kingfisher Place), 2008"
(FIRST READING)

MOVED BY: COUNCILLOR SIFERT
SECONDED: COUNCILLOR GOUDY

That Council give Bylaw No. 1159 First Reading.

CARRIED.

- d) **BYLAW NO. 1161 (File No. 3900-1161)**
"Development Cost Charge Reserve Expenditure Bylaw No. 1161, 2007".
(ADOPTION)

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR GOUDY

That Council adopt Bylaw No. 1161.

CARRIED.

- e) **BYLAW NO. 1163 (File No. 3900-1163)**
"City of Langford 2008 – 2012 Financial Plan Bylaw No. 1163, 2008"
(FIRST, SECOND AND THIRD READING)

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1163 First Reading.

CARRIED.

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1163 Second Reading.

CARRIED.

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1163 Third Reading.

CARRIED.

f) **BYLAW NO. 1164 (File No. 3900-1164)
"City of Langford Tax Rates Bylaw No. 1164, 2008"
(FIRST, SECOND AND THIRD READING)**

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1164 First Reading.

CARRIED.

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1164 Second Reading.

CARRIED.

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR BLACKWELL

That Council give Bylaw No. 1164 Third Reading.

CARRIED.

11. **'IN CAMERA' RESOLUTION**

MOVED BY: COUNCILLOR BLACKWELL
SECONDED: COUNCILLOR SIFERT

- a) That it is the opinion of Council that the public interest requires that persons other than members of Council and Officers be excluded from the meeting to consider confidential information regarding Legal Matters, New Services and Property Acquisition under section 90 (e), (f) and (k) of the Community Charter;
- b) That Council continues the meeting in closed session.

CARRIED.

12. **ADJOURNMENT**

The Mayor adjourned the meeting at 7:55 p.m.

MAYOR

CERTIFIED CORRECT
Clerk-Administrator

CITY OF LANGFORD

SPECIAL MEETING OF COUNCIL

Monday, May 12th, 2008 @ 6:30 p.m.

Council Chambers, 3rd floor, 877 Goldstream Avenue

PRESENT

Acting Mayor D. Blackwell, Councillors: L. Seaton, M. Sahlstrom, W. Sifert, L. Szpak, and J. Goudy.

ATTENDING

Clerk Administrator, Rob Buchan, Treasurer, Steve Ternent, City Engineer, Matthew Baldwin and City Planner, Matthew Baldwin.

ABSENT

Mayor Young.

1. CALL TO ORDER

The Acting Mayor called the meeting to order at 6:31 p.m.

2. BYLAWS

- a) **BYLAW NO. 1163 (File No. 3900-1163)**
"City of Langford 2008 – 2012 Financial Plan Bylaw No. 1163, 2008"
(ADOPTION)

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR SAHLSTROM

That Council adopt Bylaw No. 1163.

CARRIED.

- b) **BYLAW NO. 1164 (File No. 3900-1164)**
"City of Langford Tax Rates Bylaw No. 1164, 2008"
(ADOPTION)

MOVED BY: COUNCILLOR GOUDY
SECONDED: COUNCILLOR SAHLSTROM

That Council adopt Bylaw No. 1164.

CARRIED.

3. ADJOURNMENT

The Mayor adjourned the meeting at 6:32 p.m.

MAYOR

CERTIFIED CORRECT
Clerk-Administrator



Making a difference...together

**MINUTES OF A MEETING OF THE CAPITAL REGIONAL DISTRICT BOARD,
sitting as Committee of the Whole, held Wednesday, April 30, 2008 in the Board Room**

PRESENT: **Directors:** D. Blackwell (Chair), D. Amos, S. Brice, J. Brownoff, C. Causton, C. Clement, C. Coleman (for A. Lowe), T. Daly, V. Derman, J. Evans, D. Fortin, G. Hill, F. Leonard, E. Lund, J. Mar, J. Ranns, C. Thornton-Joe, L. Wergeland and G. Young.
Staff: K. Daniels, T. Corbett, R. Lapham, M. MacPhail, A. Orr and C. Thiel (Recorder).
Also Present: Paul Ricalla, Caliper Corporation

ABSENT: Directors M. Cardinal, S. DeGryp, G. Holman and J. Twa

The meeting was called to order at 1:33 p.m.

1. APPROVAL OF THE AGENDA

MOVED by Director Evans, SECONDED by Director Hill,
that the agenda, and supplementary agenda containing three requests to
address Committee of the Whole, be approved.

CARRIED

**2. APPROVAL OF MINUTES OF THE MEETINGS OF JANUARY 30 AND FEBRUARY 13,
2008**

MOVED by Director Clement, SECONDED by Director Brownoff,
that the minutes of the meetings of January 30 and February 13, 2008 be
adopted.

CARRIED

3. PRESENTATIONS/DELEGATIONS

**(a) Requests to Address Committee of the Whole re: Item 5 - Project Phasing for
the RGS 5-Year Review**

- Sandy McPherson spoke in favour of the need for an expansive Regional Growth Strategy review which builds in responsiveness and resiliency to emerging issues.
- Bob McMinn spoke in favour of the Board considering a Regional Growth Strategy review that gives proper perspective to sustainability.

**(b) Request to Address Committee of the Whole re: Item 4 – Travel Choices and
Item 5 - Project Phasing for the RGS 5-Year Review**

- Vicky Husband spoke in support of the need to strengthen the Regional Growth Strategy and Regional Urban Containment Boundary, strengthen implementation of the strategy and in support of the TravelChoices initiatives. She also urged Board members to watch the documentary entitled *End of Suburbia*.

4. TRAVEL CHOICES TRANSPORTATION PRESENTATION

Mr. Lapham gave a presentation on the CRD Regional Transportation Model, highlighting TravelChoices objectives, implementation and functions of a regional transportation model. Forecast scenarios to 2026 were reviewed.

Directors Daly and Causton entered the meeting at 1:46 p.m. and 1:50 p.m. respectively.

Messrs. MacPhail and Ricalla provided a demonstration of the new regional transportation model software which can function as a decision-making support tool for regional initiatives and the Regional Growth Strategy (RGS).

5. PROJECT PHASING FOR THE RGS 5-YEAR REVIEW

Ms. Corbett gave a presentation on the RGS review, providing the rationale for a review, the relationship of the RGS to CRD Strategic Priorities (2006-2008) and recommending a phased and refocused approach for the project.

It was suggested that in order to implement the goals and objectives of the RGS and sub-strategies, a greater emphasis could be placed on developing options and tools. For example implementation agreements could be used to guide decision making in areas like servicing and boundary extensions, RGS updates and amendments. These agreements would be developed in cooperation with member municipalities, with input from the RGS facilitator and legal advice, and would not be subject to the legislated review process.

A discussion period ensued and the following points were raised:

- the CRD Roundtable on the Environment is keen to participate early on in the review process;
- while there are differences, the region shares space problems and global problems which will press us to seek solutions; the RGS has to become a sustainability document – a living, working document;
- we need to consider what we want our communities to look like in the future;
- sustainability goes beyond carbon neutral considerations;
- get the school districts and DND involved since they effect green house gases;
- First Nations are asking to participate in the process;
- there are ten smart growth principles, and not just one, ie. density.

MOVED by Director Brownoff, SECONDED by Director Derman,
that Committee of the Whole recommend to the Board that:

1. the Regional Growth Strategy be revised using a phased approach;
2. staff be directed to prepare a consultation plan to guide the revision process; and
3. the Ministry of Community Services be notified of the Capital Regional District's intent to revise the Regional Growth Strategy.

During discussion on the motion, it was stressed that gap analysis from municipal administrators is critical so that we know what needs to be addressed. There was a question as to whether the Juan de Fuca forest lands are now going to be a topic of focus in the RGS. It was also suggested that we need to consider how growth is created and by whom and that our plans need to be more clear on the issue of growth.

Director Clement left the meeting at 2:35 p.m.

The motion was then put,

CARRIED

6. MOTION TO MOVE IN CAMERA

MOVED by Director Brownoff, SECONDED by Director Evans, that the Committee close the meeting and move in camera in accordance with the Community Charter, Part 4, Division 3, 90(1)(i) the receipt of advice that is subject to solicitor-client privilege, including communications necessary for that purpose.

CARRIED

7. ADJOURN

MOVED by Director Brice, SECONDED by Director Brownoff, that the open portion of the meeting adjourn at 2:47 p.m.

CARRIED

CERTIFIED CORRECT:

CHAIR

CORPORATE OFFICER

DISTRICT OF LANGFORD
2nd Floor, 877 Goldstream Ave.
Victoria, B.C.
V9B 2X8



Administration and Finance
Phone: (250) 478-7882
Fax: (250) 478-7864
Email: adminfinance@cityoflangford.ca
Web Site: <http://cityoflangford.ca>

STAFF REPORT TO COUNCIL

Date: May 20, 2008

File No.: 1970-02

Subject: Decrease in Parcel Tax Rates – DSA #1, SSA #1A, #10B, #10C, #12

COMMENTARY

Included on the agenda for this meeting are bylaws #1165, #1166, #1167, #1168 and #1169 which amend the parcel tax rates for Drainage Specified Area No. 1 and Sanitary Sewer Specified Areas No. 1A, 10B, 10C and 12 respectively. The purpose of these bylaws is to reduce the parcel tax rates for these specified areas to reflect increase taxable frontage due to various re-zonings that increased density.

RECOMMENDATION

That Council give three readings to bylaws #1165, #1166, #1167, #1168 and # 1169.

Respectfully submitted,

Handwritten signature of Steve Ternent in black ink.

Steve Ternent
Treasurer

Handwritten signature of Rob Buchan in black ink.

Rob Buchan
Clerk/Administrator

Handwritten signature of Matthew Baldwin in black ink.

Matthew Baldwin
City Planner

Handwritten signature of John Manson in black ink.

John Manson
City Engineer



Staff Report

to

Council

Date: May 20, 2008
File: Z-07-42
Subject: Lakefront Subdivision Regulations

PURPOSE

The proposed new Official Community Plan (OCP) suggests increasing the minimum lot size of lakefront lots, excluding the area of the Streamside Protection and Enhancement Area (SPEA) from the minimum lot size calculation, and allowing a smaller minimum lot size where the SPEA is gifted to the City as parkland. In light of this, Council may wish to consider inserting 'lakefront lot' regulations into the *Subdivision Lot Requirement* section of the R1 (One-Family Residential) and R2 (One- and Two-Family Residential) zones.

BACKGROUND

On August 29, 2007, two petitions were submitted to the City of Langford by residents of the Glen Lake area with respect to increasing the minimum subdivision requirements for lakefront subdivisions. The first petition, signed by 63 residents, opposes a subdivision application at 3177 Glen Lake Road due to the proposed creation of three narrow lakefront lots. The second petition, signed by 8 residents, asks Council to consider increasing the riparian setback from 43m (141ft) to 60m (197ft), to not allow any variances to side, rear or front setbacks, and to increase the minimum lot size of lakefront properties from 550 m² (5,920 ft²) to 750m² (8,073 ft²). In response to these petitions, Council passed the following resolution at their regular meeting held October 15, 2007:

That Council receive and file the submitted petitions and letter, refer the petitions to staff for further review and direct staff to work with the OCP consultant to consider policy options that would preserve larger lots adjacent to Glen Lake, Florence Lake and Langford Lake that could be incorporated into the OCP during the current OCP review process.

Staff worked with the OCP consultant during the OCP Review process to create new lakefront development policy to incorporate into the new OCP. Objective 3.15 and Policy 3.15.1, as found in OCP Bylaw No. 1200, have been inserted below:

Objective 3.15 Preserve the character of Langford's lakes.

Policy 3.15.1 Create lakefront-specific subdivision regulations, which may include:

- i. Encourage park dedication to be taken adjacent to lakes, and encourage creation of trail access, boat launches, and viewing platforms, to provide public access to and for the enjoyment of the lakes.*
- ii. Exclude streamside protection and enhancement area (SPEA) from minimum lot size calculation*
- iii. Create larger minimum lot size for lakefront lots and allow slightly smaller minimum lot size where SPEA is gifted to City as park.*

COMMENTS

Upon an analysis of lakefront lot sizes, staff have determined that there is substantial potential for lakefront subdivision to occur under current zoning regulations. It is noted that lot area alone is not an accurate indicator of subdivision potential, as factors such as lot width, existing house location, protection of riparian areas, provision of adequate access, etc must also be taken into consideration. However, this potential for subdivision could significantly change the character of the lakeshore communities counter to the desire of many lakefront residents, and could increase the pressure on the environmentally sensitive lake shorelines. Therefore, Council may wish consider implementing new, larger, subdivision lot requirements that would reduce the potential for subdivision adjacent to the lakes.

As noted above, the new OCP suggests increasing the minimum lot size of lakefront lots, excluding the area of the Streamside Protection and Enhancement Area (SPEA) from the minimum lot size calculation, and allowing a smaller minimum lot size where the SPEA is gifted to the City as parkland.

The SPEA is the minimum non-disturbance area that must be preserved adjacent to lakes, wetlands, and watercourses in order to protect riparian and fish habitat. The extent of the SPEA is determined by a Registered Professional Biologist through a detailed site assessment based on the provincial Riparian Areas Regulations, enacted under the *Fish Protection Act*. The SPEA does vary somewhat from lot to lot depending on site conditions and geographic location, from as low as 15m (49ft) from the natural boundary of the lake to as much as 30m (98ft). The area of SPEA that could be excluded from the current minimum lot size, therefore, is as follows:

- For a conventional lot (550 m² or greater) with a 16m width:
 - 15m SPEA - 240m²
 - 20m SPEA - 320 m²
 - 30m SPEA – 480 m²

- For an undersized strata lot (under 550 m²), with a 9.5m width:
 - 15m SPEA – 142.5 m²
 - 20m SPEA – 190 m²
 - 30m SPEA – 285 m²

To provide an incentive for applicants to dedicate the SPEA as parkland, Council may wish to establish a larger minimum lot size, i.e. 1,500 m² (16,146 ft²), plus require the SPEA to be excluded from the lot size calculation. As a stand-alone regulation, this would preclude the majority of the current subdivision potential around the lakes. Council could also establish a 'density bonus' provision to allow for a smaller minimum lot size, i.e. 950 m² (10,226 ft²), where the SPEA is dedicated to the City as park. The difference between the larger and smaller minimum lot sizes should be substantial enough to provide this incentive.

Council may wish to note that a number of lots abutting Glen and Florence Lakes are zoned R2 (One- and Two-Family Residential) and have sufficient land area under current regulations for the development of two-family dwellings. If Council does not wish to permit any additional two family dwellings on lakefront lots, they may also wish to direct staff to amend the *Regulations for Two-Family Dwelling Use* section of the R2 (One- and Two-Family Residential) zone such that two-family dwellings are not permitted on lakefront lots.

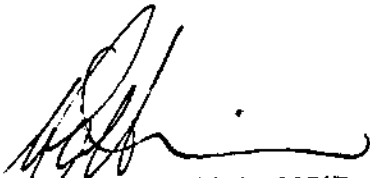
OPTIONS

That Council:

1. Direct staff to prepare a bylaw to amend the Zoning Bylaw No. 300 by:
 - a) Inserting a new subsection into the **Subdivision Lot Requirements** section of the R1 (One-Family Residential) and R2 (One- and Two-Family Residential) zones that is applicable only to lakefront lots, and includes the following subdivision lot regulations:
 - (i) No lot adjacent to Langford, Glen or Florence Lake having an area less than 1,500 m² (16,146 ft²) may be created by subdivision;
 - (ii) The streamside protection and enhancement area designated by a qualified environmental professional may NOT be included in the lot area calculation for the purpose of determining compliance with (i);
 - (iii) Notwithstanding (i), a lot having an area of 950 m² (10,226 ft²) may be created by subdivision adjacent to Langford, Glen or Florence Lake if the streamside protection and enhancement area is dedicated as park and transferred to City of Langford ownership at the time of subdivision.
 - b) Inserting a new regulation into the **Regulations for Two-Family Dwelling Use** section of the R2 (One- and Two-Family Residential) zone that prohibits two-family dwellings on lakefront lots;

OR

2. Take no further action at this time with respect to amending Zoning regulations for lakefront subdivisions and two-family dwelling uses;

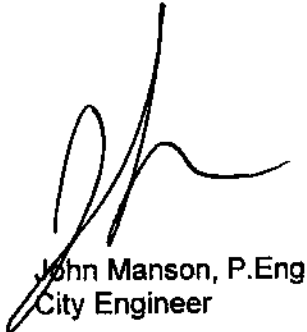


Matthew Baldwin, MCIP
City Planner

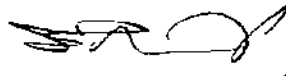


Leah Stohmann
Planner II


Bob Beckett
Fire Chief



John Manson, P.Eng
City Engineer



Steve Tement
Treasurer



Rob Buchan, MCIP
Clerk-Administrator

:ls



Staff Report
to
Council

Date: May 20th, 2008
File: 1850-01 and 5280-16
Subject: Contribution towards American Bullfrog Eradication Program

Background:

In its regular meeting of March 17th, 2008, Council received a letter from the CRD directing staff to write a letter to the Ministry of Environment requesting a funding contribution to the American Bullfrog Eradication Program in the West Shore control corridor. Staff have sent the letter as requested and to date have not heard back from the Ministry of Environment.

Council has included a \$10,000 contribution towards the bullfrog eradication program in its 2008 budget. Staff is also aware that the CRD is contributing towards the program. As the City of Langford is a benefactor of the program and had been a supporter of the program since its inception, staff request Council's direction to dispense or to provide the \$10,000 in funding to BullfrogControl.com Inc. for the program for 2008.

Recommendation:

That Council direct staff to send the \$10,000.00 contribution towards the American Bullfrog eradication program to Mr. Stan Orchard.

Respectfully submitted,

Handwritten signature of Rob Buchan in black ink.

Rob Buchan
Clerk Administrator

Handwritten signature of Jim Bowden in black ink.

Jim Bowden
Deputy Administrator

Handwritten signature of John Manson in black ink.

John Manson
City Engineer

Handwritten signature of Matthew Baldwin in black ink.

Matthew Baldwin
City Planner

Handwritten signature of Steve Ternent in black ink.

Steve Ternent
Treasurer

Bob Beckett
Fire Chief

DISTRICT OF LANGFORD - BUILDING INSPECTION DEPT.
Monthly Report to April 30, 2008 – File No. 0650-20 (Building)

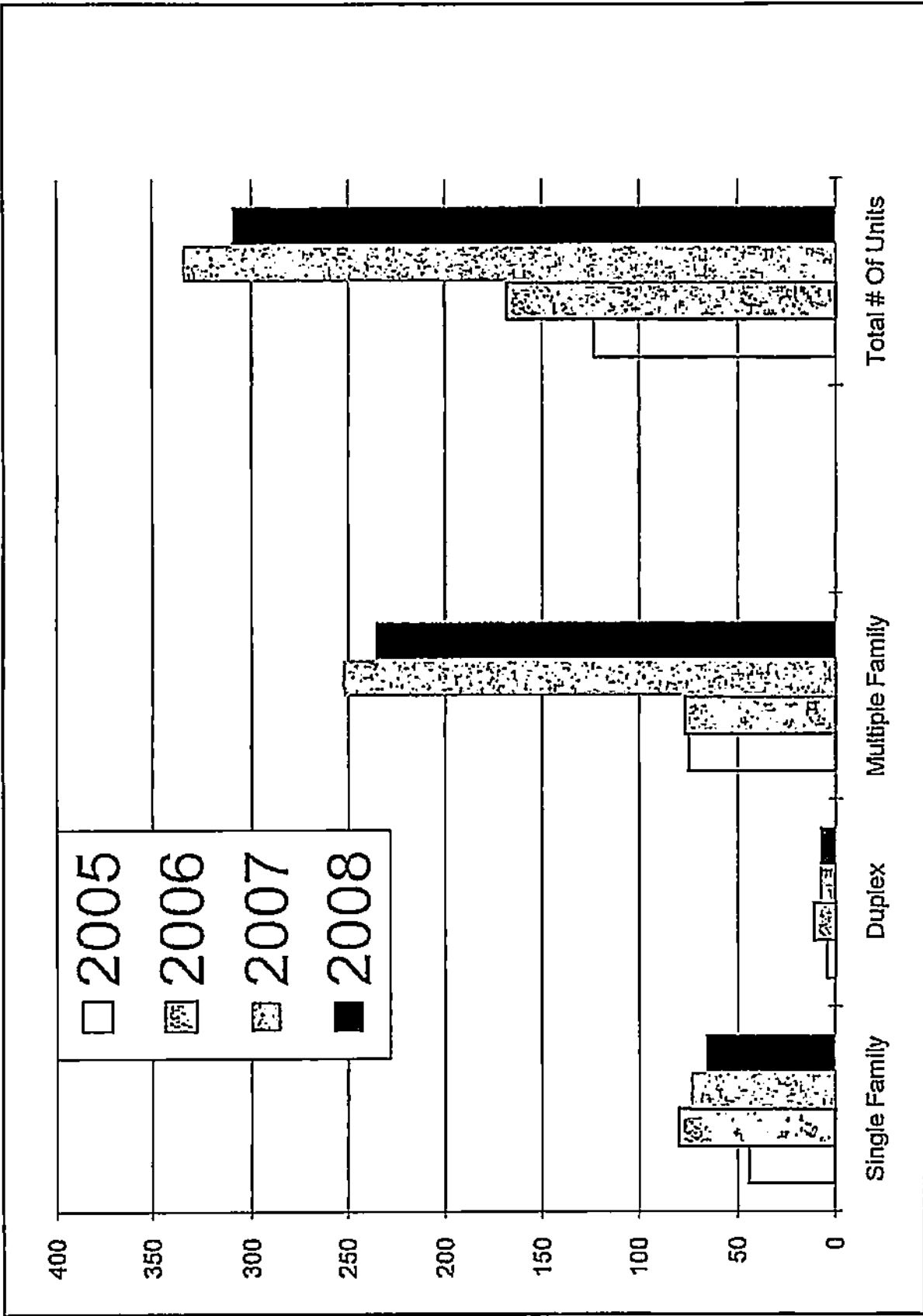
PERMITS	Apr. 2008	Apr. 2007	YEAR TO DATE 2008	YEAR TO DATE 2007
Single Family Dwelling No. of Units	20	14	66	74
Construction Cost	\$3,971,399.00	\$1,288,100.00	\$9,865,219.00	\$7,974,160.00
Permit Fee	\$34,974.00	\$13,459.00	\$103,420.00	\$88,590.00
Duplex - No. of Units	---	---	8	8
Construction Cost	---	---	\$798,190.00	\$737,245.00
Permit Fee	---	---	\$9,745.00	\$8,923.00
Multiple Family Dwelling No. of Units	62	55	236	253
Construction Cost	\$6,220,366.00	\$7,820,322.00	\$29,726,417.00	\$56,943,672.00
Permit Fee	\$36,944.00	\$43,213.00	\$147,481.00	\$196,067.00
Residential Additions/Alterations No. of Permits	12	4	46	28
Construction Cost	\$430,240.00	\$25,100.00	\$14,657,058.00	\$1,658,100.00
Permit Fee	\$4,459.00	\$574.00	\$94,183.00	\$18,385.00
Commercial/Industrial No. of Permits	8	5	26	25
Construction Cost	\$1,358,000.00	\$625,000.00	\$3,655,850.00	\$9,613,000.00
Permit Fee	\$13,262.00	\$5,670.00	\$41,195.00	\$58,165.00
Institutional - No. of Permits	---	---	---	---
Construction Cost	---	---	---	---
Permit Fee	---	---	---	---
Plumbing - No. of Permits	64	47	247	183
Permit Fee	\$23,514.00	\$11,132.00	\$82,598.00	\$59,788.00
Chimney - No. of Permits	9	3	20	25
Permit Fee	\$225.00	\$105.00	\$600.00	\$725.00
TOTAL YEAR TO DATE - 2008		TOTAL YEAR TO DATE - 2007		
Construction Cost	Permit Fees	Construction Cost	Permit Fee	
\$58,702,734.00	\$479,222.00	\$76,926,177.00	\$430,643.00	

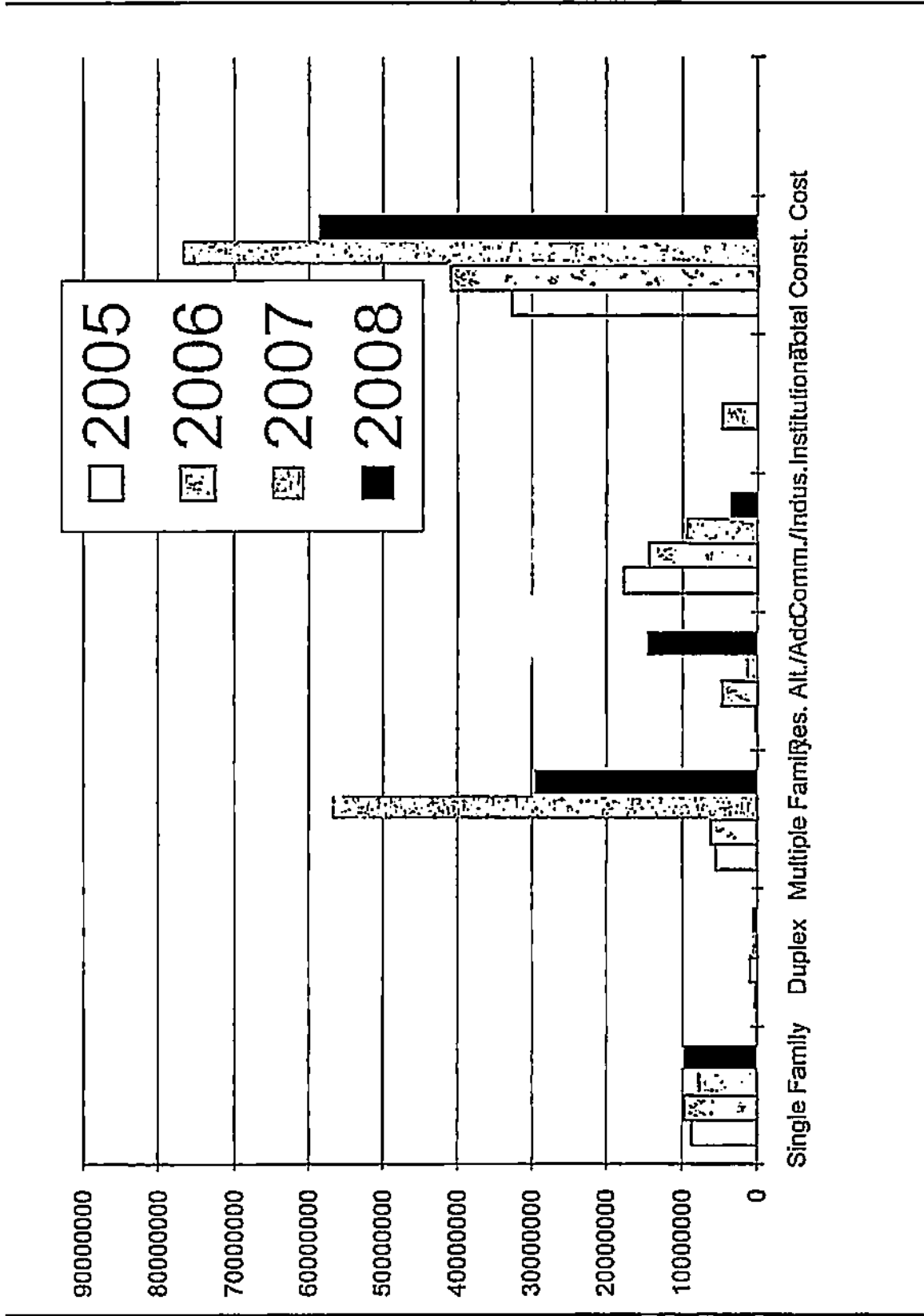
Secondary Suites = 45 for 2007 (to Dec. 31st) / 25 for 2008

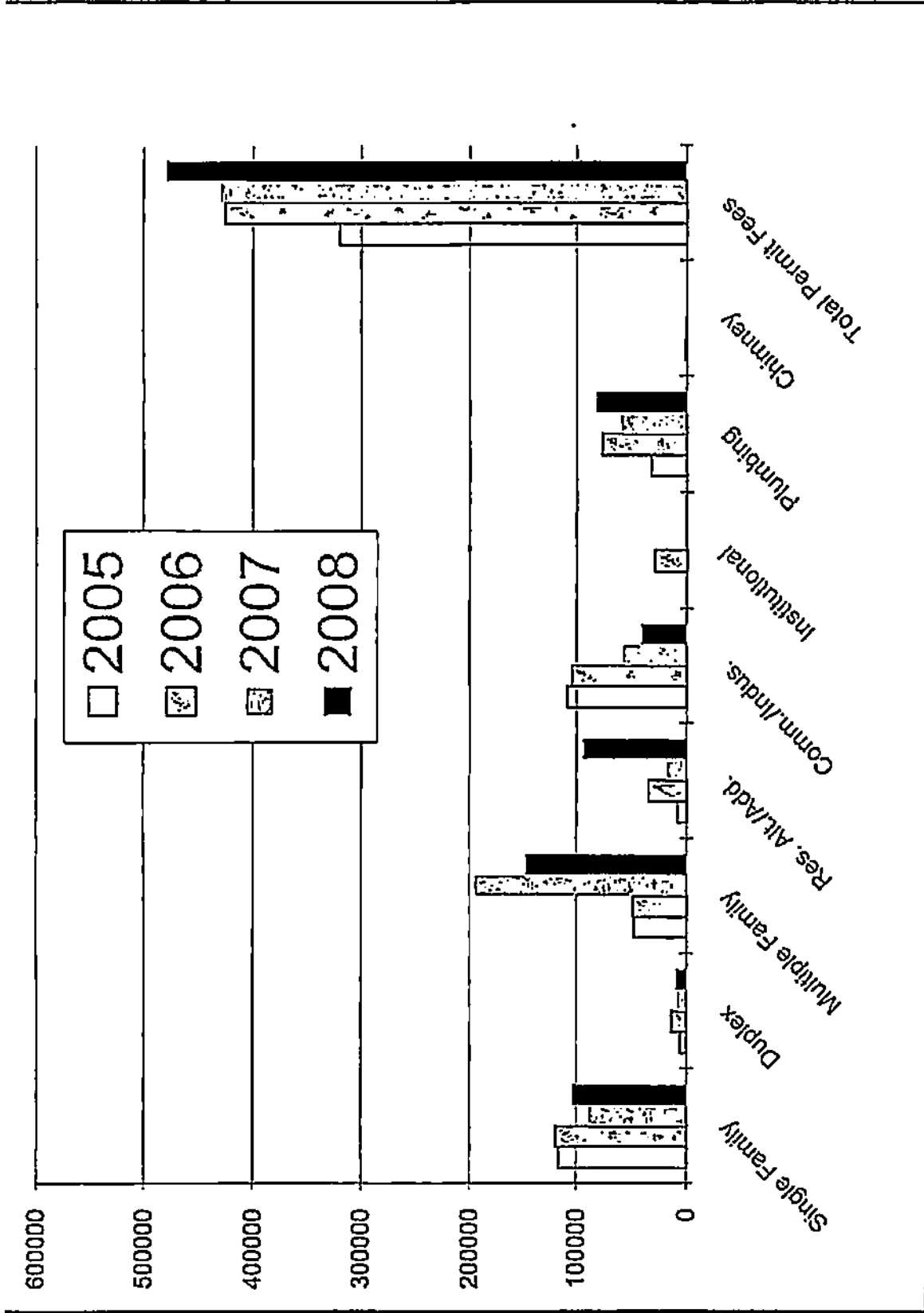
REVISED

City of Langford

Number of Units
To Apr. 30, 2008









Staff Report

to

Council

Date: May 20th 2008
File: 1855-01
Subject: Gas Tax Grant applications

BACKGROUND

On April 30, 2008, staff submitted applications for funding under the Gas Tax Agreement's General Strategic Priorities Fund (GSPF) and Innovations Fund (IF). In total four applications were made, two under each fund. As part of these applications, a Council resolution, specifically supporting the applications under each fund, has been requested.

DESCRIPTION

The City of Langford Active Transportation Plan involves the City hiring a co-op student for a period of four months, to assess the current range of various transportation modes with the exception of car usage. The results will be outlined in network plans for each transportation method. These plans will be converted into one, multi-layered plan which will link the various networks together and provide an information tool for the public as well as Langford. The plan will show the location of sidewalks, trails, bike paths, the Langford Trolley route, pedestrian crossings, bus routes and bicycle facilities.

The Westshore Transportation Priority Project #1 - Peatt - Phipps Connector related to Westshore Transit Exchange will provide for an intersection upgrade at Station and Meaford Avenue and access to West Shore Town Centre (Can West) via Peatt Road. The improvement will draw commercial traffic away from VMP and Jacklin Road to a more direct route. In addition, the 1996 CRD Regional Growth Strategy had studied Light Rail Transit (LRT) to connect Langford to downtown Victoria. When LRT develops, it is expected that the LRT station will be located at the West Shore Transit Exchange. The LRT benefits are an additional \$6.2 M annually. At this time, over 100,000 passenger kilometers will be provided for region-wide. The Peatt - Phipps Connector then becomes a major multi-modal hub when:

- The current train station at Goldstream is relocated to Peatt Road, adjacent to the West Shore Transit Exchange, either for the current train service, possibly a commuter rail alternative or ultimately a light rail station; and
- The connector provides for a pedestrian and bicycle link to the planned CRD E&N Rail Trail. The connection would be constructed along the train tracks and walking access to Langford's downtown core; and

- The connector provides access to the Langford Trolley route on Peatt Road.

This is therefore, a critical project in providing network connectivity from a Major Road and integrated transportation network perspective providing congestion and delay relief to the major adjacent highway, VMP. The relatively low cost of the project in relation to its benefits, provides for a calculated benefit to cost ratio of 3.89. The improvements are considered to be of a regional scale.

The **Permeable Paving Project** consists of retrofitting an existing road by using permeable paving technology to address engineering issues related to the environmental, social and economic fields. The project is being approached as a pilot study for evaluating the future use of permeable paver technology, as an aquatic habitat protection system, within the City. It is intended that the project achieve a variety of objectives, some of which are not easily observed, quantified and evaluated. Hence, permeable paver technology is being approached as a proactive post-construction Best Management Practices (BMP) scenario. It is felt that achievable project outcomes are increased environmental benefits, increased social benefits and decreased economic costs; all of which should occur harmoniously with the implementation of a sustainable technology such as a permeable paver system.


The objective of the **Bryn Maur Road Permeable Paving and Pilot Project (Part 2-monitoring)** is to demonstrate that permeable paving technology is a viable alternative to storm water treatment in road right of ways and to include this in future developments and road improvement projects.

OPTIONS

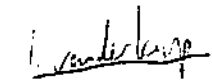
1. That Council direct staff to:
 - a) submit an application under the General Strategic Priorities Fund to the Union of BC Municipalities in the amount of \$10,000 for the City of Langford Active Transportation Plan and, if successful, that the Mayor/Chair and corporate officer be authorized to enter into any necessary funding agreements;
 - b) submit an application under the General Strategic Priorities Fund to the Union of BC Municipalities in the amount of \$2,477,362.50 for the Westshore Transportation Priority Project #1-Peatt Phipps Connector related to Westshore Transit Exchange and, if successful, that the Mayor/Chair and corporate officer be authorized to enter into any necessary funding agreements;
 - c) submit an application under the Innovations Fund to the Union of BC Municipalities in the amount of \$338,885.70 for the Permeable Paving project and, if successful, that the Mayor/Chair and corporate officer be authorized to enter into any necessary funding agreements; and
 - d) submit an application under the Innovations Fund to the Union of BC Municipalities in the amount of \$25,000 for the Bryn Maur Road Permeable Paving and Pilot Project (Part 2-monitoring) and, if successful, that the Mayor/Chair and corporate officer be authorized to enter into any necessary funding agreements;

OR

2. Take no action at this time with respect to the mentioned applications.




Matthew Baldwin, MCIP, RPP
City Planner




Ivo van der Kamp
Planner I

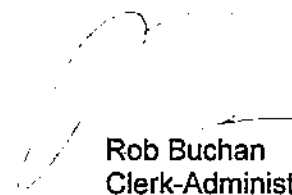
Bob Beckett
Fire Chief



Connie Nicoletti
Deputy Engineer, P. Eng.



Steve Ternent
Treasurer



Rob Buchan
Clerk-Administrator

:ivdk



Making a difference...together

Regional Parks
490 Atkins Avenue,
Victoria, BC, Canada V9B 2Z8

T 250.478.3344
F. 250.478.5416
www.crd.bc.ca

April 21, 2008

Mayor Stewart Young & Council
City of Langford
2nd Floor, 877 Goldstream Avenue
Victoria, BC V9B 2X8

Dear Mayor ~~Young~~ and Council:

**RE: REGIONAL PARKS LAND ACQUISITION FUND –
SUMMARY OF 2007 ACQUISITIONS**

In 2000, the CRD Board established the CRD Parks Land Acquisition Fund. The 10-year fund generates approximately \$1,600,000 per year to purchase land for regional parks and trails as identified through a public consultation process and documented in the CRD Parks Master Plan.

The CRD Board has endorsed a motion that Regional Parks staff annually visit municipal councils and electoral area directors to report on the status of the Land Acquisition Fund.

Enclosed is *Bulletin 8, Land Acquisition Fund – Summary of 2007 Acquisitions*. This bulletin summarizes land acquisition revenue and expenditures in 2007, future land acquisition financial commitments and acquisitions made in 2007.

Regional Parks staff are available to attend one of your council meetings to make a presentation and answer questions on the status of the Land Acquisition Fund and recent acquisitions. Please contact Jeanette Mollin at 478-3344 or email jmollin@crd.bc.ca to arrange a time for Parks staff to attend a council meeting.

Yours truly,

Denise Blackwell
Chair, CRD Board

Enclosures

JW/jm

pc: Christopher Causton, Chair, Regional Parks Committee

**CITY OF LANGFORD
Recommendation**

Refer to:

- Receive & File
- Admin Finance Comm
- Parks & Rec Committee
- P & Z Committee
- Protective Services Comm
- T & P Committee
- Clerk Administrator
- City Engineer
- City Planner
- Senior Bylaw Officer

Regional Parks Land Acquisition Fund

Summary of 2007 Acquisitions

In 2000, the Capital Regional District Board established the Regional Parks Land Acquisition Fund. The ten-year fund generates about \$1,600,000 per year to purchase land for regional parks and trails identified by the public and documented in the Regional Parks Master Plan. The Master Plan presents a vision for the future of the regional parks and trails system.

The establishment of the fund was supported by the public in an opinion poll that was held in several municipalities during the 1999 municipal elections. The cost to property owners in the CRD for the fund in 2007 was \$1.91 per \$100,000 assessed residential property value, or \$10 per average residential household value in the CRD.

Prior to 1999, Regional Parks purchased land by borrowing money. Regional Parks is still paying for these purchases. The total debt requisition in the 2007 Regional Parks budget was \$605,060. This debt will be paid off in 2012.

A summary of land acquisition revenue and expenditures from January 1 to December 31, 2007 is provided in the tables in this bulletin. A summary of the 2007 acquisitions is also provided.



Golden Lake - Sea to Sea Green Blue Belt

Land Acquisitions 2000 to 2007

Since the establishment of the Land Acquisition Fund in 2000, Regional Parks, with its partners, has purchased \$23,472,264 of land. Regional Parks has contributed \$16,749,214 (71%) and partners have contributed 6,723,050 (29%). The target established in the Land Acquisition Strategy for the financial contribution of Regional Parks and its partners to land acquisition was Regional Parks 65% and partners 35%. The total area purchased is 2821 hectares. The locations of the acquisitions are shown on the map.



Land Acquisition Fund Status - Acquisitions 2000-2007

- Acquisitions
- 2000 ○ 2003 ○ 2006
 - 2001 ○ 2004 ● 2007
 - 2002 ○ 2005
 - ▭ Regional Park and Trail

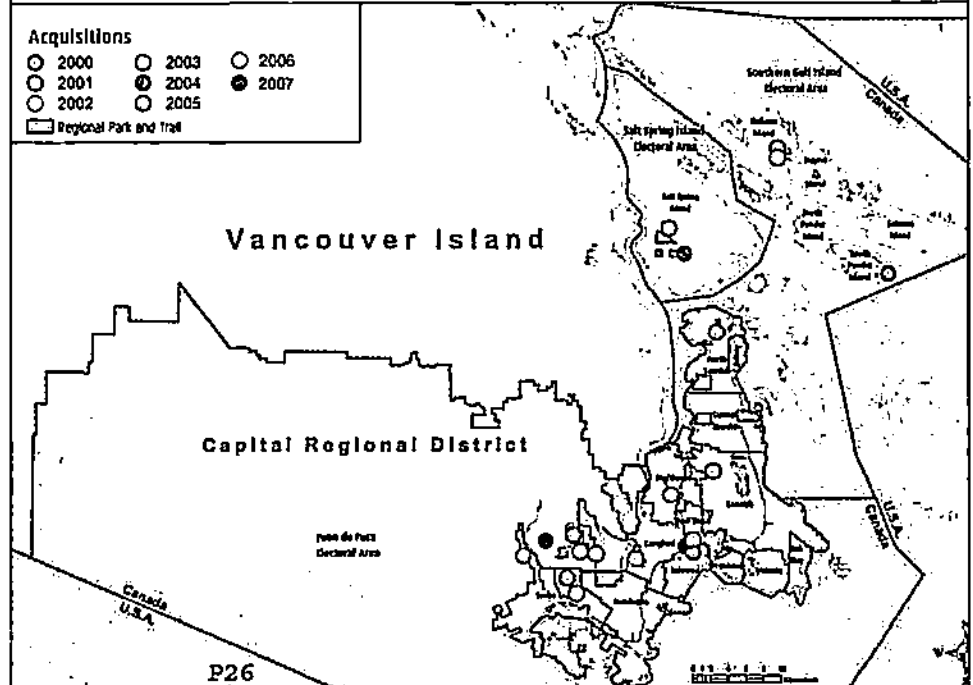


Table 1. Summary of 2007 Acquisitions

Area	Year Acquired	Location	Area (Hectares)	Expenditures		
				Acquisition Value	Regional Parks' Contribution	Partners' Contribution
Sea to Sea Green Blue Belt (TimberWest)	2007	Juan de Fuca Electoral Area, forming part of the Sea to Sea Green Blue Belt located east of the Sooke Potholes Regional Park	932.0	\$5,842,314	\$5,342,314	\$ 500,000
Mill Hill Regional Park	2007	Town of View Royal	7.26	\$ 250,000	\$ -	\$ 250,000
TOTAL			939.26	\$6,092,314	\$5,345,314	\$ 750,000

Table 2. 2007 Land Acquisition Revenue and Expenditures**2007 REVENUE**

Prior Year Carry-over	\$	1,420,669
Short Term Loan	\$	2,400,000
Transfer Revenue Fund	\$	1,617,830
Transfer Reserve Fund	\$	250,000
TLC Contribution	\$	250,000
Other Income	\$	3,467
Accrued Interest	\$	92,899
Total 2007 Revenue	\$	6,034,865

2007 EXPENDITURES

Land Purchase	\$	5,842,314
Interest on Loan	\$	6,320
Associated Land Acquisition Costs*	\$	108,628
Total 2007 Expenditures	\$	5,957,262
Balance of 2007 Regional Parks Land Acquisition Fund (carry forward to 2008)	\$	77,602

* This includes costs for land value appraisals, legal services, property taxes, surveying, acquisition negotiations, land title fees and development costs.

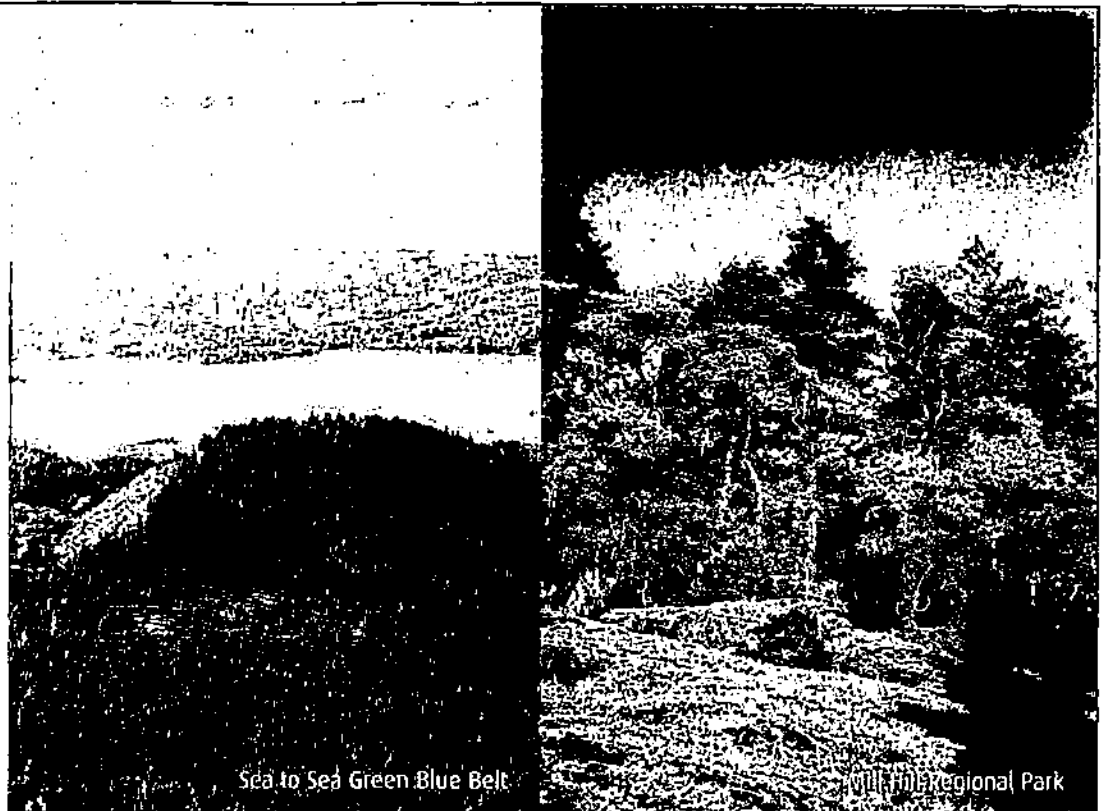
Table 3. Future Land Acquisition Financial Commitments by Regional Parks (2008 - 2009)

REVENUE	2008 - Year 9	2009 - Year 10
Prior Year Carryover	\$ 77,602	\$ 410,882
Annual Requisition	\$ 1,663,530	\$ 1,620,000
Proposed Sale of Matthews Point Subdivision	\$ 300,000	\$ 300,000
TLC Contribution	\$ 250,000	\$ -
Total Revenue	\$ 2,291,132	\$ 2,330,882
EXPENDITURES		
Proposed Acquisition Expenses	\$ 20,000	\$ 20,000
Short-Term Loan Payment	\$ 1,500,000	\$ 900,000
Interest on Short-Term Loan	\$ 110,250	\$ 33,750
The Land Conservancy Principal Payment	\$ 250,000	\$ -
Total Expenditures	\$ 1,880,250	\$ 953,750
Estimated Funds Available for Parkland Acquisition	\$ 410,882	\$ 1,377,132

2007

Acquisitions

In 2007, Regional Parks acquired two areas: (1) land from TimberWest for an addition to the Sea to Sea Green Blue Belt located east of Sooke Potholes Regional Park, and (2) an addition to Mill Hill Regional Park.

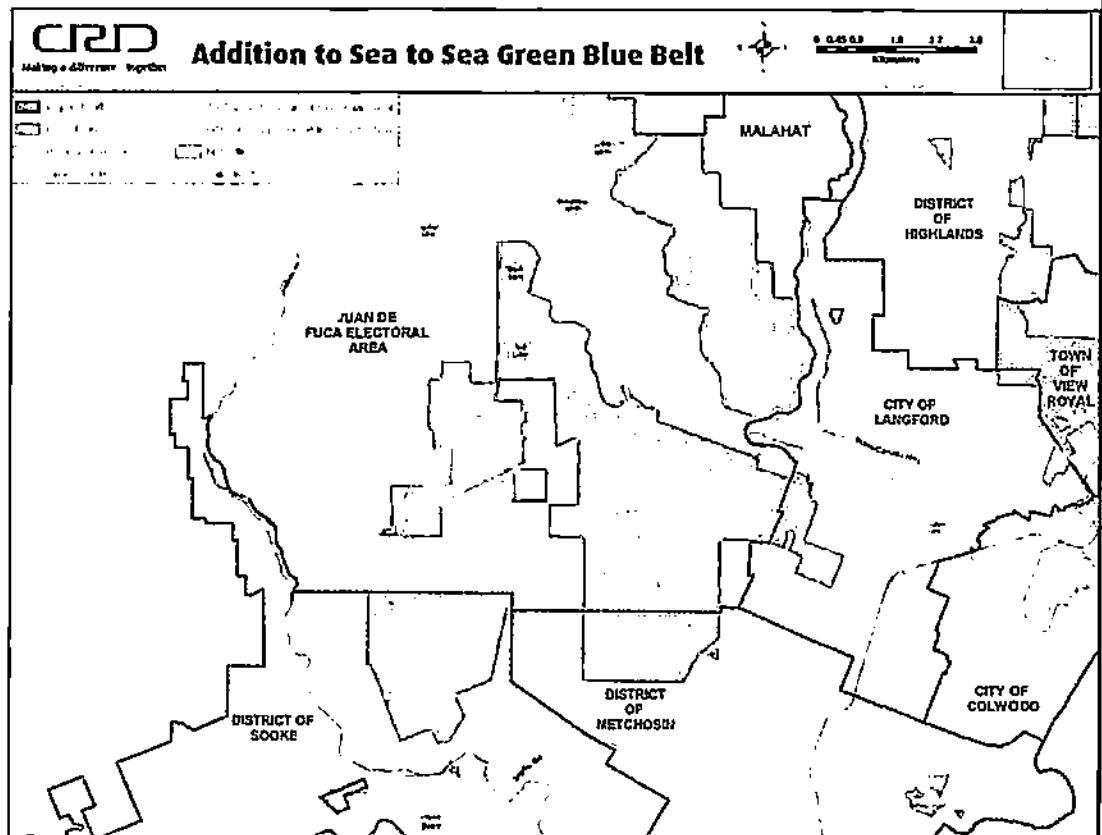


Sea to Sea Green Blue Belt Addition

The establishment of a Sea to Sea Green Blue Belt, extending from Saanich Inlet to Sooke Basin and Sooke River, is a major CRD initiative supported in the CRD's Strategic Plan, Regional Growth Strategy, Regional Parks Master Plan and Regional Parks Land Acquisition Strategy.

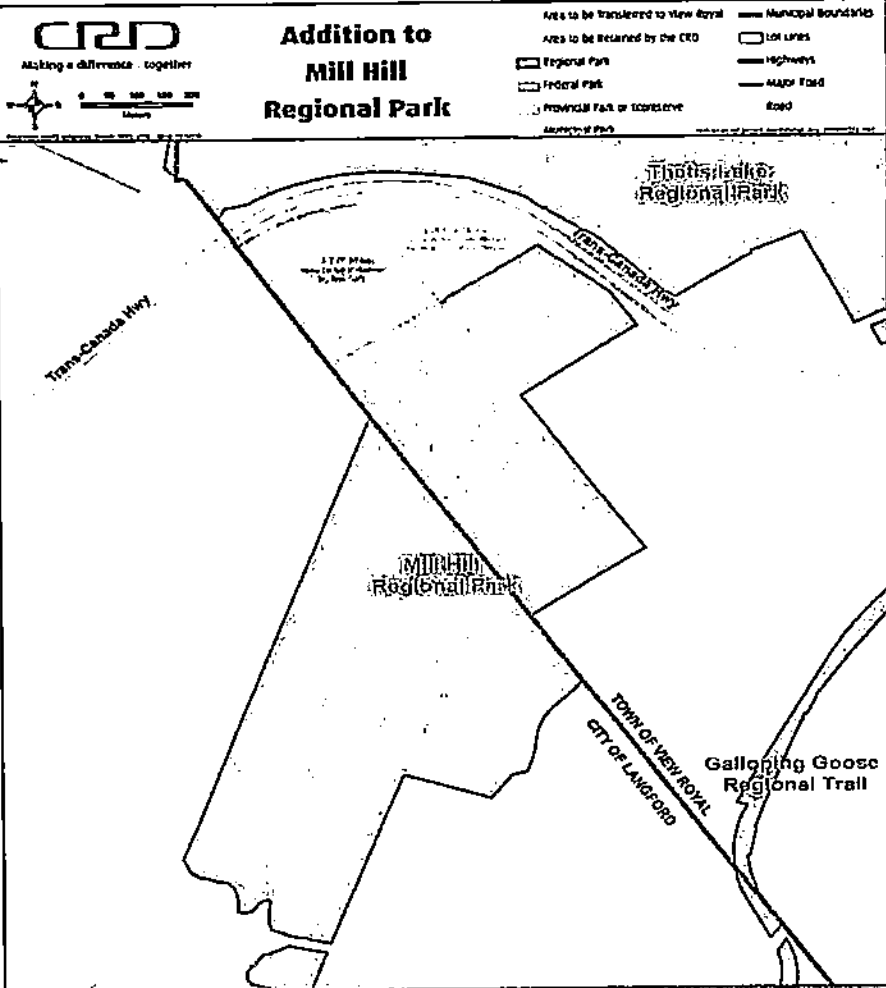
In 2007, Regional Parks and The Land Conservancy of British Columbia purchased a 932-hectare addition to the Sea to Sea Green Blue Belt. This acquisition is essential in helping to achieve the vision for the

Sea to Sea Green Blue Belt, to provide a buffer to Sooke Potholes Regional Park and protect areas adjacent to the Galloping Goose Regional Trail from development. The land includes 4 km of land along the east boundary of Sooke River and three watersheds that flow into the Sooke River. The purchase price was \$5,842,314. Regional Parks' contribution was \$5,342,314 and The Land Conservancy's contribution was \$500,000.





Regional Parks Land Acquisition Fund Bulletin 8



Mill Hill Regional Park Addition

In 2005, the CRD and the City of Victoria agreed to a land exchange under which the CRD would receive 10.38 hectares of land owned by the City of Victoria adjacent to the northwest boundary of Mill Hill Regional Park. The land transfer was completed in 2007. The transfer was subject to the CRD transferring a 3.12 hectare portion to the Town of View Royal for municipal park purposes. The addition of the remaining 7.26 hectares to the regional park will protect an older Douglas-fir forest and provide a buffer to the sensitive Garry oak ecosystems in Mill Hill.

For more information, or if you have any questions, please contact:

Partnership with The Land Conservancy (TLC)

Upon establishment of the Land Acquisition Fund in 2000, The Land Conservancy of British Columbia (TLC) made a commitment to be a major partnership in the Regional Parks' land acquisition program. Since 2000, TLC has been a partner with Regional Parks in 10 of 18 acquisitions. The TLC has contributed \$2,678,000 to assist Regional Parks with the purchase of land.

Jeff Ward, Manager
 Planning, Resource Management & Development
 Regional Parks
 490 Atkins Avenue, Victoria, BC V9B 2Z8
 T: 250.478.3344 F: 250.478.5416
www.crd.bc.ca/parks



Regional Parks
 490 Atkins Avenue,
 Victoria, BC Canada V9B 2Z8

250 478 3344
www.crd.bc.ca/parks



Association of Douglas Street Businesses

OPEN LETTER

Denise Blackwell, Chair, Capital Region District
 The Honourable Kevin Falcon, Minister of Transportation
 Mayor Jack Mar & Council, District of Central Saanich
 Mayor Jody Twa & Council, City of Colwood
 Mayor Chris Clement & Council, Township of Esquimalt
 Mayor Mark Cardinal & Council, Highlands District
 Mayor Stew Young & Council, City of Langford
 Mayor John Ranns & Council, District of Metchosin
 Mayor Ted Daly & Council, District of North Saanich
 Mayor Christopher Causton & Council, District of Oak Bay
 Mayor Graham Hill & Council, Town of View Royal
 Mayor Don Amos & Council, Town of Sidney
 Mayor Frank Leonard & Council District of Saanich
 Mayor Janet Evans & Council, District of Sooke
 Mayor Alan Lowe & Council, City of Victoria

May 2, 2008

CITY OF LANGFORD Recommendation	
Refer to:	
<input type="checkbox"/>	Receive & File
<input type="checkbox"/>	Admin Finance Comm
<input type="checkbox"/>	Parks & Rec Committee
<input type="checkbox"/>	P & Z Committee
<input type="checkbox"/>	Protective Services Comm
<input type="checkbox"/>	T & P Committee
<input type="checkbox"/>	Clerk Administrator
<input type="checkbox"/>	City Engineer
<input type="checkbox"/>	City Planner
<input type="checkbox"/>	Senior Bylaw Officer
<input checked="" type="checkbox"/>	<i>Refer to council</i>

*nothing important
of will come out*

Re: Capital Region District Transportation Plan

The Regional Growth Strategy (2003) for the Capital Region District identified the need to support "the development of (complete) communities that offer a variety of housing types and tenures in close proximity to places of work, schools, shopping and recreation". The Growth Strategy acknowledged the important link between land use and transportation and the "need to reduce the demand for trips".

Many of the South Island communities are preparing or have already updated their Official Community Plans (OCPs) taking into account the Regional Growth Strategy. Individual community plans include transportation components that sometimes attempt to address external or inter-municipal transportation issues.

All citizens in all the CRD communities use the major (arterial) roadways in each other's municipalities every day, yet there is no coordinated regional transportation plan that properly provides for the integration all modes of transportation with current or future land use plans.

Instead, we have seen independent initiatives to plan, acquire, and construct transportation and transit infrastructure that is not necessarily viewed as consistent or compatible with cross-community needs or priorities.

It is no surprise that some of these initiatives are easily discounted and discredited by groups of concerned citizens that have their focus both on local impacts and broader regional priorities.

The failed Douglas Street centre-of-street busway attempted to serve the dual and contradictory purpose of being both an express inter-urban commuter service to and from Langford and also a local community-friendly "transit spine" for Victoria's northerly downtown expansion. It could do neither job, and instead would have inflicted great economic harm and job losses on the community.

The inter-urban busway also focused on the dated notion of the external communities being "bedroom" communities of Victoria with a need to accommodate large increases in commuter travel to and from Victoria. This is contrary to the CRD's "Complete Communities" strategy and contrary to how mixed development is now occurring.

The failure to recognize the E&N rail corridor as a viable and important "green" transportation link is an opportunity that has not been properly explored. The E&N route represents one third of the commute time of the Douglas / Highway 1 corridor to Langford and could be built at a small fraction of the close to billion dollar cost of an LRT line to Langford via Douglas Street.

Additionally, there is a risk that the Douglas Street Highway 1 LRT alignment may not be accepted as a long term goal of the Westshore Communities in their development plans. It would make little sense to focus new development on the congested Highway 1 corridor, with the "green" E&N corridor already available within these communities.


There are no simplistic solutions to managing travel demand. Simply increasing transit service does not mean that people will use it. Travel between home, work, schools, shopping and other destinations cannot be reduced without a comprehensive approach. Some hard choices will have to be made. But all choices must be "on the table" and all the choices that are made should produce results - not promises, or worse - a series of unintended consequences.

The citizen's of the CRD cannot afford expensive mistakes that divert financial resources away from projects that can be effective in achieving transportation goals or that divert assessment-based funding away from local municipal needs.

We need a real Capital Region transportation plan. A plan that is developed by an independent body that will place all the viable options in front of the public for full scrutiny and input prior to acceptance and approval.

We therefore call upon the capital region district communities to work together and jointly develop a coordinated Capital Region District transportation plan that emphasizes green, practical, and workable solutions to the region's transportation issues.

Yours truly



Randy Northey, Chairman
Association of Douglas Street Businesses

May 13, 2008

Mayor and Council
City of Langford
By e-mail

Your Worship and Honoured Council Members,

Re: Protective zoning for public school lands

I ask leave to address you at your public meeting on Tuesday May 20, 2008. I am a member of an organization dedicated to stopping the sale of public school lands across British Columbia. Our guiding principle is that public education is the foundation of our society, our economy and our democracy.

As you may know, the Ministry of Education requires schools districts to bring money to the table if they want funding for capital projects like new schools or seismic upgrades. The Provincial government is, in effect, forcing the sale of the school lands. Desperate for cash to keep schools safe for students, the districts are selecting schools for sale which will be needed by future generations of public school children.

Declining enrolment has been grossly exaggerated by the Ministry and will be over soon. Meanwhile, schools like Belmont, which represent the last green-space in high-density areas, are being marketed for development. This green space is especially vital to a high density community, particularly as we pass peak oil and people live and work closer to home. Belmont is an excellent example of a property that will be needed as a walking-distance school and local green space for the residents of Langford's high density areas. Indeed, the more development, the greater the need for public schools will be.

I beg leave to come to council and introduce you to some innovative by-laws passed by such municipalities as Oak Bay and Cowichan Regional district. These far-sighted communities have shown real leadership in protecting school lands for future generations.

Thank you for considering my request.

Yours truly,

Jessica Van der Veen
LANDS (Let's Agree Not to Dispose of Schools)

**CITY OF LANGFORD
Recommendation**

Refer to:

- Receive & File
- Admin Finance Comm
- Parks & Rec Committee
- P & Z Committee
- Protective Services Comm
- T & F Committee
- Clerk Administrator
- City Engineer
- City Planner
- Senior Bylaw Officer

SENIOR BYLAW OFFICER FOR [unclear]

6630-01 OCP SICKLE

Lindy Kaercher

From: Pamela Fraser [zippycat@telus.net]
Sent: May 14, 2008 11:59 AM
To: Lindy Kaercher
Cc: Julie Coneybeer; adminassist@cityoflangford.ca; Wendy Hobbs; Pam Fraser (E-mail); ifraser@telus.net
Subject: URGENT- Ian Fraser, Request for Council meeting, May 20th, 2008
Importance: High

May 14th, 2008

Langford Mayor and Council
City of Langford
By email:

c/o Ms. Lindy Kaercher, City of Langford.
From: Ian Fraser - ifraser@telus.net cc: zippyfraser@telus.net

Your Worship and Honoured Council Members,
RE: Protective zoning for public school lands, SD62

CITY OF LANGFORD Recommendation	
Refer to:	
<input type="checkbox"/>	Receive & File
<input type="checkbox"/>	Admin Finance Comm
<input type="checkbox"/>	Parks & Rec Committee
<input type="checkbox"/>	P & Z Committee
<input type="checkbox"/>	Protective Services Comm
<input type="checkbox"/>	T & P Committee
<input type="checkbox"/>	Clerk Administrator
<input type="checkbox"/>	City Engineer
<input type="checkbox"/>	City Planner
<input type="checkbox"/>	Senior Bylaw Officer

I am requesting permission to address you at the public meeting, on Tuesday May 20th, 2008. I have grown up in Langford and am a resident. My child currently attends Glen Lake School, which will be closing this June. I would like to address my concerns about the importance of our community schools, and our publicly-funded school land properties. I believe that it should be a long-term goal for our Langford municipality and leaders (and municipalities in BC) to provide and mandate zoning protection for school land and properties. The reduction of public lands & neighbourhood school recreational greenspaces is a huge concern for our growing, compact communities and future generations. I believe that a new by-law should be set in place -protecting these crucial spaces. This is essential to give a municipality more control over the usage of land if schools are closed. I feel my comments are pertinent to the OCP hearing since this topic goes hand-in-hand with the planned growth in our area.

Please consider my request and thank you for your time,

Regards,

Ian Fraser
994 Springhill Rd., Langford V9B 4K4
479-0778 c:516-8014

May 2, 2008

MAY 2 2008

Attn: Mayor and Council
City of Langford

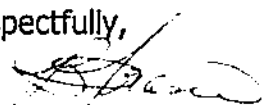
As a resident and tax payer in the City of Langford I would appreciate consideration of a new City bylaw regarding the free run of cats in our city.

I am the owner of two licensed dogs; we pick up after both dogs, and leash them in public spaces as required.

My address is 2387 Selwyn Road. Our home is located next to an open park area that has been equipped with one of the new bag stands for dogs. (Excellent) We keep up the park area directly beside our home by mowing and trimming, watering and picking up after the odd person who is too lazy to use a dog bag, but over all dog owners are using the system effectively. Now we come to the cat issue... I have picked up 3 - 4 litre pails of cat litter so far this spring. The flower beds throughout our yard and in the park area are oversized cat litter boxes. The cat owners on our street let them run free day and night. Not only are they leaving behind their infectious mess throughout the neighborhood, but they are constantly sitting on my decks and front step as well as fighting and growling in the early hours of the morning with other cats. We like to sleep with our window open for fresh air not to listen to fighting cats at 3 am. Two of the new shrubs I planted this year look like a tornado hit them. I've discovered the cats are using them as scratching posts. Cat owners need to be held accountable. Hopefully I've made my point!

Thank you for addressing this issue. I look forward to the inception of a cat bylaw for the City of Langford which will be enforced.

Respectfully,



Bonnie Fraser
2387 Selwyn Road
Langford V9B 6L8

478-6447

CITY OF LANGFORD Recommendation	
Refer to:	
<input type="checkbox"/>	Receive & File
<input type="checkbox"/>	Admin Finance Comm
<input type="checkbox"/>	Parks & Rec Committee
<input type="checkbox"/>	P & Z Committee
<input type="checkbox"/>	Protective Services Comm
<input type="checkbox"/>	T & P Committee
<input type="checkbox"/>	Clerk Administrator
<input type="checkbox"/>	City Engineer
<input type="checkbox"/>	City Planner
<input type="checkbox"/>	Senior Bylaw Officer
<input checked="" type="checkbox"/>	SICA For comment

CITY OF LANGFORD
Recommendation

Refer to:

- Receive & File
- Admin Finance Comm
- Parks & Rec Committee
- P & Z Committee
- Protective Services Comm
- T & P Committee
- Clerk Administrator
- City Engineer
- City Planner
- Senior Bylaw Officer



28 April 2008

The Honourable Mayor Stewart Young
Municipality of Langford
2nd Floor, 877 Goldstream Avenue
Victoria, BC

Dear Sir,

Capital Families is writing you today with a simple request.

The first annual Mayor's Golf Challenge (was it 1998 or 1999?) donated over \$10,000 to the child and youth programs at Capital Families Association. It was a true godsend at the time.

We would like to be considered this year, 2008, as a recipient of the Golf Tournament proceeds. Last year was a difficult financial year for Capital Families and this upcoming fiscal year will be even more daunting. Overhead expenses here on Wale Road have more than doubled and the funding just simply does not meet the expenses.

Programs for children, youth and families are full and reports from parents indicate objectives are being met and families' resiliency is being strengthened. Over 60% of the families we see are residents of Langford – and are proud of it!

So, again, our request is simple – we need funding support to continue to offer the high quality programs that the community relies on. A brochure is enclosed that outlines the current work of Capital Families. I would be happy to discuss our programs and our funding if so desired.

Yours truly,

Prairie Escallier
Executive Director

CITY OF LANGFORD

BYLAW NO. 1157

A BYLAW TO AMEND BYLAW NO. 300, LANGFORD ZONING BYLAW, 1999"

The Council of the City of Langford, in open meeting assembled, hereby enacts as follows:

A. Langford Zoning Bylaw No. 300, 1999 is amended as follows:

1. By deleting from the R1 (ONE-FAMILY RESIDENTIAL) ZONE and adding to the MU1A (MIXED USE RESIDENTIAL COMMERCIAL A) ZONE the property legally described as Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.
2. By adding to the list of permitted uses in Section 6.51.01A in appropriate alphabetical order the words "townhouses, on land legally described as Lot 6 Section 5 Esquimalt District Plan 8120" and renumbering the following subsection accordingly.
3. By replacing the words "the floor area ratio may not exceed 2.0" in Section 6.51.04A(1) with the words "not more than one dwelling unit per 550 m² of lot area may be constructed on any lot and the floor area ratio for all uses may not exceed 0.3" and by replacing the words "the minimum floor area ratio may be increased to 2.0" in Section 6.51.04A(3) with the words "the density of development shall be regulated by floor area ratio only and the maximum floor area ratio is 2.0".
4. By adding the following paragraphs to Section 6.51.04A(3) in alphabetical order:
 - (j) pays to the City the amount specified in Column 11 of Table 1 below at time of building permit, to be deposited in the School Crossing Guards Reserve Fund;
 - (k) pays to the City the amount specified in Column 12 of Table 1 below at time of building permit, to be deposited in the Affordable Housing Reserve Fund;
 - (l) pays to the City the amount specified in Column 13 of Table 1 below at the time of building permit, to be deposited in the Equipment Replacement Reserve Fund.

A. By adding to Table 1 in the section 6.51.04(A)

Table 1

1. Legal description	2.	3.	4.	5.	6.	7.	8.	9.	10.	11.	12.	13
Lot 6, Section 5, Esquimalt District, Plan 8120 (2710 Peatt Road)	\$500 per dwelling unit	\$100 per dwelling unit	\$0	\$1,000 per dwelling unit	\$0	\$300 per dwelling unit	\$0	\$0	\$0	\$100 per dwelling unit	\$500 per dwelling unit	\$200 per dwelling unit

B. This Bylaw may be cited for all purposes as "Langford Zoning Bylaw, Amendment No. 229, (2710 Peatt Road), 2008".

READ A FIRST TIME this 21ST day of April, 2008.

PUBLIC HEARING held this day of, 2008.

READ A SECOND TIME this day of, 2008.

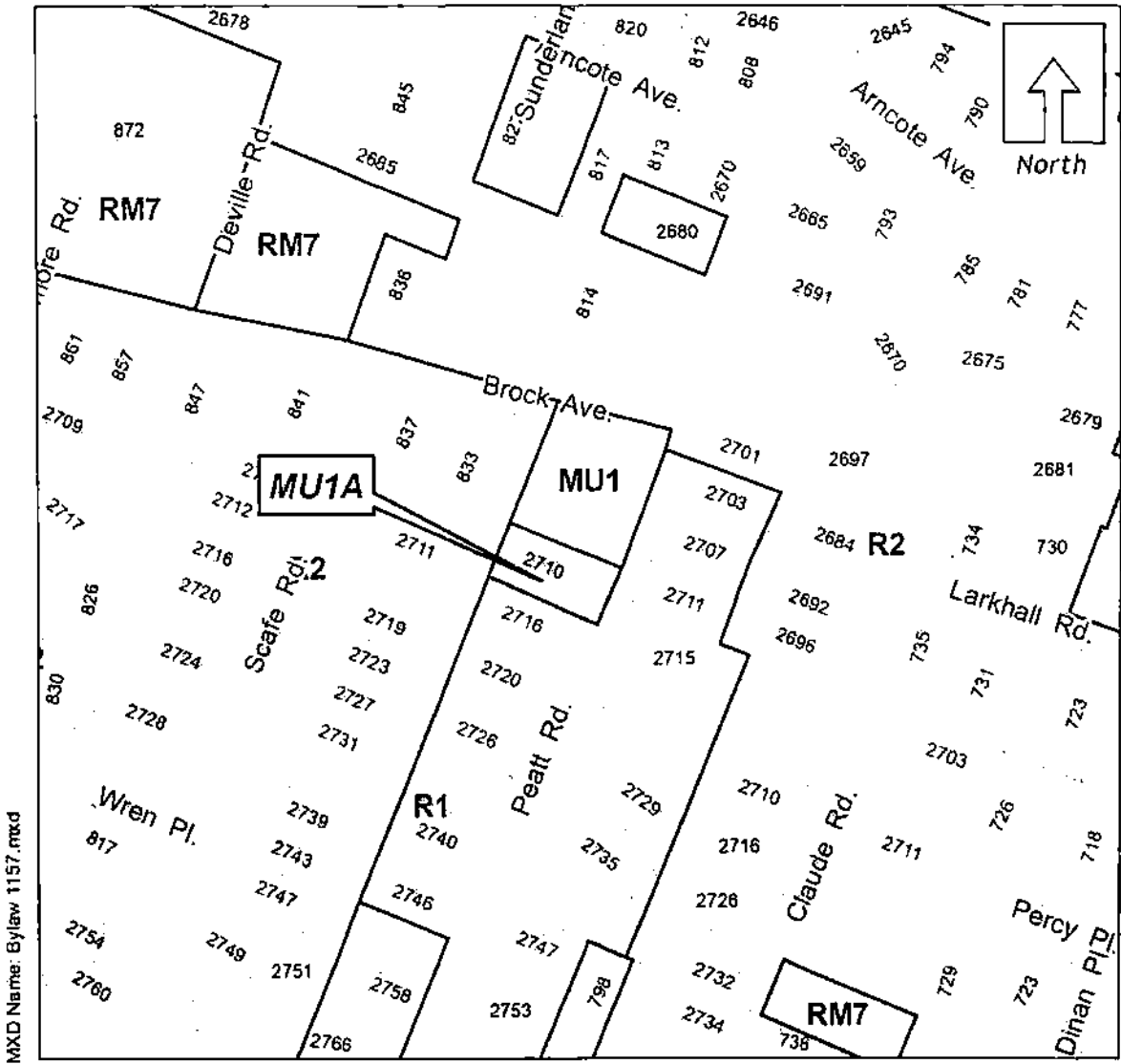
READ A THIRD TIME this day of, 2008.

APPROVED BY THE MINISTRY OF TRANSPORTATION this day of, 2008.

ADOPTED this day of, 2008.

MAYOR

CLERK-ADMINISTRATOR



MXD Name: Bylaw 1157.mxd

LEGAL DESCRIPTION: Lot 6, Section 5, Esquimalt District, Plan 8120; as shown shaded on Plan No. 1 attached to and forming part of this Bylaw.

I HERBY CERTIFY THIS TO BE A TRUE COPY
OF PLAN No. 1 AS DESCRIBED
IN SECTION ____ OF BYLAW No. 1157

MAYOR

ADOPTION: _____

CLERK - ADMINISTRATOR

Scale: N T S

Last Revised: February 06, 2003

CITY OF LANGFORD

BYLAW NO. 1165

A BYLAW TO IMPOSE A PARCEL TAX TO PROVIDE FUNDING FOR LANGFORD DRAINAGE SYSTEM SPECIFIED AREA NO. 1.

WHEREAS City of Langford has adopted the Langford Drainage System Specified Area No. 1 Construction and Loan Authorization Bylaw No. 256, 1998

AND WHEREAS certain costs have been incurred by the City of Langford in providing drainage services to lands within Langford Drainage System Specified Area No. 1:

AND WHEREAS it is deemed desirable and expedient to impose and levy a parcel tax on land benefiting from such drainage service to meet such costs:

NOW THEREFORE: the Council of the City of Langford in open meeting assembled enacts as follows:

1. In this Bylaw, unless the context otherwise requires:

"Collector" means the person appointed by Council resolution under the Community Charter;

"Taxable frontage" means the distance which a parcel of land is deemed to abut on the drainage or highway, and in respect of which parcel the frontage tax is levied for the drainage service;

"Total taxable frontage" means the sum of the taxable frontage of the parcels of land, which abut or are deemed to abut on the drain or highway;

"Municipality" means City of Langford.

2. A tax shall be and is hereby imposed upon the owners of land or real property within Langford Drainage System Specified Area No. 1, the aforesaid tax to be hereinafter referred to as the "Parcel Tax".
3.
 - (1) The Parcel Tax shall be levied in each year on each parcel of land aforementioned, and the amount thereof, except as otherwise provided in this bylaw, will be the product of the taxable frontage and the annual rate.
 - (2) The Parcel Tax shall be in force and in effect until the complete discharge and satisfaction by the municipality of all obligations presently incurred, and to be incurred, in respect of the aforesaid service.
 - (3) The annual rate shall be \$6.80 per metre of taxable frontage.

4. For the purposes of this bylaw, a regularly shaped parcel of land is rectangular and has an area of 835 square metres or less.
5. (1) To place the Parcel Tax on a fair and equitable basis, the taxable frontage of the following parcels of land shall be the number of metres fixed by the Collector:
 - a) a triangular or irregularly shaped parcel of land;
 - b) a parcel of land wholly or in part unfit for building purposes;
 - c) a parcel of land which does not abut on the drain but is nevertheless deemed to abut on the drain;
 - d) a parcel of land which not only abuts on the drain area, but is also traversed by the drain;
 - e) a parcel of land situated at the junction or intersection of highways such that the drain service abuts more than one side of the parcel;
 - f) a parcel of land, the front and rear boundaries of which each abuts on the drain.
- (2) The Collector, in fixing the taxable frontage under subsection (1) shall have due regard to:
 - (a) the condition, situation, value and superficial area of the parcel as compared with other parcels of land; and
 - (b) the benefit derived from the drainage services
6. For the purposes of this bylaw:
 - (1) All rectangular parcels of land with an area of 835 square metres or less shall be deemed to have a taxable frontage of 16 metres.
 - (2) All rectangular parcels of land with an area of more than 835 square be deemed to have a taxable frontage that is the result of dividing the area of the parcel by 835 square metres multiplied by 16 metres.
 - (3) As an exception to the taxable frontage calculated in this section, all rectangular parcels of land that are zoned commercial, industrial or business park shall have a taxable frontage equal to the amount determined in 6(1) or 6(2) multiplied by 1.20833333 and all rectangular properties with multi-family zoning shall have a taxable frontage that is the maximum number of residential units permitted by the Langford Zoning Bylaw multiplied by 12 metres per unit.
8. "Langford Drainage Specified Area No. 1 Parcel Tax Bylaw No. 576, 2001" is hereby rescinded.
9. This bylaw may be cited as "Langford Drainage Specified Area No. 1 Parcel Tax Bylaw No. 1165, 2008"

READ A FIRST TIME this day of , 2008.

READ A SECOND TIME this day of , 2008.

READ A THIRD TIME this day of , 2008.

ADOPTED this day of , 2008.

MAYOR

CLERK ADMINISTRATOR

CITY OF LANGFORD

BYLAW NO. 1166

A BYLAW TO IMPOSE A PARCEL TAX ON OWNERS OF LAND WITHIN SEWER SPECIFIED AREA NO. 1A

WHEREAS City of Langford has adopted the Langford Sanitary Sewer Collection System Specified Area No. 1A Construction and Loan Authorization Bylaw No. 659, 2002:

AND WHEREAS certain costs have been incurred by the City of Langford in providing sewer services to lands within its boundaries:

AND WHEREAS it is deemed desirable and expedient to impose and levy a parcel tax on land benefiting from such sewer service to meet such costs:

NOW THEREFORE: the Council of the City of Langford in open meeting assembled enacts as follows:

1. In this Bylaw, unless the context otherwise requires:

“Collector” means the person appointed by Council under the Community Charter;

“Taxable frontage” means the distance that a parcel of land is deemed to abut on the sewer or highway, and in respect of which parcel the parcel tax is levied for the sewer service;

“Total taxable frontage” means the sum of the taxable frontage of the parcels of land which abut or are deemed to abut on the sewer or highway;

“Municipality” means City of Langford.

2. A tax shall be and is hereby imposed upon the owners of land or real property within Sewer Specified Area No. 1A, the aforesaid tax to be hereinafter referred to as the Parcel Tax.
3.
 - (1) The Parcel Tax shall be levied in each year on each parcel of land aforementioned, and the amount thereof, except as otherwise provided in this bylaw, will be the product of the taxable frontage as authenticated by the Parcel Tax Review Panel for Sewer Specified Area No. 1A and the annual rate.
 - (2) The Parcel Tax shall be in force and in effect until the complete discharge and satisfaction by the municipality of all obligations presently incurred, and to be incurred, in respect of the aforesaid service.

- (3) The annual rate shall be \$17.00 per metre of taxable frontage.
4. For the purposes of this bylaw, a regularly shaped parcel of land is rectangular and has an area of 835 square metres or less.
5. (1) To place the Parcel Tax on a fair and equitable basis, the taxable frontage of the following parcels of land shall be the number of metres fixed by the Collector:
 - a) a triangular or irregularly shaped parcel of land;
 - b) a parcel of land wholly or in part unfit for building purposes;
 - c) a parcel of land which does not abut on the sewer but is nevertheless deemed to abut on the sewer;
 - d) a parcel of land which not only abuts on the sewer area, but is also traversed by the sewer;
 - e) a parcel of land situated at the junction or intersection of highways such that the sewer service abuts more than one side of the parcel;
 - f) a parcel of land, the front and rear boundaries of which each abuts on the sewer.
- (2) The Collector, in fixing the taxable frontage under subsection (1) shall have due regard to:
 - (a) the condition, situation, value and superficial area of the parcel as compared with other parcels of land; and
 - (b) the benefit derived from the sewer services.
6. For the purposes of this bylaw:
 - (1) All rectangular parcels of land with an area of 835 square metres or less shall be deemed to have a taxable frontage of 16 metres.
 - (2) All rectangular parcels of land with an area greater than 835 square metres but less than 1,390 square metres shall be deemed to have a taxable frontage of 24 metres.
 - (3) All rectangular parcels of land with an area of 1,390 square metres or more shall be deemed to have a taxable frontage that is the result of dividing the area of the parcel by 695 square metres multiplied by 16 metres.
 - (4) As an exception to the taxable frontage calculated in this section, all rectangular parcels of land that are zoned commercial, industrial or business park shall have a taxable frontage equal to the amount determined in 7(3) multiplied by 2.58 and all rectangular properties with multi-family zoning shall have a taxable frontage that is the maximum number of residential units permitted by the Langford Zoning Bylaw multiplied by 12 metres per unit.

7. Langford Sewer Specified Area No. 1A Parcel Tax Bylaw No. 764, 2003 is hereby rescinded
8. This bylaw may be cited as "Langford Sewer Specified Area No. 1A Parcel Tax Bylaw No. 1166, 2008"

READ A FIRST TIME this day of, 2008.

READ A SECOND TIME this day of, 2008.

READ A THIRD TIME this day of, 2008.

ADOPTED this day of, 2008.

MAYOR

CLERK-ADMINISTRATOR

CITY OF LANGFORD

BYLAW NO. 1167

A BYLAW TO IMPOSE A PARCEL TAX ON OWNERS OF LAND WITHIN SEWER SPECIFIED AREA NO. 10B

WHEREAS City of Langford has adopted the Langford Sanitary Sewer Collection System Specified Area No. 10B (Galloping Goose) Construction and Loan Authorization Bylaw No. 715, 2003:

AND WHEREAS certain costs have been incurred by the City of Langford in providing sewer services to lands within its boundaries:

AND WHEREAS it is deemed desirable and expedient to impose and levy a parcel tax on land benefiting from such sewer service to meet such costs:

NOW THEREFORE: the Council of the City of Langford in open meeting assembled enacts as follows:

1. In this Bylaw, unless the context otherwise requires:

“Collector” means the person appointed by Council under the Community Charter;

“Taxable frontage” means the distance that a parcel of land is deemed to abut on the sewer or highway, and in respect of which parcel the parcel tax is levied for the sewer service;

“Total taxable frontage” means the sum of the taxable frontage of the parcels of land which abut or are deemed to abut on the sewer or highway;

“Municipality” means City of Langford.

2. A tax shall be and is hereby imposed upon the owners of land or real property within Sewer Specified Area No. 10B, the aforesaid tax to be hereinafter referred to as the Parcel Tax.
3. (1) The Parcel Tax shall be levied in each year on each parcel of land aforementioned, and the amount thereof, except as otherwise provided in this bylaw, will be the product of the taxable frontage as authenticated by the Parcel Tax Roll Review Panel for Sewer Specified Area No. 10B and the annual rate.
(2) The Parcel Tax shall be in force and in effect until the complete discharge and satisfaction by the municipality of all obligations presently incurred, and to be incurred, in respect of the aforesaid service.

- (3) The annual rate shall be \$3.90 per metre of taxable frontage.
4. For the purposes of this bylaw, a regularly shaped parcel of land is rectangular and has an area of 835 square metres or less.
5. (1) To place the Parcel Tax on a fair and equitable basis, the taxable frontage of the following parcels of land shall be the number of metres fixed by the Collector:
 - a) a triangular or irregularly shaped parcel of land;
 - b) a parcel of land wholly or in part unfit for building purposes;
 - c) a parcel of land which does not abut on the sewer but is nevertheless deemed to abut on the sewer;
 - d) a parcel of land which not only abuts on the sewer area, but is also traversed by the sewer;
 - e) a parcel of land situated at the junction or intersection of highways such that the sewer service abuts more than one side of the parcel;
 - f) a parcel of land, the front and rear boundaries of which each abuts on the sewer.
- (2) The Collector, in fixing the taxable frontage under subsection (1) shall have due regard to:
 - (a) the condition, situation, value and superficial area of the parcel as compared with other parcels of land; and
 - (b) the benefit derived from the sewer services.
6. For the purposes of this bylaw:
 - (1) All rectangular parcels of land with an area of 835 square metres or less shall be deemed to have a taxable frontage of 16 metres.
 - (2) All rectangular parcels of land with an area greater than 835 square metres but less than 1,390 square metres shall be deemed to have a taxable frontage of 24 metres.
 - (3) All rectangular parcels of land with an area of 1,390 square metres or more shall be deemed to have a taxable frontage that is the result of dividing the area of the parcel by 695 square metres multiplied by 16 metres.
 - (4) As an exception to the taxable frontage calculated in this section, all rectangular parcels of land that are zoned commercial, industrial or business park shall have a taxable frontage equal to the amount determined in 7(3) multiplied by 2.58 and all rectangular properties with multi-family zoning shall have a taxable frontage that is the maximum number of residential units permitted by the Langford Zoning Bylaw multiplied by 12 metres per unit.

7. "Langford Sewer Specified Area No. 10B Parcel Tax Bylaw No. 949, 2005" is repealed.
8. This bylaw may be cited as "Langford Sewer Specified Area No. 10B Parcel Tax Bylaw No. 1167, 2008"

READ A FIRST TIME this day of , 2008.

READ A SECOND TIME this day of , 2008.

READ A THIRD TIME this day of , 2008.

ADOPTED this day of , 2008.

MAYOR

CLERK-ADMINISTRATOR

CITY OF LANGFORD

BYLAW NO. 1168

A BYLAW TO IMPOSE A PARCEL TAX ON OWNERS OF LAND WITHIN SEWER SPECIFIED AREA NO. 10C

WHEREAS City of Langford has adopted the Langford Sanitary Sewer Collection System Specified Area No. 10C (Sooke Road) Construction and Loan Authorization Bylaw No. 716, 2003:

AND WHEREAS certain costs have been incurred by the City of Langford in providing sewer services to lands within its boundaries:

AND WHEREAS it is deemed desirable and expedient to impose and levy a parcel tax on land benefiting from such sewer service to meet such costs:

NOW THEREFORE: the Council of the City of Langford in open meeting assembled enacts as follows:

1. In this Bylaw, unless the context otherwise requires:

“Collector” means the person appointed by Council under the Community Charter;

“Taxable frontage” means the distance that a parcel of land is deemed to abut on the sewer or highway, and in respect of which parcel the parcel tax is levied for the sewer service;

“Total taxable frontage” means the sum of the taxable frontage of the parcels of land which abut or are deemed to abut on the sewer or highway;

“Municipality” means City of Langford.

2. A tax shall be and is hereby imposed upon the owners of land or real property within Sewer Specified Area No. 10C, the aforesaid tax to be hereinafter referred to as the Parcel Tax.
3.
 - (1) The Parcel Tax shall be levied in each year on each parcel of land aforementioned, and the amount thereof, except as otherwise provided in this bylaw, will be the product of the taxable frontage as authenticated by the Parcel Tax Roll Review Panel for Sewer Specified Area No. 10C and the annual rate.
 - (2) The Parcel Tax shall be in force and in effect until the complete discharge and satisfaction by the municipality of all obligations presently incurred, and to be incurred, in respect of the aforesaid service.

- (3) The annual rate shall be \$3.70 per metre of taxable frontage.
4. For the purposes of this bylaw, a regularly shaped parcel of land is rectangular and has an area of 835 square metres or less.
5. (1) To place the Parcel Tax on a fair and equitable basis, the taxable frontage of the following parcels of land shall be the number of metres fixed by the Collector:
 - a) a triangular or irregularly shaped parcel of land;
 - b) a parcel of land wholly or in part unfit for building purposes;
 - c) a parcel of land which does not abut on the sewer but is nevertheless deemed to abut on the sewer;
 - d) a parcel of land which not only abuts on the sewer area, but is also traversed by the sewer;
 - e) a parcel of land situated at the junction or intersection of highways such that the sewer service abuts more than one side of the parcel;
 - f) a parcel of land, the front and rear boundaries of which each abuts on the sewer.
- (2) The Collector, in fixing the taxable frontage under subsection (1) shall have due regard to:
 - (a) the condition, situation, value and superficial area of the parcel as compared with other parcels of land; and
 - (b) the benefit derived from the sewer services.
6. For the purposes of this bylaw:
 - (1) All rectangular parcels of land with an area of 835 square metres or less shall be deemed to have a taxable frontage of 16 metres.
 - (2) All rectangular parcels of land with an area greater than 835 square metres but less than 1,390 square metres shall be deemed to have a taxable frontage of 24 metres.
 - (3) All rectangular parcels of land with an area of 1,390 square metres or more shall be deemed to have a taxable frontage that is the result of dividing the area of the parcel by 695 square metres multiplied by 16 metres.
 - (4) As an exception to the taxable frontage calculated in this section, all rectangular parcels of land that are zoned commercial, industrial or business park shall have a taxable frontage equal to the amount determined in 7(3) multiplied by 2.58 and all rectangular properties with multi-family zoning shall have a taxable frontage that is the maximum number of residential units permitted by the Langford Zoning Bylaw multiplied by 12 metres per unit.

7. "Langford Sewer Specified Area No. 10C Parcel Tax Bylaw No. 950, 2005" is repealed.
8. This bylaw may be cited as "Langford Sewer Specified Area No. 10C Parcel Tax Bylaw No. 1168, 2008"

READ A FIRST TIME this day of , 2008.

READ A SECOND TIME this day of , 2008.

READ A THIRD TIME this day of , 2008.

ADOPTED this day of , 2008.

MAYOR

CLERK-ADMINISTRATOR

CITY OF LANGFORD

BYLAW NO. 1169

A BYLAW TO IMPOSE A PARCEL TAX ON OWNERS OF LAND WITHIN SEWER SPECIFIED AREA NO. 12

WHEREAS City of Langford has adopted the Langford Sanitary Sewer Collection System Specified Area No. 12 Construction and Loan Authorization Bylaw No. 709, 2002

AND WHEREAS certain costs have been incurred by the City of Langford in providing sewer services to lands within its boundaries:

AND WHEREAS it is deemed desirable and expedient to impose and levy a parcel tax on land benefiting from such sewer service to meet such costs:

NOW THEREFORE: the Council of the City of Langford in open meeting assembled enacts as follows:

1. In this Bylaw, unless the context otherwise requires:

"Collector" means the person appointed by Council under the Community Charter;

"Taxable frontage" means the distance that a parcel of land is deemed to abut on the sewer or highway, and in respect of which parcel the parcel tax is levied for the sewer service;

"Total taxable frontage" means the sum of the taxable frontage of the parcels of land which abut or are deemed to abut on the sewer or highway;

"Municipality" means City of Langford.

2. A tax shall be and is hereby imposed upon the owners of land or real property within Sewer Specified Area No. 12, the aforesaid tax to be hereinafter referred to as the Parcel Tax.
3.
 - (1) The Parcel Tax shall be levied in each year on each parcel of land aforementioned, and the amount thereof, except as otherwise provided in this bylaw, will be the product of the taxable frontage as authenticated by the Parcel Tax Roll Review Panel for Sewer Specified Area No. 12 and the annual rate.
 - (2) The Parcel Tax shall be in force and in effect until the complete discharge and satisfaction by the municipality of all obligations presently incurred, and to be incurred, in respect of the aforesaid service.
 - (3) The annual rate shall be \$20.50 per metre of taxable frontage.
4. For the purposes of this bylaw, a regularly shaped parcel of land is rectangular and has an area of 835 square metres or less.
5.
 - (1) To place the Parcel Tax on a fair and equitable basis, the taxable frontage of the following parcels of land shall be the number of metres fixed by the Collector:
 - a) a triangular or irregularly shaped parcel of land;
 - b) a parcel of land wholly or in part unfit for building purposes;
 - c) a parcel of land which does not abut on the sewer but is nevertheless deemed to abut on the sewer;
 - d) a parcel of land which not only abuts on the sewer area, but is also traversed by the sewer;

- e) a parcel of land situated at the junction or intersection of highways such that the sewer service abuts more than one side of the parcel;
 - f) a parcel of land, the front and rear boundaries of which each abuts on the sewer.
- (2) The Collector, in fixing the taxable frontage under subsection (1) shall have due regard to:
- (a) the condition, situation, value and superficial area of the parcel as compared with other parcels of land; and
 - (b) the benefit derived from the sewer services.
6. For the purposes of this bylaw:
- (1) All rectangular parcels of land with an area of 835 square metres or less shall be deemed to have a taxable frontage of 16 metres.
 - (2) All rectangular parcels of land with an area greater than 835 square metres but less than 1,390 square metres shall be deemed to have a taxable frontage of 24 metres.
 - (3) All rectangular parcels of land with an area of 1,390 square metres or more shall be deemed to have a taxable frontage that is the result of dividing the area of the parcel by 695 square metres multiplied by 16 metres.
 - (4) As an exception to the taxable frontage calculated in this section, all rectangular parcels of land that are zoned commercial, industrial or business park shall have a taxable frontage equal to the amount determined in 7(3) multiplied by 2.58 and all rectangular properties with multi-family zoning shall have a taxable frontage that is the maximum number of residential units permitted by the Langford Zoning Bylaw multiplied by 12 metres per unit.
7. "Langford Sewer Specified Area No. 12 Parcel Tax Bylaw No. 951, 2005" is repealed.
8. This bylaw may be cited as "Langford Sewer Specified Area No. 12 Parcel Tax Bylaw No. 1169, 2008"

READ A FIRST TIME this day of, 2008.

READ A SECOND TIME this day of, 2008.

READ A THIRD TIME this day of, 2008.

ADOPTED this day of, 2008.

MAYOR

CLERK-ADMINISTRATOR



Staff Report
to
Council

Inserted into Public
Hearing Package
May 9, 2008
W. Bluffe

Date: May 20, 2008
File: OCP-06-04
Subject: Langford 2008 Official Community Plan (OCP) – Bylaw 1200

Purpose

This report outlines the proposal from staff to change the OCP maps after the Public Hearing, to address a few mapping errors and to expand development permit areas in light of new information that has been provided to the municipality.

This report also serves to discuss comments on the OCP which were received by the public and referral agencies since first reading of the OCP.

Background

Council has given first reading to Bylaw 1200 on April 21, 2008. Public Hearing for this bylaw is being held in Council Chambers on May 20, 2008.

The City of Langford's 2006-2008 Official Community Plan review was executed jointly with the City of Colwood. This sub-regional planning exercise was a first in BC, and allowed numerous opportunities for creative integration, public feedback and genuine input.

An estimated 650 people from the Cities of Colwood and Langford provided input at six public events held between June 23rd, 2007 and Mar. 8th, 2008.

Each public event not only provided written feedback opportunities, but also focused on dialogue-based work groups where participants were provided the opportunity to shape the agenda, choose the topics they discussed and provide recommendations on those topics for the OCP review. An OCP Steering Committee was also struck that included representatives from Council, committees and citizens at large. Findings and public feedback summaries were provided online for public review for each event. The public was also presented with an extensive OCP Policy Options report (that contained a review of public concerns, analysis results and policy recommendations) and numerous draft OCPs online and at public events.

Additionally, the Langford public was invited to comment on the draft OCP at the Planning and Zoning Committee meeting held on March 31, 2008.

Comments

Mapping Errors:

Council may wish to amend the following mapping errors, which do not constitute a change in land use, and are permissible amendments following the close of Public Hearing:

1. Adding missing ALR parcels to 'Agricultural Strategy Lands' designation on Maps 2, 5 and 6:

Despite that Map 10: Agricultural Land Reserve (ALR) Inventory is correct, Map 2: Growth Management & Land Use Strategy, Map 5: Natural Setting, Parks & Open Spaces Inventory, and Map 6: Natural Setting, Parks & Open Spaces Integration Strategy all contain the same mapping error, such that not all ALR parcels are shown highlighted on the map. Council may wish to amend this mapping error for the sake of clarity, while noting that amending this mapping error does not affect land use of ALR parcels, as the management of ALR lands is fully regulated by the Province of British Columbia.

2. Adding Bilston Creek to Map 3: Sensitive Ecosystems & Habitat Areas:

Bilston Creek was identified in the former City of Langford (1996) Official Community Plan, and its lack of delineation in Map 3 of the current OCP was a mapping error.

3. Amending Steep Slopes development permit areas on Map 17:

The Steep Slopes development permit areas do not accurately reflect the Steep Slopes DP area mapping that Council adopted in Bylaw 302 (date).

Amended Maps 2, 3, 5, 6 and 17 have been added to this report as appendices, for Council's reference.

Mapping Additions:

Council may also wish to amend the Map 15: Areas with Potential Habitat & Biodiversity Values (attached) such that it includes additional Potential Habitat and Biodiversity areas. Council had created this new category of Development Permit area in 2005 to address the need to professional assess rural properties for their habitat and biodiversity value, when development or alteration of the land is under consideration. An outside agency has provided the City with a recently completed mapping inventory of Garry Oak and Associated Ecosystems. As this mapping inventory has not been verified by City staff or consultants, Council may wish to add the small portion of lands that is not already inside one of our Environmental Protection Development Permit areas into the Potential Habitat and Biodiversity area, so that a Registered Professional Biologist or other appropriate professional will be assessing the property's habitat and ecosystem health prior to development.

Comments from the Public:

Staff have received the following five comments from the public, noting that concerns from the public have been addressed where possible, and might be the result of misunderstanding in other instances.

1. Additional protection for lakeside neighbourhoods and riparian values

The new OCP expands Council policy around protection of shoreline areas.

Resident concerns raised in two separate petitions are also being addressed with a proposed rezoning of the Subdivision Lot Requirements section of the R1 (One-Family Residential) and R2 (One- and Two-Family Residential) zones, with respect only to lakefront parcels. Consideration of this report follows the Public Hearing OCP, at the 20 May 2008 Council meeting.

Consideration is also being given to amending the design guidelines for shoreline development in the Environmental Protection Area Development Permit area guidelines, which will occur concurrently with a broader review of design guidelines in the coming months.

2. Adequacy of timeline to review OCP

OCP policies, which derived from public meetings at the 2-day Futures Forum in June 2007, were first presented to stakeholder agencies and non-profit organizations at October 2007 meetings and to the general public at four (4) days of public events in November 2007. Changes to OCP policies have all been in keeping with the general themes of sustainability and smart growth, and have been in response to stakeholder and public feedback and recommendations. A Policy Options Report that fully outlines OCP policy directions was publically released in January 2008, and the introduction of Bylaw 1200 follows two previous public drafts of the OCP which were available for review on the City of Langford website, at the 8 March 2008 Open House, and the 31 March 2008 Planning and Zoning Committee meeting.

OCP policies on encouragement of mixed use where appropriate is in keeping with sustainability principles espoused by provincial ministries, regional authorities, and non-profit organizations such as Smart Growth BC; as well as with all drafts of OCP documents. As a high-level strategy or goal, this policy will inform the staff, public and decision-making bodies, without hampering their ability to weigh each rezoning proposal on its own merits.

3. ALR parcels mapping error

This mapping error was brought to the attention of the City of Langford by a concerned resident. Staff have instructed OCP planning consultants to amend the mapping error, for Council's consideration following Public Hearing.

4. Notification requirements

A couple of City of Langford residents have noted a concern that the new OCP will allow new permitted uses in their neighbourhoods, without notification. Council will note that full

notification and public engagement is required by the British Columbia Local Government Act and by City of Langford procedures policy, whether or not a rezoning application is accompanied by an OCP amendment. No new uses can be permitted in any zone under the jurisdiction of the City of Langford without a rezoning process.

Council may also wish to note that the City of Langford does not have the issue of "pre-zoned" lands, in which development occurs with no public notification or review process, which has been an issue of concern in some other BC municipalities.

5. Advertising of Public Hearing

In addition to all of the requirements of the British Columbia Local Government Act and by City of Langford procedures policies, a Public Hearing notice and separate "notice board" (including copies of OCP) have been made available for review on the City of Langford website; extra and larger notices have been placed in the paper; and notice signs have been erected throughout the community.

Comments from Referral Agencies:

Some agencies have applauded the City of Langford OCP for its focus on sustainability and the measures that have been taken to protect environmentally sensitive areas. Some comments relate to Council policy which may be required, as a follow-up to the completion of the OCP review process, that addresses how agencies can work together to implement OCP policies.

Some small housekeeping items and requested additions (such as CRD's request that we add descriptions of regional parks and describe their benefits to residents) can occur when City of Langford amends the OCP in future to meet the coming Provincial requirements to add GHG emissions targets to the OCP (Bill 27).

Other referral agency responses are addressed below:

1. Town of View Royal:

While a Core Municipality, the Town of View Royal looks forward to working closely with the City of Langford on integrated transportation, economic development, and other initiatives.

2. Tsawout First Nation:

The Tsawout First Nation has a concern that the five Saanich First Nations, which have Indian Reserve #13 in common, have not been consulted in regards to the designation of their lands in the Official Community Plan. Council will note that none of the maps or policies in the new OCP change or in any way address the land use of I.R. #13. As neighbouring governing bodies, the Tsawout, Tsartlip, Pauquachin, Tseycum and Malahat First Nations have received Referrals on the draft and final OCP's, at the same time as the District of Metchosin, the Town of View Royal, the City of Colwood, the District of the Highlands, and the Capital Regional District.

3. District of Metchosin:

The mapping error with regard to the inclusion of the Bilston Creek in OCP mapping has been addressed.

Edge planning has been incorporated into the Official Community Plan as a guiding principle, but will not affect current zoning and land use, nor the ability of the City of Langford to require clustering of housing density, where appropriate, when the goal of ecological preservation can be achieved.

4. Capital Regional District (CRD):

The recommendations from the CRD to make some small changes to the labeling on maps has been incorporated where possible and other small suggestions can be easily incorporated with other small housekeeping items when the City of Langford amends the OCP in future to meet the coming Provincial requirements of Bill 27, introduced recently to the legislature by Ida Chong.

5. BC Transit

All of BC Transit's suggestions have been incorporated into the OCP in Bylaw 1200.

6. Garry Oak Ecosystem and Recovery Team (GOERT)

Council may wish to include Garry Oak and associated ecosystems mapping which has been provided to the City into its Potential Habitat and Biodiversity development permit area.

Other suggestions from GOERT's consultant are being incorporated into the draft amended design guidelines where possible. The amended design guidelines will be brought before the Planning and Zoning Committee and Council the near future, after completion of an internal staff review.

Options


That Council:

1. Give Bylaw No. 1200 amended first reading, second reading and third reading;

OR


2. Take no action with respect to the Official Community Plan at this time.

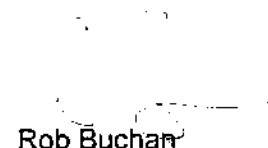

Matthew Baldwin, MCIP
City Planner


Emilie K. Adin
Deputy City Planner

Bob Beckett
Fire Chief

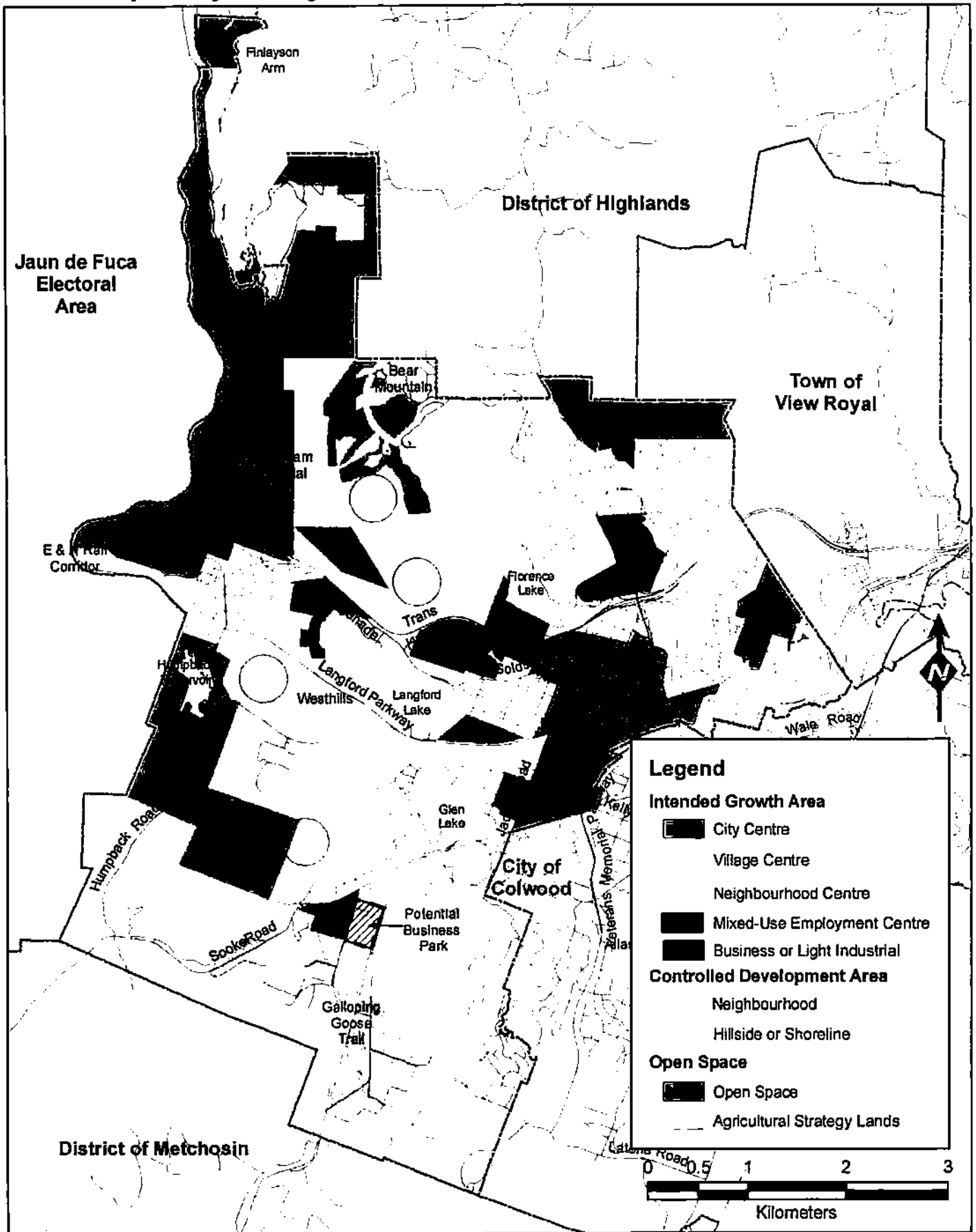
John Manson, P.Eng.
City Engineer


Steve Ternent
Treasurer

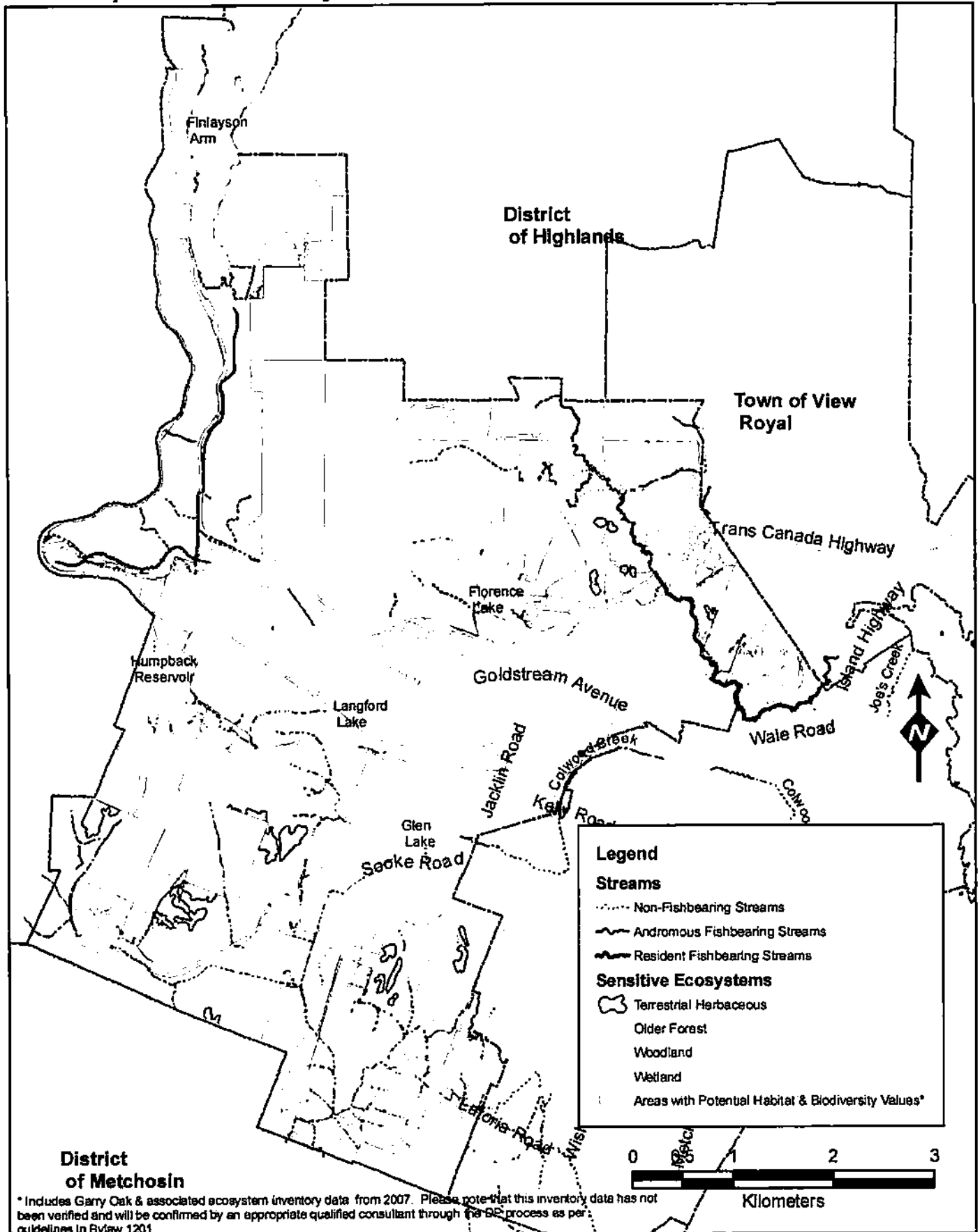

Rob Buchan
Clerk-Administrator

eka

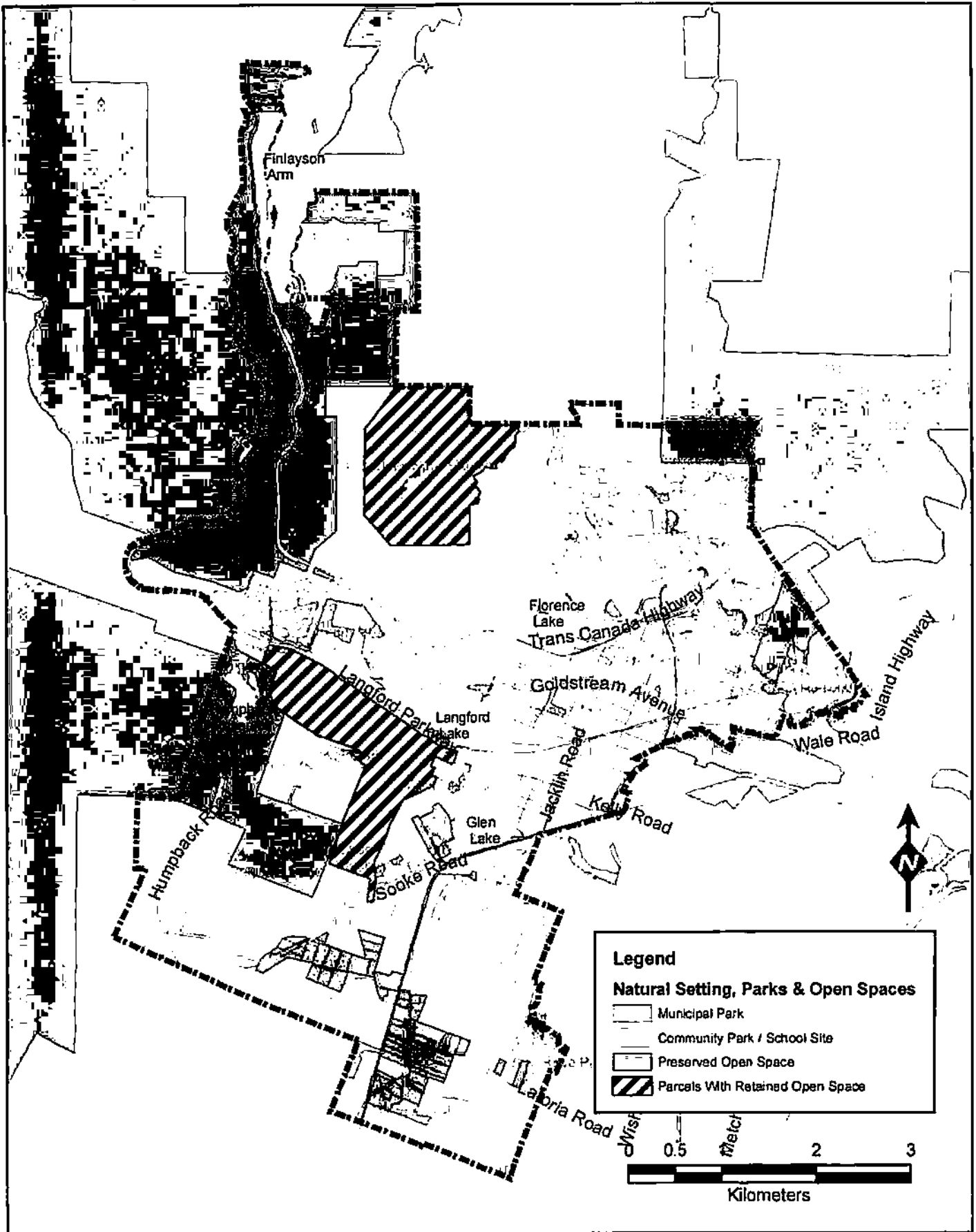
Map 2: City of Langford – Growth Management & Land Use Strategy



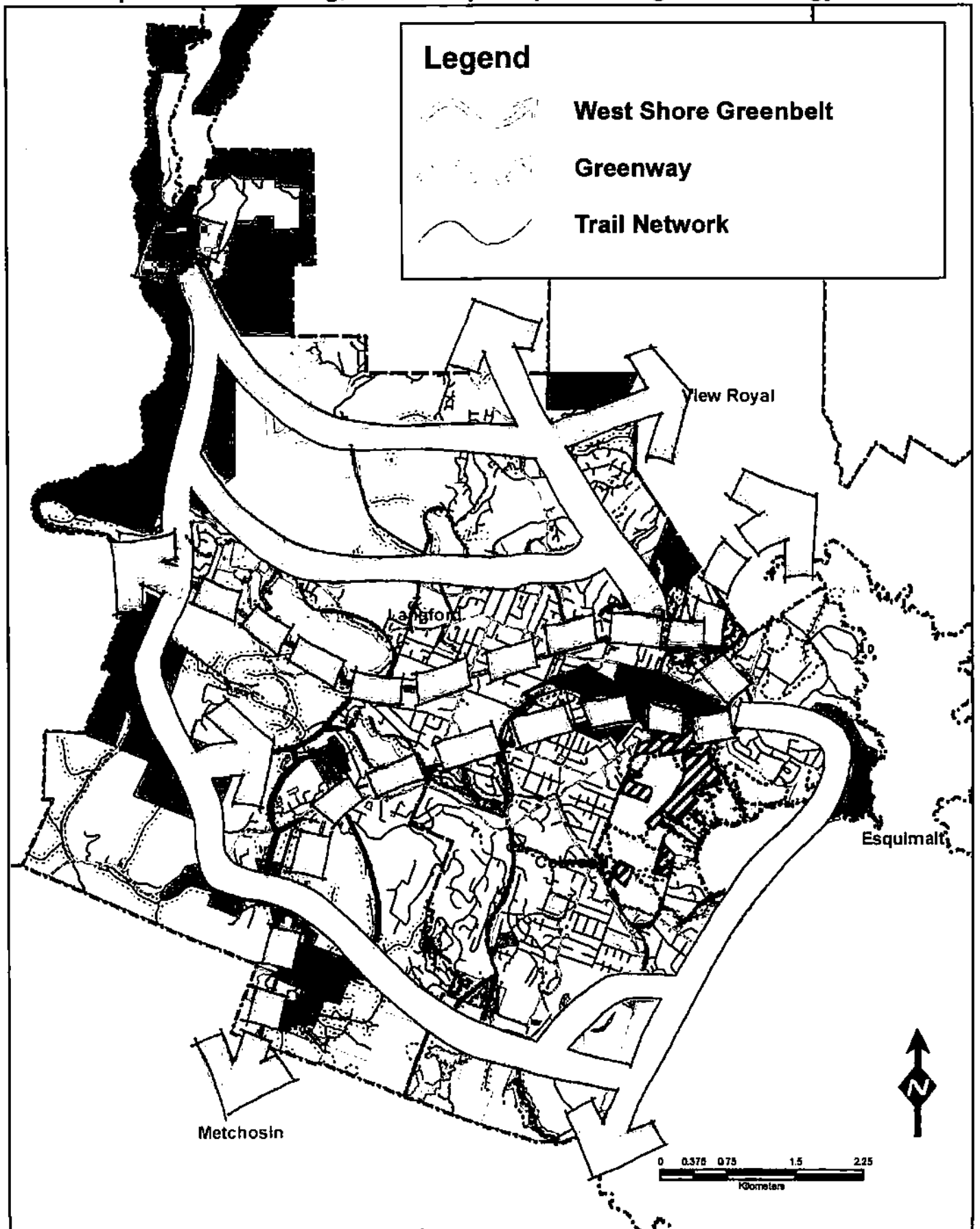
Map 3: Sensitive Ecosystems & Habitat Areas



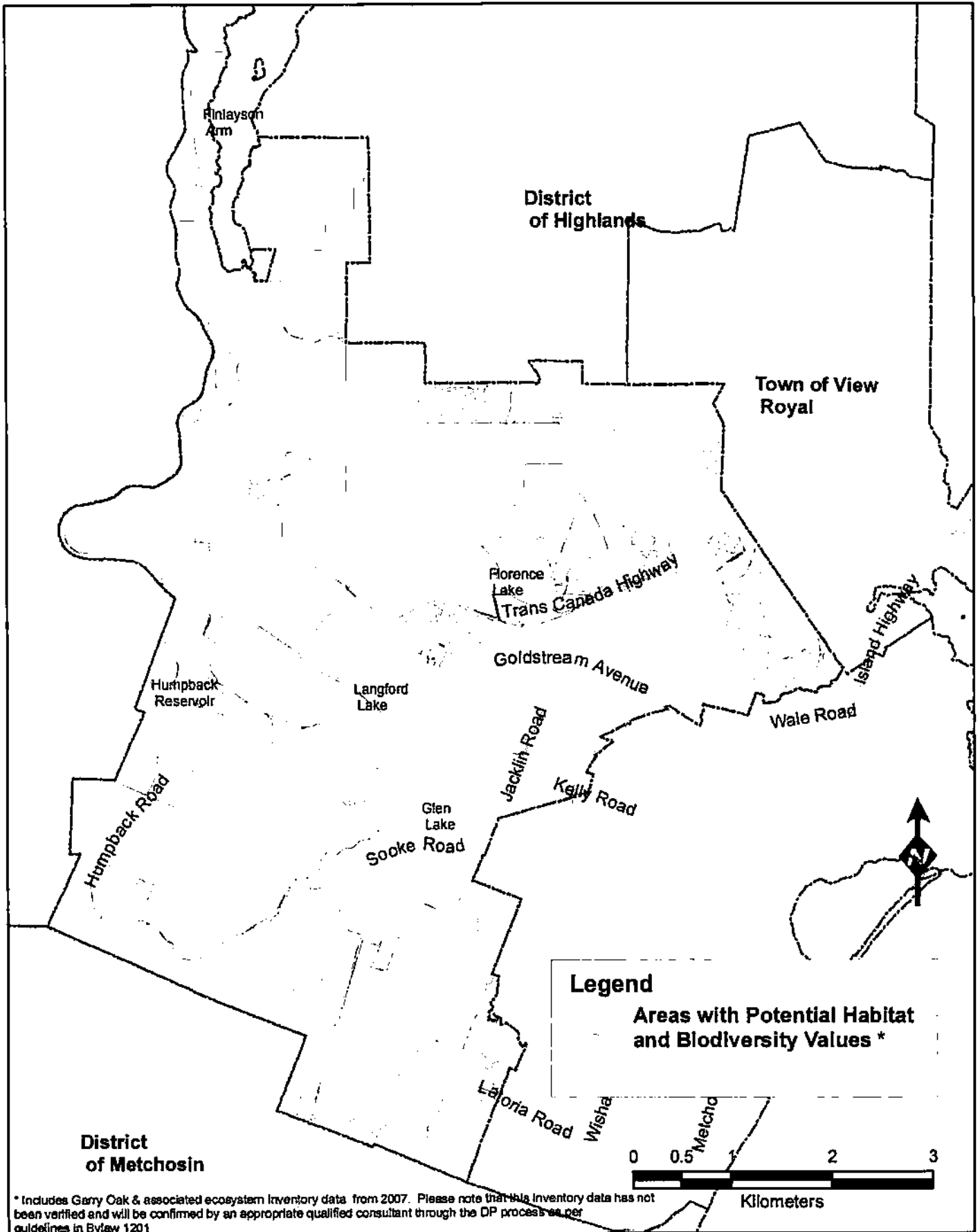
Map 5: Natural Setting, Parks & Open Spaces Inventory



Map 6: Natural Setting, Parks & Open Spaces Integration Strategy

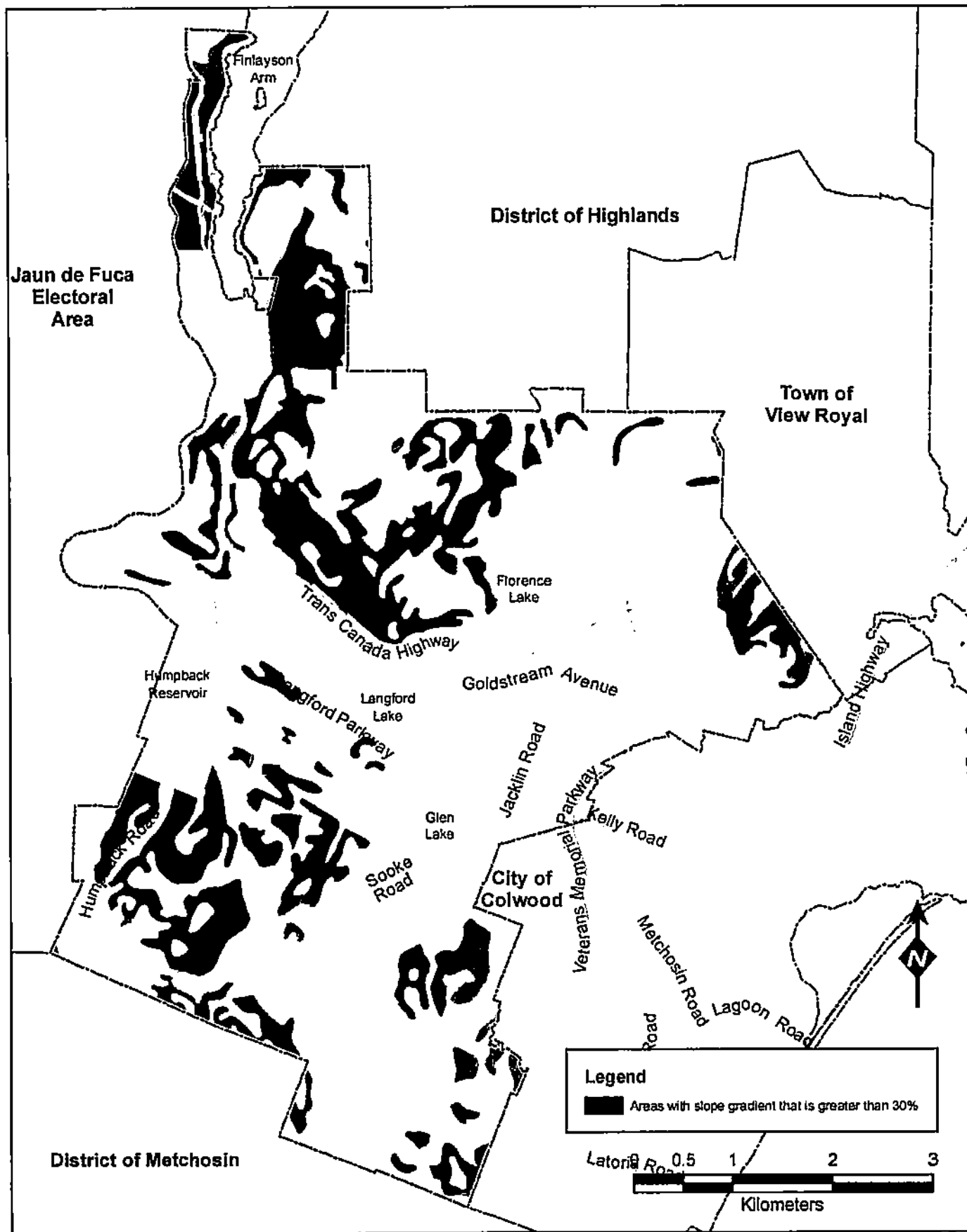


Map 15: Areas with Potential Habitat & Biodiversity Values



* Includes Garry Oak & associated ecosystem inventory data from 2007. Please note that this inventory data has not been verified and will be confirmed by an appropriate qualified consultant through the DP process as per guidelines in Bylaw 1201

Map 17: Steep Slopes



CITY OF LANGFORD
BYLAW NO. 1200

**A BYLAW TO ADOPT AN OFFICIAL COMMUNITY PLAN FOR THE ENTIRE MUNICIPALITY
PURSUANT TO S.876 OF THE LOCAL GOVERNMENT ACT**

The Council for the City of Langford in open meeting assembled and having observed the relevant provisions of the *Local Government Act*, enacts the following:

1. Title

This Bylaw may be cited as "Langford Official Community Plan Bylaw, 2008".

2. The City of Langford Official Community Plan

The document entitled "The Langford Official Community Plan", annexed hereto and marked Schedule "A" to Bylaw No. 1200 is adopted as the Official Community Plan of the City of Langford.

3. Severability

No provision of this Bylaw depends for its validity on any other provision and the provisions of this Bylaw shall remain in full force and effect, save and except only those specific provisions of the Bylaw which may for any reason be held to be invalid.

4. Repeal

City of Langford Bylaw No. 150 "Langford Official Community Plan Bylaw, 1996" is repealed.

READ A FIRST TIME this day of, 2008.

PUBLIC HEARING HELD this day of, 2008.

READ A SECOND TIME this day of, 2008.

READ A THIRD TIME this day of, 2008.

ADOPTED this day of, 2008.

MAYOR

CERTIFIED CORRECT
(Clerk-Administrator)



City of Langford

Official Community Plan - 2008

Bylaw No. 1200

Copies of this Bylaw may be viewed at Langford City Hall, Second Floor, 877 Goldstream Avenue, Victoria, BC, or on our website at www.cityoflangford.ca.

This OCP was prepared by the Holland Barrs Planning Group in 2008 with staff from the Cities of Colwood & Langford.

Other contributors included Lees & Associates Landscape Architects; Eberle Planning & Research; Stevens Engineering; and, EcoPlan International who all provided policy advice and Lees & Associates Landscape Architects, MVH Design & Planning, Frank Ducote Urban Design, and D'Ambrosio Architecture + Urbanism who provided urban and open space design concepts.

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Part I: OCP Overview

1.0 Introduction

The City of Langford is a rapidly growing community that has been transitioning from a suburban centre to a major urban centre in the Capital Region for some time. Almost 20% of the people residing in Langford in 2006 were new to the City; this statistic places Langford amongst the fastest growing communities in Canada in the last census period. Generally, change has been accepted and even embraced. The benefits have been numerous. For instance, increased tax revenues from new development have been reinvested into beautification initiatives, public amenities and new facilities. New development has also brought new jobs, services, affordable housing, and greater housing diversity. Despite this change, Langford has retained its family-oriented nature, and we remain one of the most affordable communities in the region.

Despite the tangible benefits of development, climate protection and sustainability are at the forefront of our minds. What does the future hold and how do we know we are making the right choices today? More specifically, citizens are keenly interested in increasing options for how they get around. Many have expressed a strong interest in promoting a viable local food system. Other desires have been articulated: increasing local jobs, improving housing diversity and affordability and preserving open spaces are top priorities for many. Langford's residents have become accustomed to ensuring new development benefits the entire community and addresses community objectives efficiently.

This Official Community Plan (OCP) harnesses public feedback and current thinking on sustainability to provide a strong rationale for how and where development and land use changes are to occur.

This OCP was developed jointly with the City of Colwood.

Both communities make up the urban core of the West Shore and have a strong track record of collaboration.

→ What is an Official Community Plan?

An Official Community Plan is a land use plan and policy document for a community. An OCP vision reflects the ideas and input of our residents, stakeholders, professionals and staff who participated in the preparation of the Plan. The OCP integrates land use, economy, environment, transportation, community facilities and services, and creates a broad strategy to direct growth and development. When focused on sustainability, an OCP is an essential means to assist communities in considering and organizing responses to global, regional and local challenges such as climate change and ecosystem health, social development, and the increasingly changing patterns of economic stability.

OCP's are strategic plans and are mainly intended to guide decision-making and dialogue about choices and directions. Conditions and opinions change frequently in the same way that opportunities or new challenges emerge without notice. For this reason, the *Local Government Act* states that an OCP does not commit or authorize a municipality to implement policies specified in the Plan, however, all bylaws (as enacted or amended) or works undertaken by a Council after the adoption of an OCP must be consistent with the Plan. Also, when a Regional Growth Strategy is in place, as is the case for the Capital Regional District, the OCP must provide a Regional Context Statement (see Section 13.0).

An effective OCP provides clear direction but does not preclude amendments to the plan based on changing circumstances.

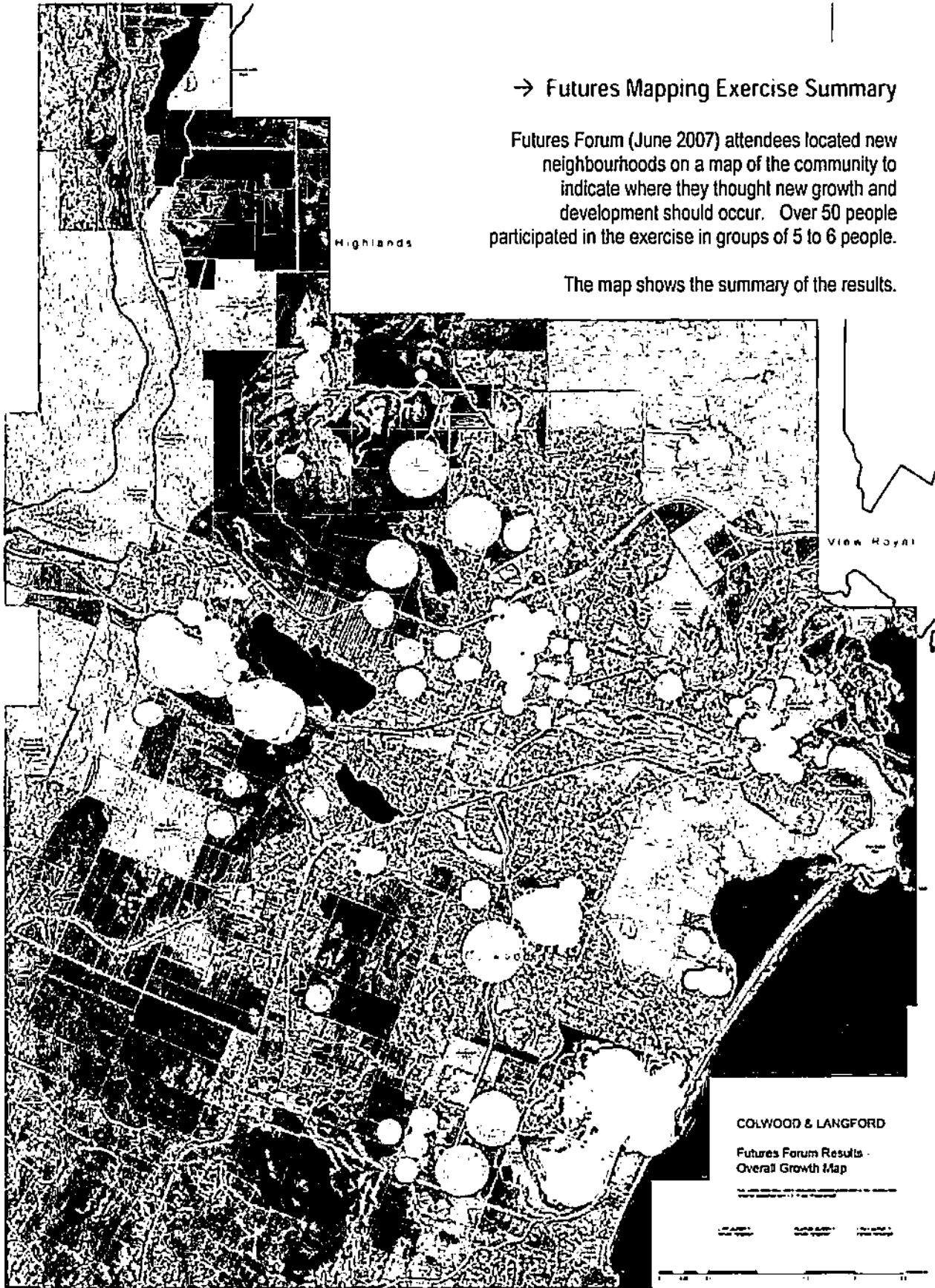


Futures Forum (June 2007) attendees locate new neighbourhoods on a map of the community.

→ Futures Mapping Exercise Summary

Futures Forum (June 2007) attendees located new neighbourhoods on a map of the community to indicate where they thought new growth and development should occur. Over 50 people participated in the exercise in groups of 5 to 6 people.

The map shows the summary of the results.



→ How was this OCP developed and who was involved?

The cities of Colwood and Langford initiated an innovative joint review of their respective Official Community Plans (OCP's) with the objective of creating an integrated and sustainable future for both communities. The process for reviewing the OCPs combined opportunities for community and stakeholder feedback, technical analysis and policy and design development. The diagram below shows the main phases and key events of the OCP review process.

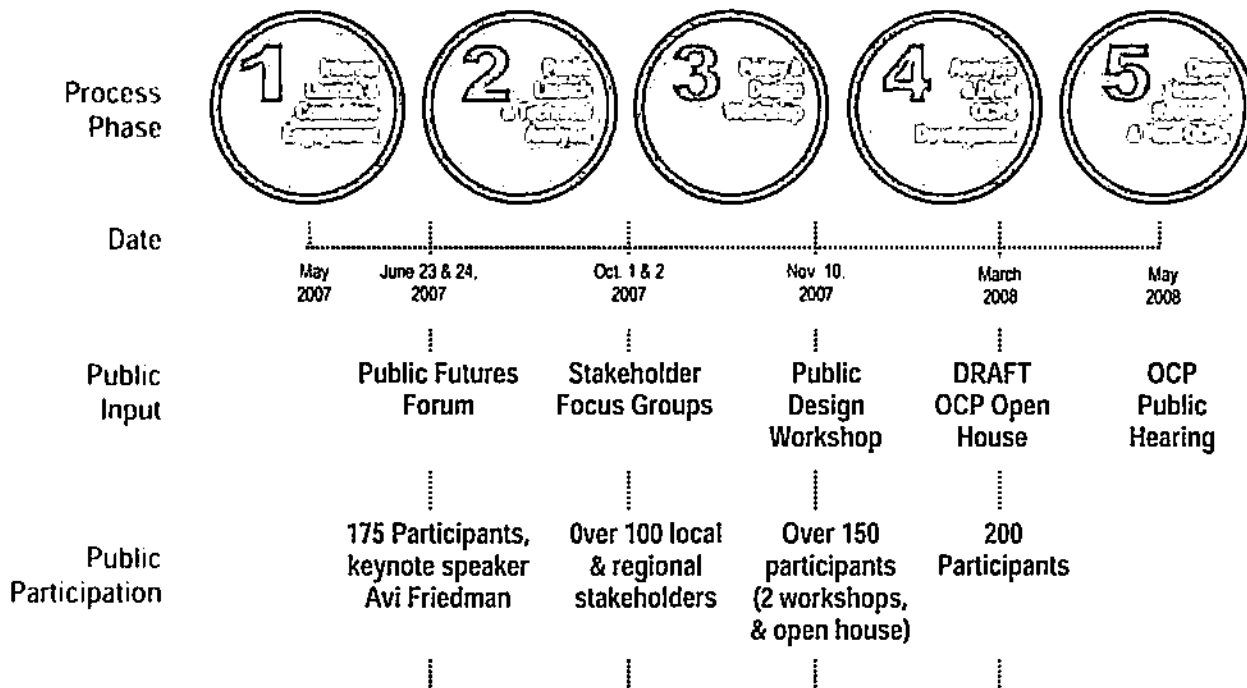
The OCP review process benefited from the oversight of an OCP Steering Committee made up of citizens who are passionate about their community. The Colwood and Langford OCP Steering Committees met separately and together to guide the OCP Review team at key stages. The OCP Review team was made up of staff from Langford and Colwood and professional planners, architects, landscape architects, transportation engineers, and housing and economic development experts.



A Futures Forum attendee shares highlights from a group discussion about community priorities.

For a list of stakeholder groups who provided input into the process, please see Appendix A.

OCP Review Process Overview



→ Why are we planning?

Our community is facing an uncertain future. Climate change will likely not only compromise quality of life and cause environmental change, but could also undermine local and global economies and ecosystems if not kept in check. To address these concerns, the Province of BC has committed to reducing GHG emissions by 33% by 2020. To reach this commitment, the active involvement of cities will be critically important. Other changes are anticipated. Wetter winters, dryer summers, severe weather events, rising sea levels will increase infrastructure demand. Worse yet, despite concerns about climate change, most energy analysts agree that we are fast approaching "peak oil". Peak oil refers to the predicted peaking of world oil production in the coming decades. Anticipated results include dwindling supplies and therefore increasing cost of fuels that we rely on for everything from heating buildings to powering cars to producing food. In the face of these changes and many others, the imperative to develop in a more sustainable manner is clearer than ever.

Langford's imperative for this OCP is to harness development forces to contribute to sustainable and positive change.

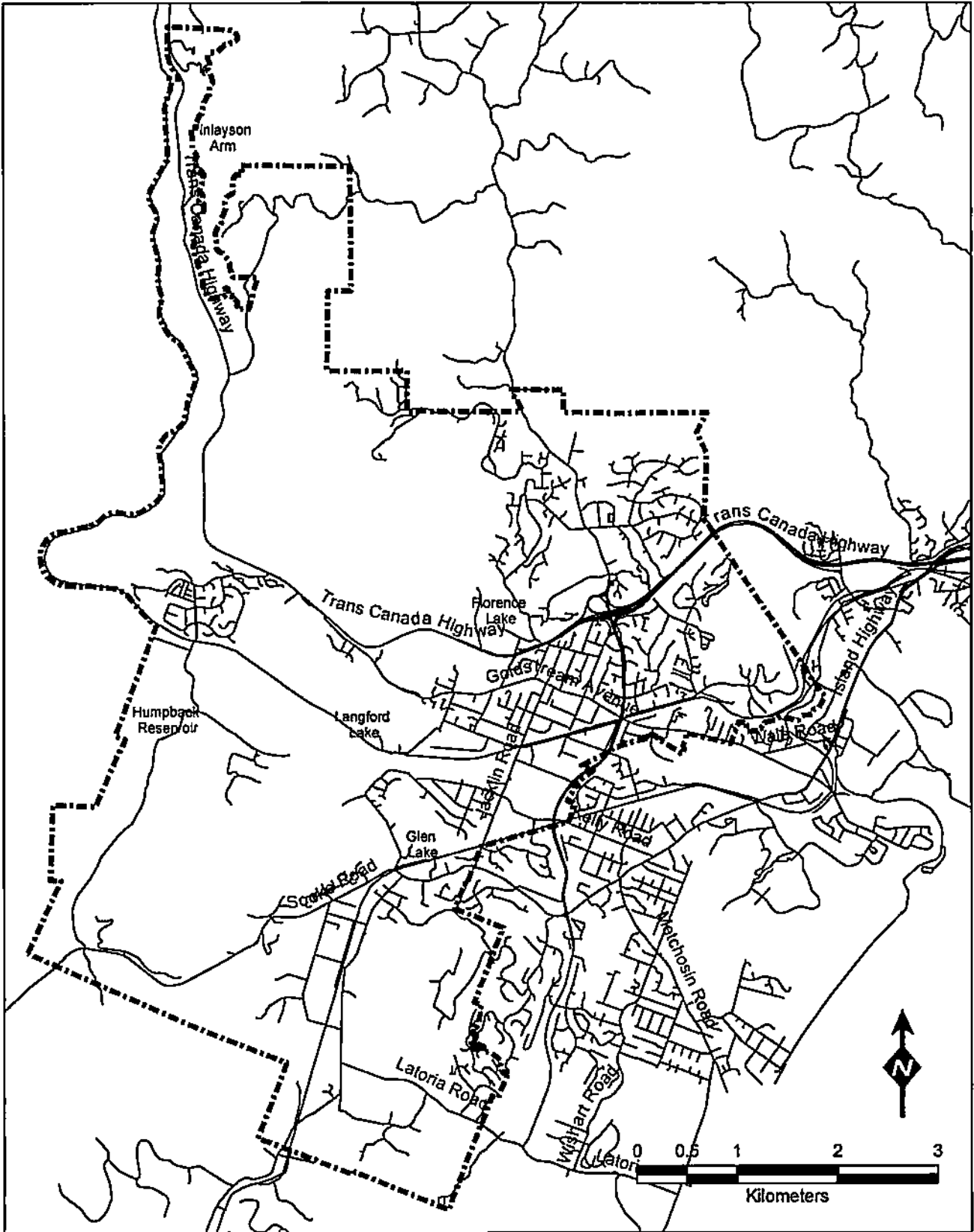
In the meantime, significant growth is coming our way. The Capital Regional District's *Regional Growth Strategy* (RGS) has designated our city as a major growth area within the region's *urban containment boundary* (UCB); the UCB is intended to shape and contain the extent of growth in the region.

The population in the Capital Regional District (CRD) is anticipated to grow from 353,441 (2006 statistic) to 421,820 in 2026. As a key growth area in the region, the RGS has planned for Langford to absorb 22,000 new people in approximately 11,035 new homes in the same period. This translates to a population of over 47,000 by 2028. In 2008, some 22,000 new units, or 200% of long term housing forecasts, were in various stages of approval (rezoned, development permits) or planning (OCP) and capacity for new housing still remains in abundance throughout the community. While approved units do not always get built, this indicates that Langford may potentially *absorb a greater proportion* of the CRD's projected population and/or the *housing absorption* period will extend beyond 2028. Given the strong demand for housing and the draw to the West Shore, it is more likely that the community will absorb more newcomers to the CRD with a possibility of a population much greater than the current projection in the coming decades.

	City of Langford		CRD
	Population	Dwellings	Population*
2006	24,892	8,650	353,441
2028	47,244	19,685	421,820
Increase	22,352	11,035	68,379
Annual			
Increase	1,016		3,108
Annual %			
Increase	4.0%		0.9%

Notes: 2006 dwellings occupied by usual residents, CRD RGS average household size projections based on BC Stats People 32 population projections for CRD August 2007, with share apportioned to Langford as per CRD Regional Planning Dept. & Eberle Planning & Research Inc.

Map 1: Planning Area



2.0 Our Sustainability Goals

→ The Sustainability Imperative

"*Sustainable development*" emerged from the United Nations Brundtland Report in 1987, in recognition that societies need to create prosperous economies and communities, but not irreversibly damage the planet in the process. The report was important because it not only considered the long term implications of environmental and social health that would result from economic growth, but it also predicted the opposite as well: the impacts that degraded ecosystems would have on economic growth into the future. The report concluded that to sustain economic growth, the approach to economic development that would lead to long term prosperity must be transformed from one that generally ignores the environmental and social impacts on the planet, to one that integrated those concerns into everyday business and political decisions and activities.

The insight that this approach has led to, that differentiates it fundamentally from others, was that it offered *a framework for thinking about development that enabled social, environmental and economic concerns to be addressed proactively so that synergistic solutions can be implemented.*



*This OCP is
our community's
sustainability
roadmap.*

→ Community Sustainability Goals

To make progress on community sustainability, it is imperative to address the fundamental aspects of life and living that will enable the community and its residents to function healthily and sustainably over the long term. The following fundamental sustainability goals will be applied to all aspects of community planning and development:

- | | |
|--|--|
| <i>Ecosystems & Biodiversity</i> | 1. <i>Maximize the ecological value of natural areas. Where development occurs, maintain ecosystem values.</i> |
| <i>Community Health</i> | 2. <i>Develop or enhance environments that enable healthy choices in all areas of living that are safe, secure and welcoming for all.</i> |
| <i>Basic Needs</i> | 3. <i>Ensure healthy housing and food is available for all.</i> |
| <i>Personal Health</i> | 4. <i>Ensure healthy recreational, educational, emotional, spiritual, artistic and cultural opportunities for all.</i> |
| <i>Sense of Place</i> | 5. <i>Ensure community planning and design celebrates the community's unique history and natural setting and embraces diversity.</i> |
| <i>Vibrant Local Economy</i> | 6. <i>Enhance economic vitality and opportunity. Ensure local employment and business opportunities.</i> |
| <i>Energy, Climate Protection & Adaptation</i> | 7. <i>Promote energy-use choices that reduce greenhouse gas emissions and contribute to clean air. Plan for climate change adaptation.</i> |
| <i>Water, Resources & Wastes</i> | 8. <i>Promote efficient use and re-use of water and other resources. Reduce the consumption of non-renewable resources in favour of renewable resources.</i> |

→ Climate Action Commitment

The City of Langford became a Signatory of the British Columbia Climate Action Charter in 2007 and is committed to taking action on climate change, including planning liveable, sustainable communities, encouraging green developments and transit-oriented developments, and implementing innovative infrastructure technologies including production of clean energy.

The adoption of this sustainability-focused OCP as a bylaw demonstrates, *in the most meaningful way*, the city's commitment to ensuring its role in addressing issues related to climate change. Our city also recognizes that reducing emissions will generate social, environmental, economic and health benefits for individuals, families, and businesses throughout the community.

Through this OCP, our city is demonstrating its commitment to implementing actions outlined in the Charter that include:

- Aiming to reduce GHG emissions, including emissions from government buildings and operations, and those created by members of the community;
- Amending legislative, regulatory, policy, or other barriers to taking action on climate change;
- Implementing programs, policies, or legislative actions, within the City's jurisdiction, that facilitates reduced GHG emissions, where appropriate;
- Encouraging centres that are complete and compact and socially responsive; and
- Encouraging infrastructure and a built environment that supports the economic and social needs of the community while minimizing its environmental impact.

As a planned growth area, reducing GHG emissions will be a challenge.

Sustainable development in our community will facilitate lower GHG emissions in the region on a per capita basis.

This is an opportunity for our community to be a leader in sustainability.

Our efforts will benefit the region, the Province and the planet.

3.0 Our Sustainable City

Our buildings, streets, and open spaces contribute to the community's overall 'look and feel' and largely determines activity patterns (*e.g. how we get around, how we interact with neighbours, how we play*). Our community's design and layout are critical for setting the stage for sustainable development. Development often involves changing or modifying natural ecosystems and regardless of location, development has an impact on life sustaining elements such as air, water and soil. Development should therefore maintain the values of natural ecosystems and minimize other impacts when and where feasible. Thoughtful urban design is one of our community's most powerful tools to advance the quality of life of our residents, as well the competitive advantage and attractiveness of the community, which is so critical for attracting business investment and jobs. Breathtaking natural features such as mountains, lakes and the sea will shape future developments as will our city's unique history and people.

→ Our Vision

Our sustainable community, nestled in a rich network of hills, creeks, lakes and the sea, is defined by attractive, compact and complete centres connected by multi-use transportation corridors.

Our sustainable community is welcoming and inclusive for all and is planned and designed to support the needs of a diverse and changing population.



Langford will use its regulatory and non-regulatory powers to implement our vision.

→ Growth Management & Land Use Strategy

Our growth management strategy defines centres where growth will be focused as well as areas where development will be controlled. Unique conditions must be addressed, and corresponding development intensities and patterns will emerge.

The following key growth management and land use designations will guide planning and decisions about development intensity. Within this context, site-specific land use and density will be determined at the time of re-zoning. *Map 2* details the general configuration of land use designations.

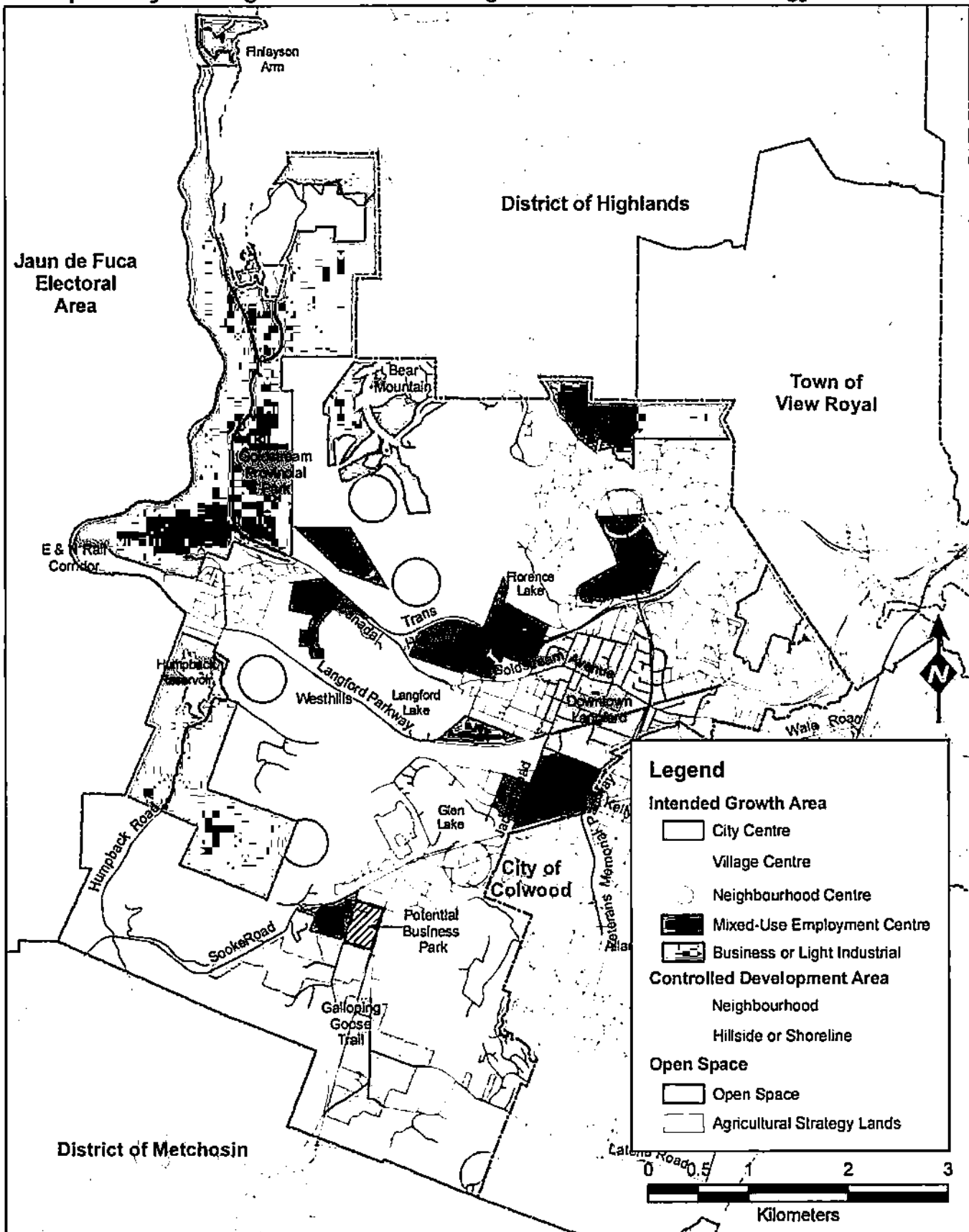
The Growth Management & Land Use Strategy is a key tool to ensure our vision is implemented.

Land Use & Growth Management Strategy Brief Overview

	Land Use Designation	General Location in City
Growth Management Strategy	Intended Growth Area	
	City Centre	Downtown Langford
	Village Centre	Bear Mountain Village Centre Westhills Village Centre
	Mixed-Use Employment Centre	West Shore Town Centre Millstream Village Goldstream Meadows Trans-Canada / Leigh Rd
	Business or Light Industrial Centre	Various
	Neighbourhood Centre	Various
	Controlled Development Area	
	Neighbourhood	Existing Settled Areas
	Hillside or Shoreline	Undeveloped or Existing Low Intensity Hillside or Shoreline Areas
	Open Space	
Agricultural Strategy Lands	Lands currently held in the Agricultural Land Reserve	
Open Space	Public and Private Open Spaces	

A detailed overview of the Land Use and Growth Management Strategy by designation follows.

Map 2: City of Langford – Growth Management & Land Use Strategy



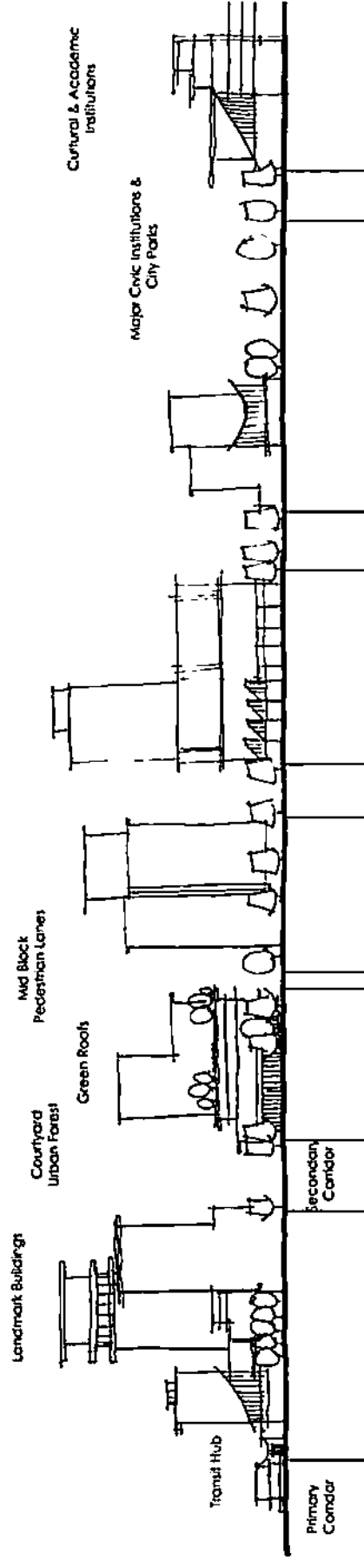
City Centre

- A major regional growth centre that support a wide range of high density housing, including affordable and rental housing
- A major employment area for institutional, office, commercial, light industrial uses
- Major civic uses and public buildings are key landmarks
- A major place of community gathering and celebration
- A wide range of public squares, parks and open spaces are integrated throughout
- The City's major entertainment and/or cultural precinct
- Inter-city and/or inter-regional transit hub connect residents



A Concept for the City Centre

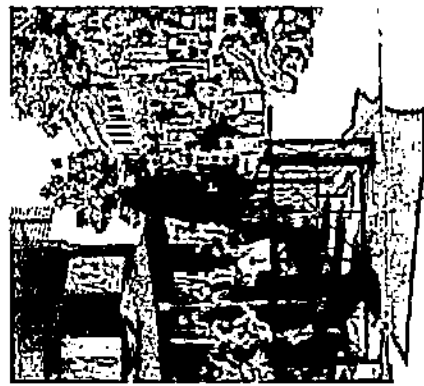
Sculpted skyline to accommodate view corridors and concentrate tower heights as a node



Possibilities for our City Centre



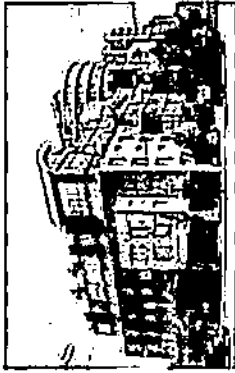
Contextual design directions



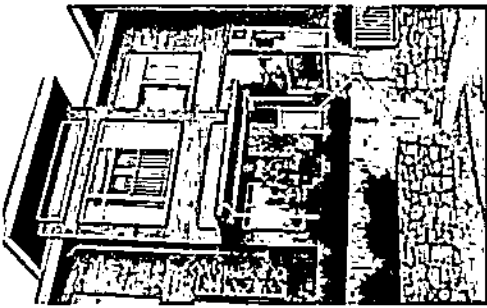
Rich public realm designed for people



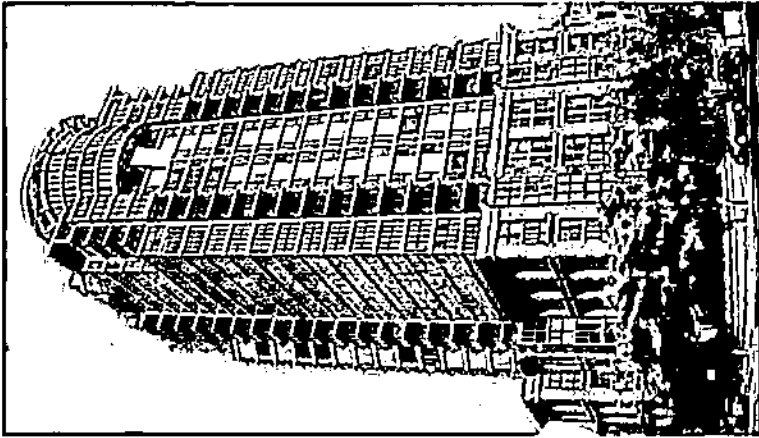
Government & business offices



Mid-rise mixed use buildings



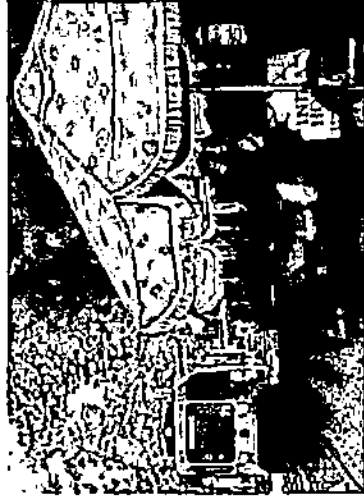
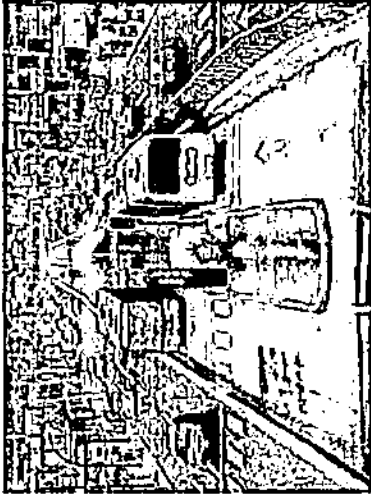
Family-oriented townhouses



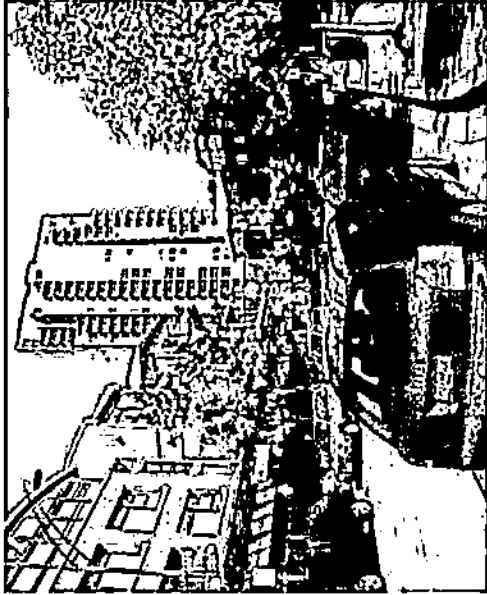
Attractive hi-rise residential buildings



Distinctive Live/Work units



Integrated rail- and/or road-based rapid transit



Hi- & mid-rise buildings together with retail at street level

→ City Centre

Objective 3.1 Promote civic activities in the City Centre.

Policy 3.1.1 Locate and/or maintain civic buildings in the City Centre to reinforce the function and focus of the city centre.

Policy 3.1.2 Ensure that civic buildings are / remain distinctive as a landmark in the city centre.

Policy 3.1.3 Create a major gathering place along Goldstream Ave. between Jacklin Rd. and Veterans' Memorial Parkway for civic activities such as parades, celebrations, in proximity to adjacent to commercial, civic, or institutional buildings.

Objective 3.2 Maintain a regional focus.

Policy 3.2.1 Commercial and institutional land uses, services and amenities in the City Centre will balance regional services with amenities and services for local residents.

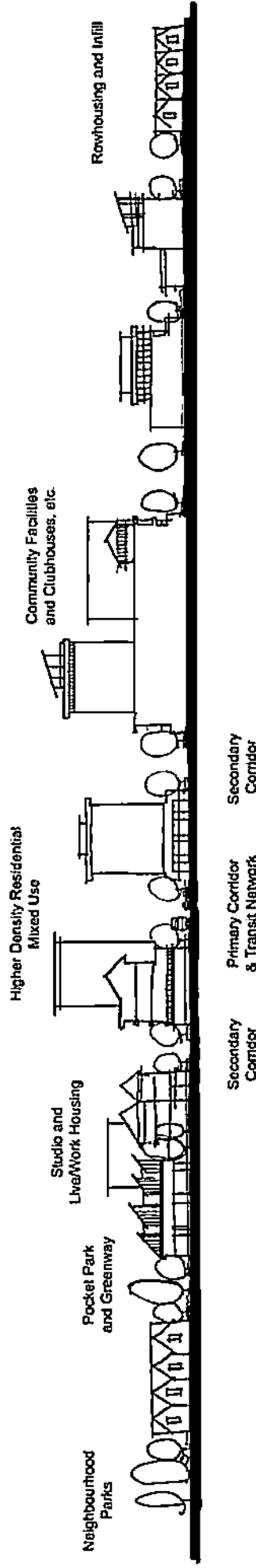
Policy 3.2.2 Transportation services and infrastructure in the City Centre will connect users to regional transportation systems.



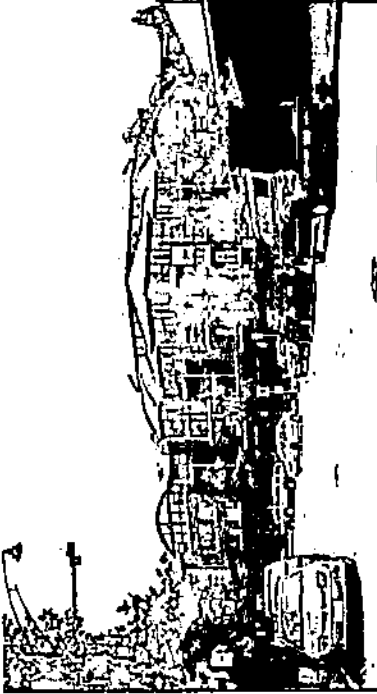
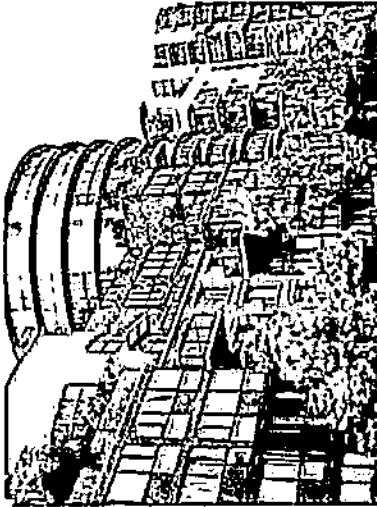
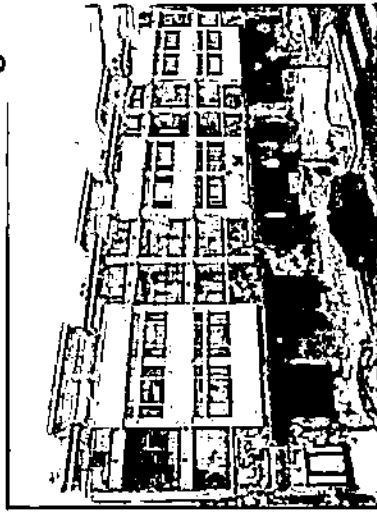
Village Centre

- A predominantly residential precinct that supports a wide range of high and moderate density housing, including affordable and rental housing
- A key location in the city for shopping, services, amenities, and any other function that supports local residents daily needs
- Parks and open spaces and recreational facilities are integrated throughout
- Major educational, health and child care facilities serve the needs of residents in the city
- Inter-city and/or inter-regional transit hub connect residents

A Concept for a Village Centre



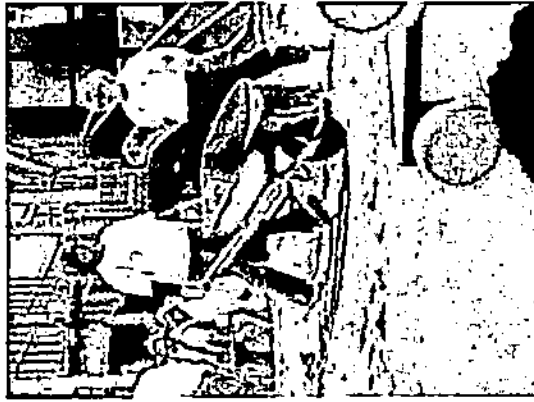
Possibilities for our Village Centres



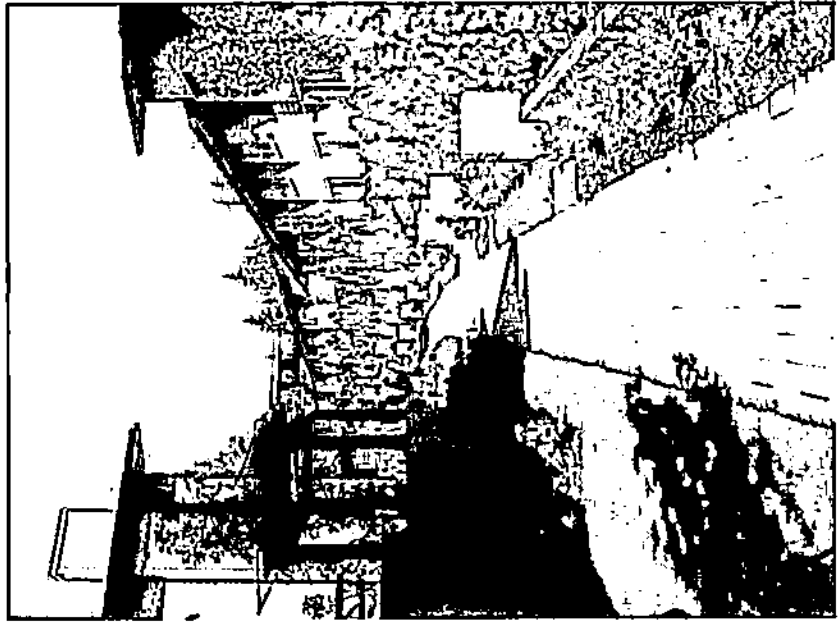
Pedestrian-friendly public realm

Variety – mid rise & low rise buildings

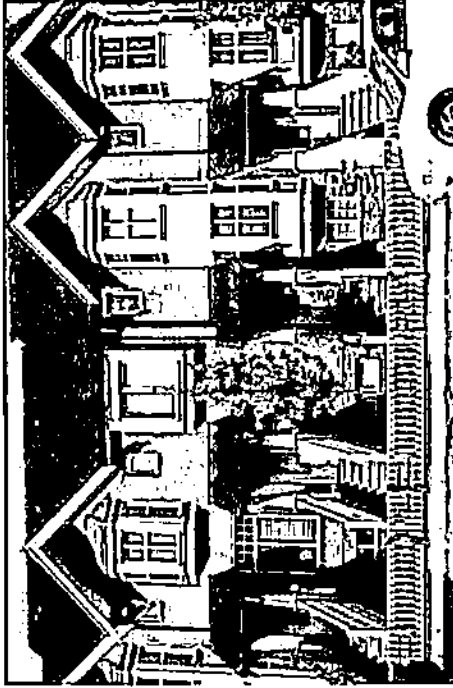
Lively main streets



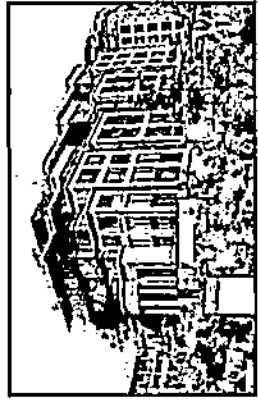
Community facilities & involvement



Great pedestrian connections



Housing of all types



Terraced mid-rise buildings



Convenient cycle paths



Intimate public gathering places

→ Village Centres

Objective 3.3 Maintain a sub-regional focus.

Policy 3.3.1 Commercial and institutional land uses, services and amenities in will balance sub-regional services with provision of ample local services and amenities.

Policy 3.3.2 Transportation services and infrastructure in the Village Centres will connect users to sub-regional and regional transportation systems.

Objective 3.4 Serve a distinctive role in the City.

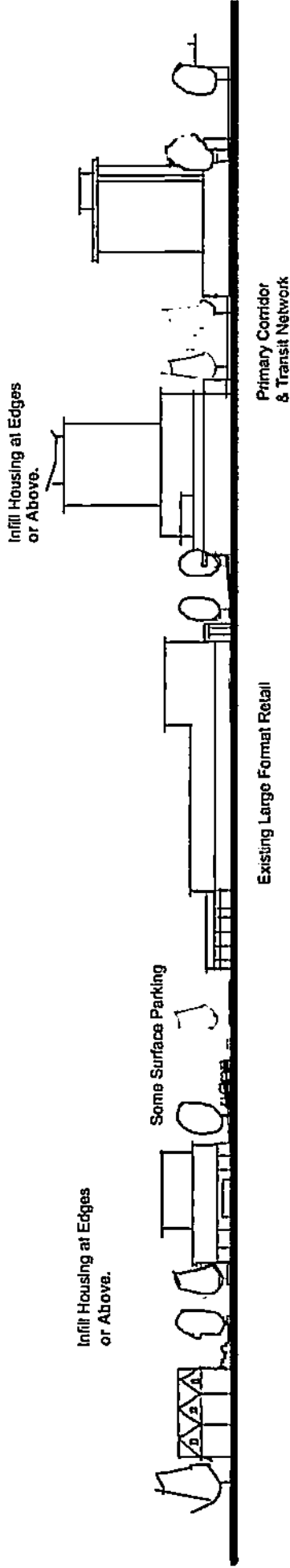
Policy 3.4.1 Commercial and institutional land uses, services and amenities in a Village Centre will fulfill a distinctive role in the City and not compete with the role of the City Centre as the major civic centre.



Mixed-Use Employment Centre

- A predominantly workplace precinct that includes business of all types including commercial, light industrial, and institutional
- An ideal location for creative or innovative infill housing (such as artisan live-work, mixed use buildings, etc.) that does not jeopardize the long-term function of the centre as an employment node
- Parks, public squares and open spaces are integrated throughout
- Centre is an inter-city and/or inter-regional transit hub that connects residents and employees

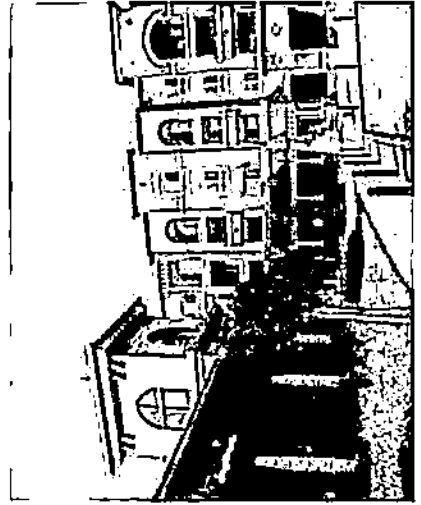
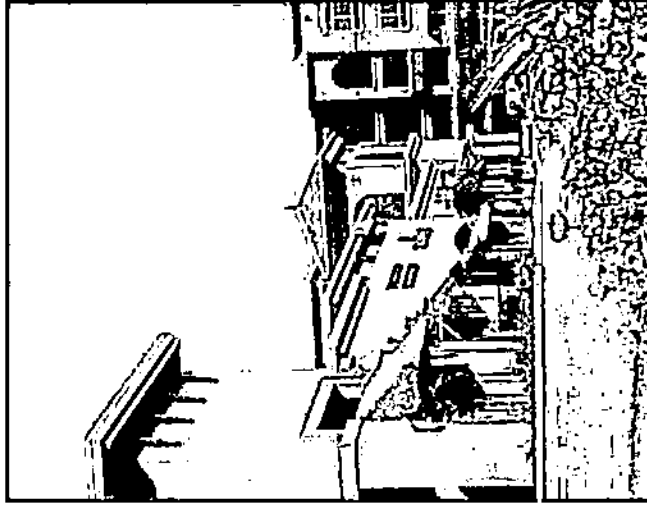
A Concept for a Mixed-Use Employment Centre



Possibilities for our Mixed-Use Employment Centres

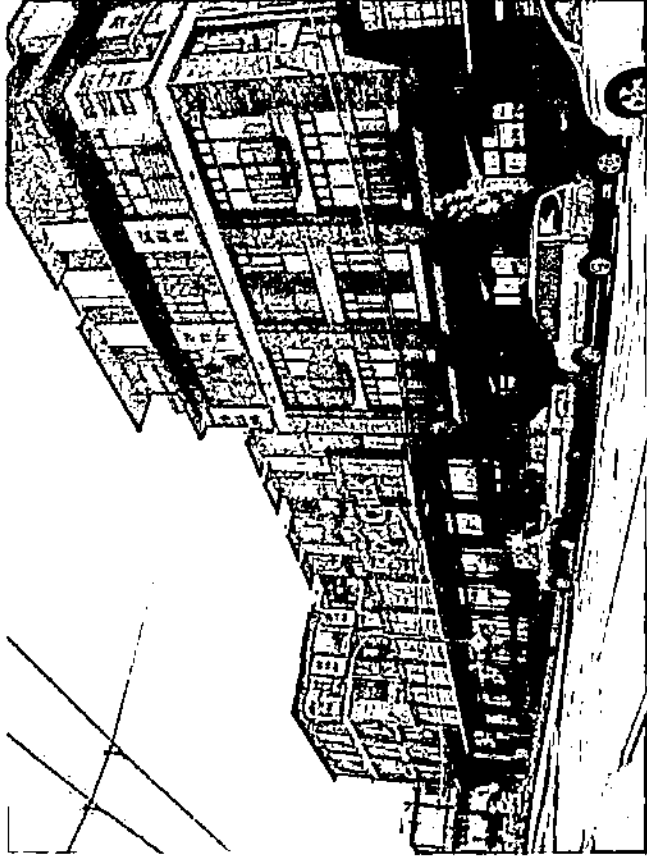
"The Wrap":

Residential & other uses integrated into existing large floor plate commercial / light industrial sites. Parking is at-grade or underground.



"Integrate the Big Box":

Street-oriented residential and other uses above large floor plate commercial uses. Parking is usually underground, on rooftops or minimal.



→ Mixed-Use Employment Centres

Objective 3.5 Maximize adaptive re-use opportunities in greyfield or underdeveloped commercial sites.

Policy 3.5.1 Encourage the infill of auto-oriented commercial sites, big box sites, and strip malls with mixed-use residential development.

Policy 3.5.2 Promote the re-development of brownfield sites as predominantly work place areas that integrate housing.

Policy 3.5.3 Enact property tax incentives for a period of up to 10 years for new mixed use residential development on greyfield sites.

Policy 3.5.4 Accommodate a broad range of retail places by integrating large floor plate retail stores into mixed use buildings.

Policy 3.5.5 Promote high intensity, small parcel business and light industrial development that is compatible with residential uses.

Objective 3.6 Maintain a workplace focus.

Policy 3.6.1 Ensure a long term supply of employment lands in centres is maintained.

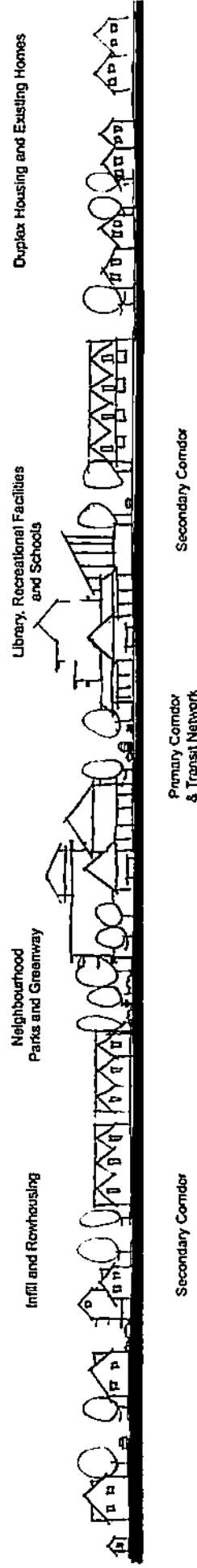
Policy 3.6.2 Ensure choices about land use and density do not preclude the long term role of the centre as an employment node.



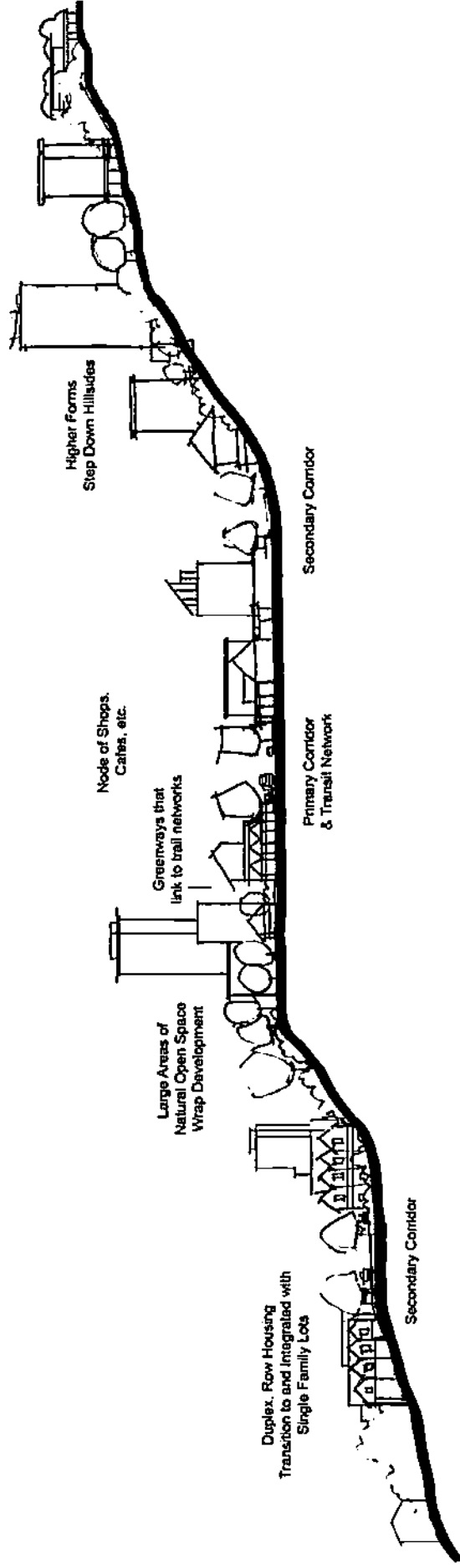
Neighbourhood Centre

- Predominantly residential precinct that supports a range of medium and high density housing, including affordable and rental housing
- Schools, community facilities and other institutional uses also define neighbourhood centres when supported by housing and services in close proximity
- A range of parks and open spaces are integrated throughout centres
- Transit stops are located at centres where appropriate
- Small scale local-serving retail node defines the predominant commercial uses

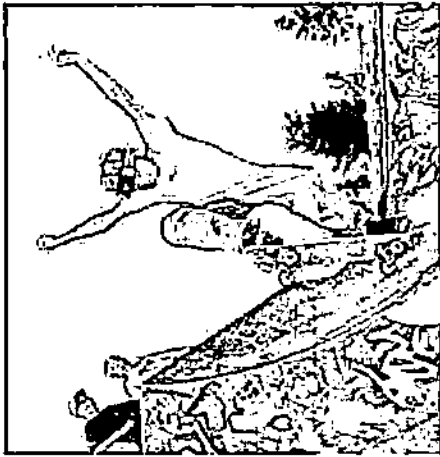
A Concept for a Neighbourhood Centre on the Valley Floor



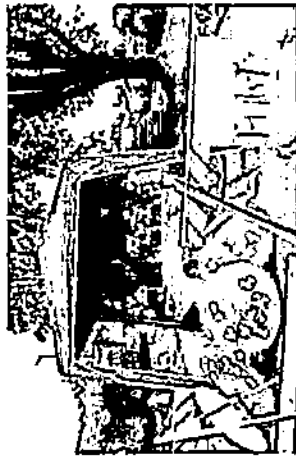
A Concept for a Neighbourhood Centre on a Hillside



Possibilities for our Neighbourhood Centres



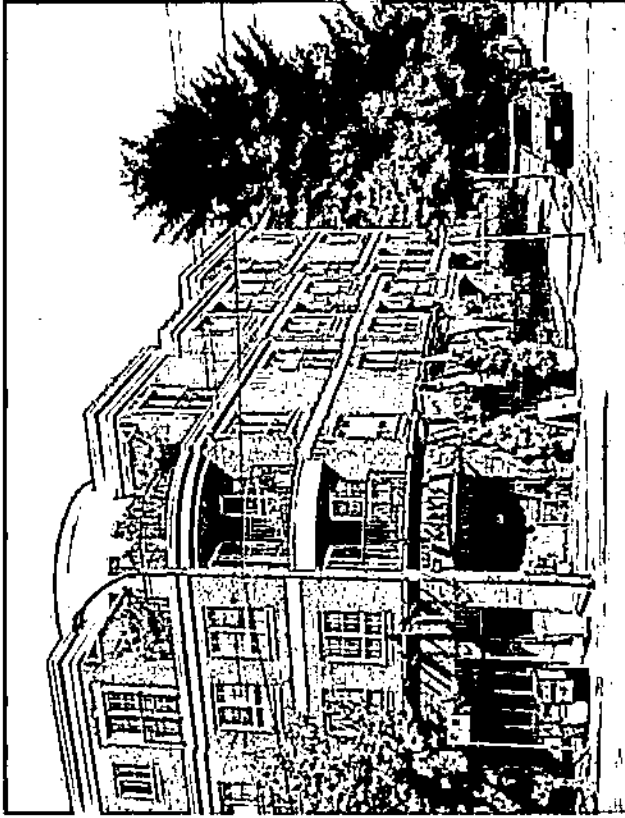
Public places for all members of the community



Community gathering



Farmers markets



Mixed use buildings at key intersections with rental & affordable housing



Greenways connecting places

"Woonerfs" – streets for all users



Neighbourhood Centre cluster on a hillside

→ Neighbourhood Centres

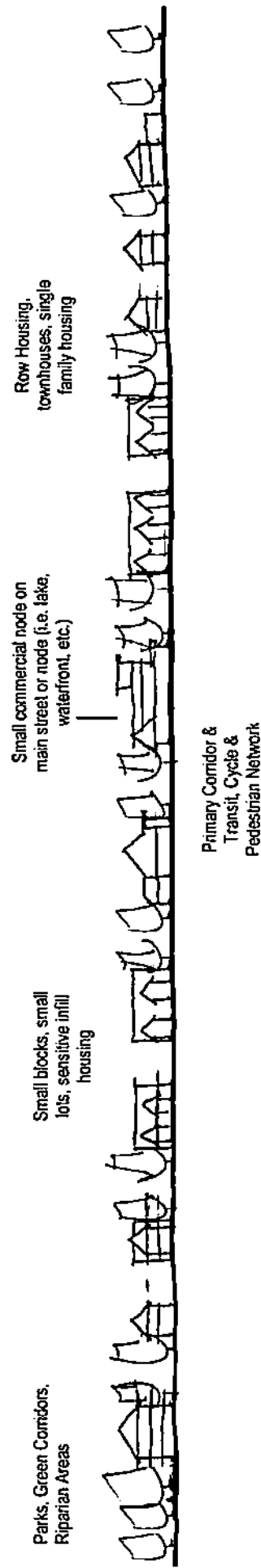
- Objective 3.7** Allow for the emergence of new neighbourhoods areas and/or centres and areas in all parts of the community.
- Policy 3.7.1** Recognizing that general densification over time may lead to the need and/or viability of neighbourhoods and centres that serve the needs of residents, neighbourhoods in all areas of the city will be permitted.
- Policy 3.7.2** Council may request a Neighbourhood Area Plan (NAP) or a Neighbourhood Centre Plan (NCP) prior to considering changes in land uses and/or development approvals, at its discretion. An NAP or NCP will define, at a minimum, the following:
- i. Rationale for location of neighbourhood and/or centre and how it contributes to overall to community vision and goals.
 - ii. Land uses including parks & open space
 - iii. Information about building siting based on proposed setbacks
 - iv. Architectural, landscape and/or public realm guidelines
 - v. Amenities
 - vi. Servicing / Infrastructure strategy
- Policy 3.7.3** An NAP can contain information about a neighbourhood centre. In these instances, an NCP will not be required so long as the plan for the centre meets the objectives of Section 5.0.
- Policy 3.7.4** Council reserves the right to complete an NAP or an NCP on a cost recovery basis. That is, all costs associated with developing and adopting a NAP or NCP, including public process, will be funded by the development proponent(s) and/or property owner(s).
- Policy 3.7.5** NAP or NCP processes will include public and stakeholder education and consultation. The process can be executed concurrently with other development approvals.
- Policy 3.7.6** NAPs or NCPs will guide Council decisions about land use and density when in place. NCPs will be adopted by resolution or by bylaw. In instances where an NCP is in effect and changes are proposed over time, they may be considered concurrently with rezoning applications.

Neighbourhood

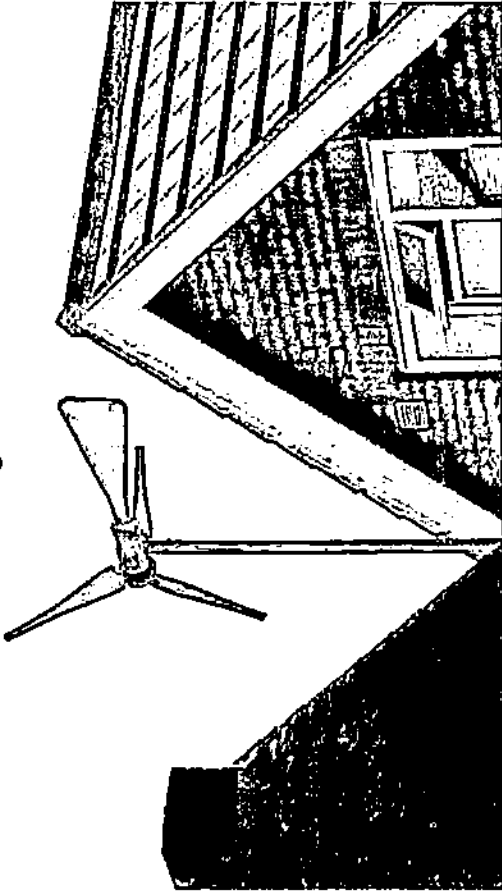
Existing settled areas throughout the community predominantly located on the valley floor.

- Predominantly residential precinct that supports a range of low and medium density housing choices including secondary suites
- This area allows for residential and mixed use commercial intensification of streets that connect centres and/or are serviced by transit
- Schools, community facilities and other institutional uses are permitted throughout the area
- Retail serving local residents is encouraged along transportation corridors
- Home-based businesses, live-work housing is encouraged
- Parks, open spaces and recreational facilities are integrated throughout the area
- This area allows for *Neighbourhood Centres* to emerge in the form of medium density mixed-use nodes at key intersections.
- Transit stops are located where appropriate

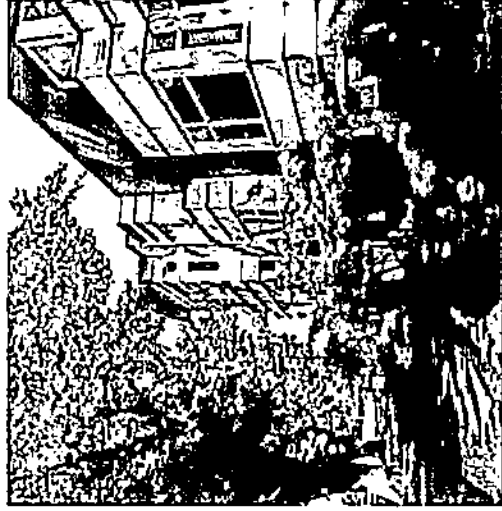
A Concept for Neighbourhood Areas



Possibilities for our Neighbourhood Areas



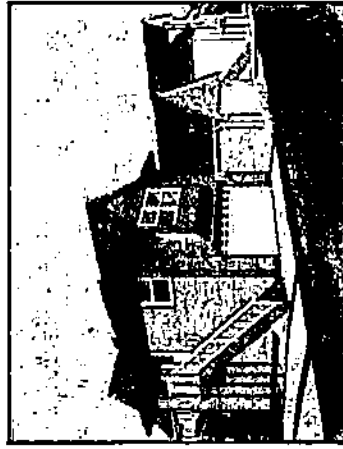
Alternative energy systems



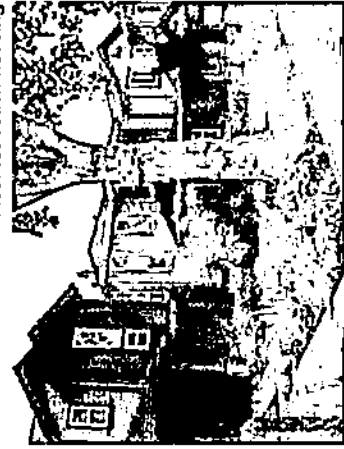
Multi-family buildings



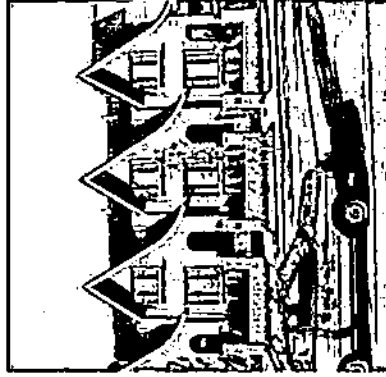
Parks & open spaces



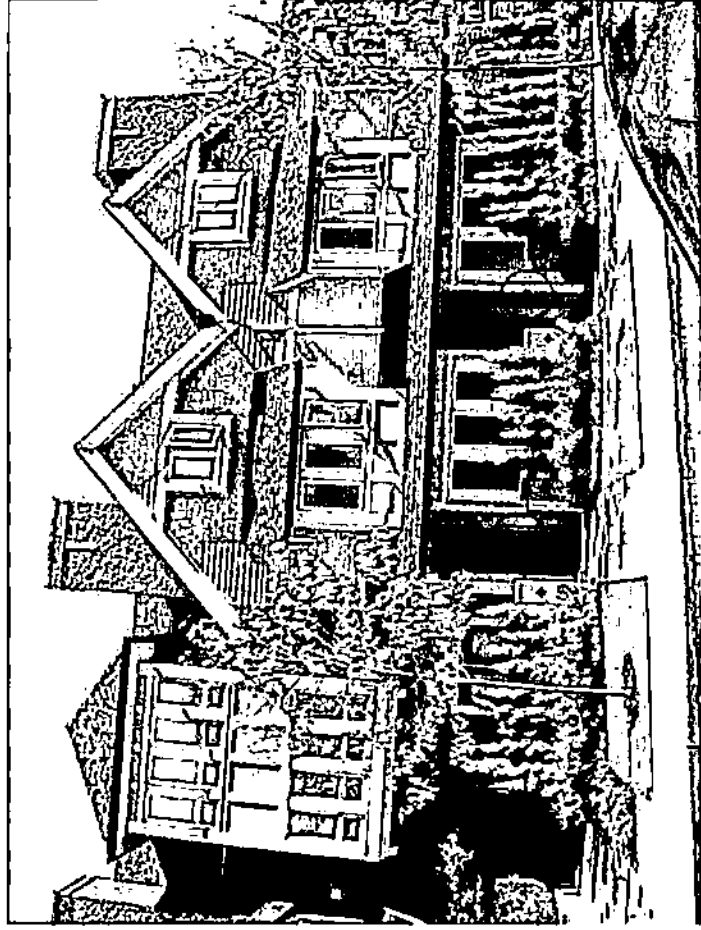
Rear lot coach housing



Small lot housing



Row housing



3-storey townhouses



Corner store / cafe

→ Neighbourhood Areas

Objective 3.8 **Redevelop and intensify corridors connecting centres.**

Policy 3.8.1 Encourage intensification of transportation corridors in and out of centres as a means to further increase the viability of transit services.

Policy 3.8.2 Ensure redevelopment along corridors is pedestrian-oriented and consistent with liveable street strategies.

Objective 3.9 **Permit sensitive infill development.**

Policy 3.9.1 Encourage alternative housing types such as coach housing, row housing, live/work units and townhouses that diversify the housing stock.

Policy 3.9.2 Permit small lot subdivision and clustered densification in established areas.

Policy 3.9.3 An overall density objective of 40 units per hectare (16 units per acre) for infill development in Neighbourhood designated area will guide choices about density. Ensure buildings are sited to complement the type, use and character of adjacent buildings and ensure private outdoor spaces for adjacent properties are respected.

Objective 3.10 **Improve road, cycle and pedestrian connections.**

Policy 3.10.1 Retrofit cycle, pedestrian and road networks into existing neighbourhoods.

Policy 3.10.2 Secure easements or public right of way (ROW) dedications through rezoning and subdivisions to improve road, cycle or pedestrian network connectivity.

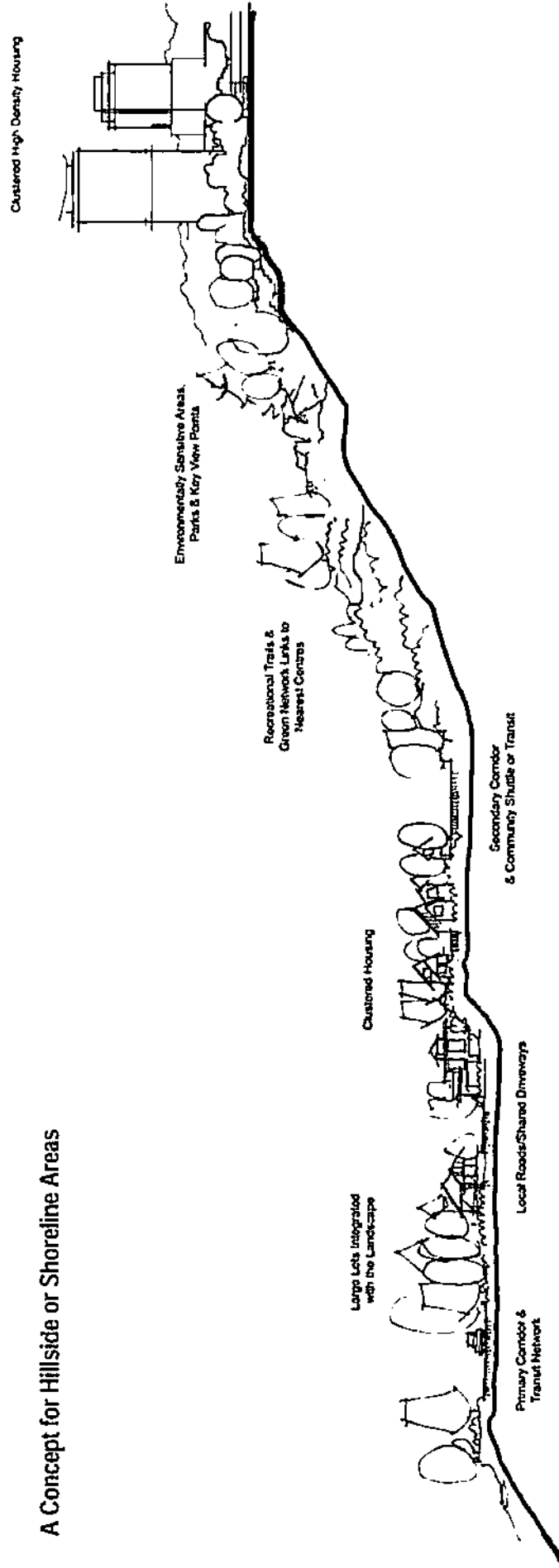


Hillside or Shoreline

Predominantly existing low intensity settled areas throughout community with a high percentage of open space and undeveloped areas located on a hillside or near the shoreline.

- Predominantly residential precinct that supports a range of clustered (See Glossary & next page) low, medium and high density housing choices including secondary suites. Higher building forms, such as point towers, will be permitted on hillsides to maximize open space provided some conditions are satisfied (see policies for this area)
- Schools, community facilities and other institutional uses are permitted throughout the area
- Home-based businesses, live-work housing is encouraged; Home-based accommodations (e.g. Bed & Breakfasts) are permitted.
- Parks, open spaces and green corridors (creeks, wildlife corridors, trails, etc.) are integrated throughout the area. Large playfields are discouraged due to grading requirements. Site and topographic responsive pocket parks, enhanced viewpoints, graded hiking and walking trails, children's play areas, including 'tot lots' and outdoor exercise areas are strongly encouraged on hillside areas.
- This area allows for *Neighbourhood Centres* to emerge in the form of high and medium density *clustered* mixed-use nodes
- Transit stops are located where appropriate

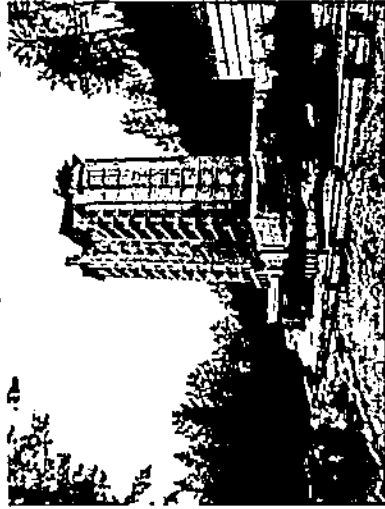
A Concept for Hillside or Shoreline Areas



Possibilities for our Hillside or Shoreline Areas



Integrated stormwater management



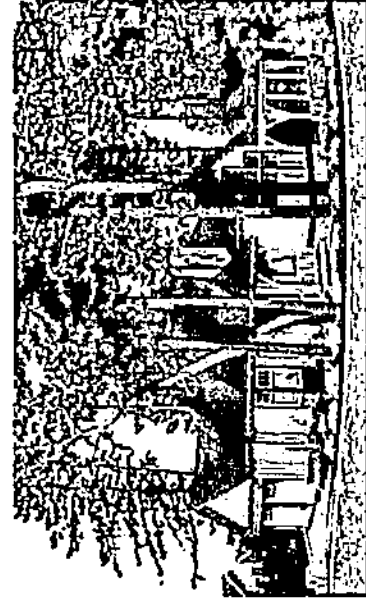
Clustered high density building forms



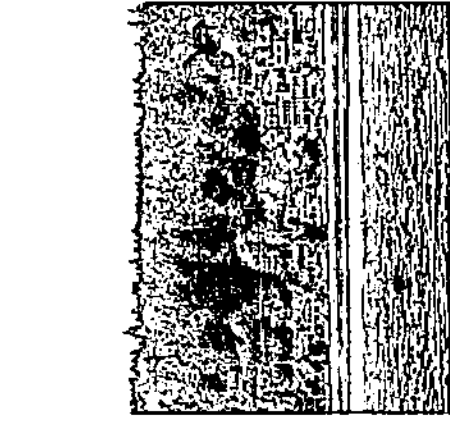
Properly functioning creeks



Natural creeks



Hillside lots that preserve trees where possible



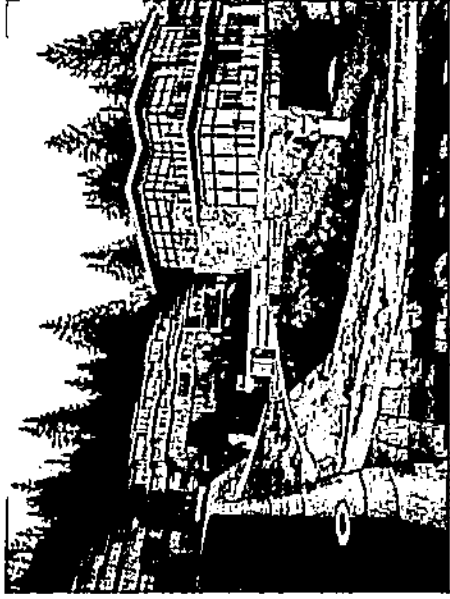
Shoreline views



Bed & breakfast accommodations



Lake enjoyment



Clustered and terraced medium density building forms



Natural trail networks



Preserve the character of Langford's lakes



Enhanced trails

→ Hillside or Shoreline Areas

Objective 3.11 Retain significant open space and maintain ecosystem values.

Policy 3.11.1 When considering development on greenfield sites, retain a minimum of 40% of the site area as public and/or private open space. 25% open space retention will be considered if other conditions that are provided deliver significant community benefit such as affordable housing are met. If an NAP or NCP is in place, each subdivision application will benefit from the overall conditions of the NAP as it relates to open space retention on an area-wide basis. That is, if 40% of the area has been retained for open space through the area-wide plan, individual parcels created through subdivision within designated development areas in the NAP will not be expected to achieve the 40% open space on a site by site basis.

Policy 3.11.2 Ensure connectivity of open spaces so as to create a network that supports water flow (i.e. creeks), trails and/or wildlife movement.

Objective 3.12 Consider view impacts from the valley or from water bodies and from hillside development sites.

Policy 3.12.1 Set targets for view preservation from the valley and creation from hillsides or shorelines when considering development applications that addresses the community's desire to maintain views of key landscape features such as ridgelines, peaks, rocky outcrops, shorelines and environmentally sensitive areas with the strong demand for hillside development sites with views.

Policy 3.12.2 Encourage the use of 3-D modeling as a visual tool for proposed hillside or shoreline developments to guide decisions about siting and massing so as to ensure views to the site (from the valley) are pleasing and contribute to the sense of place.

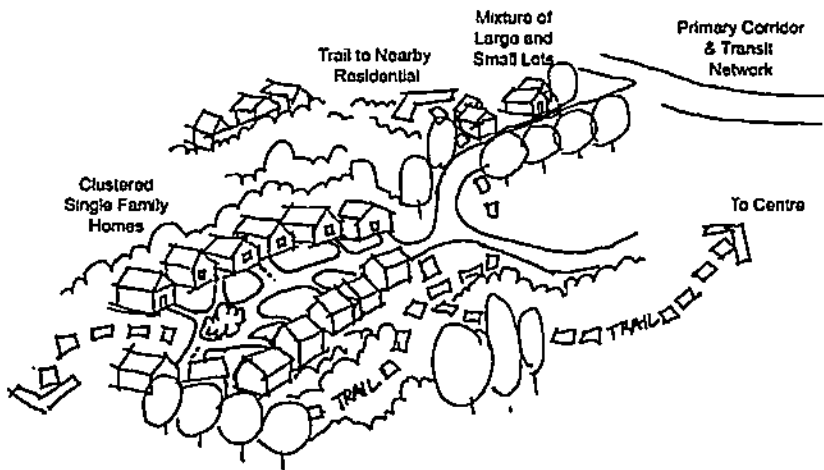


Objective 3.13 Cluster development on hillside sites to facilitate the protection of all or a significant portion of open space and/or environmentally sensitive areas.

Policy 3.13.1 Lower density development sites such as single detached homes and townhouses shall integrate public and private open space and/or preserved historical or environmentally sensitive features extensively in and around development sites.

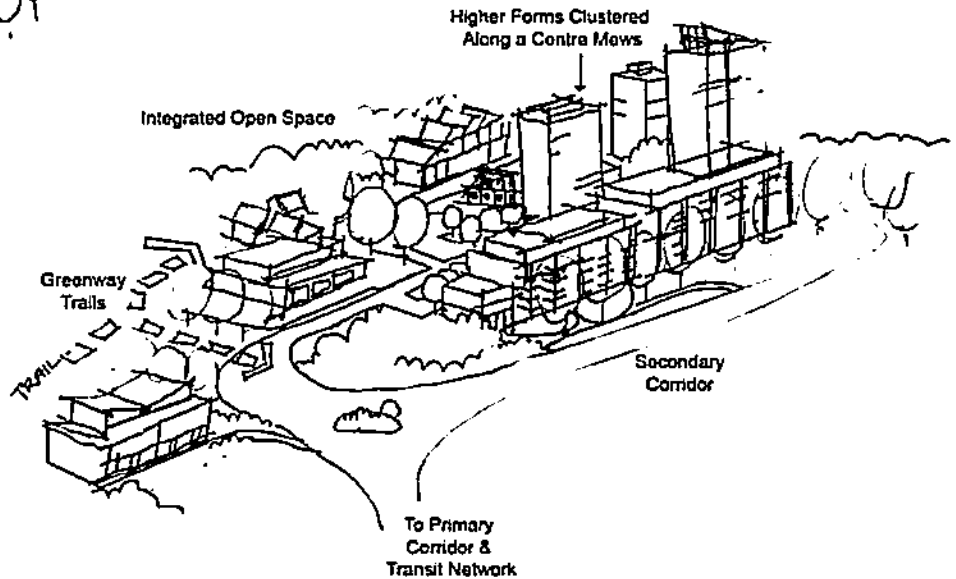
Policy 3.13.2 Higher building forms, such as point towers, will be permitted to maximize open space provided some of the following conditions are provided:

- i. Retention of 50% of ESAs is achieved on an area wide basis.
- ii. Designation of 40% minimum open space on an area wide basis.
- iii. Parking for residents is structured or covered.
- iv. Podium (or base buildings) are included with active uses at grade subject to topography. Acceptable active uses include retail, commercial, private amenity space, or ground-oriented dwelling units.



Low Density Clustered Development
Development that concentrates buildings and infrastructure into a series of development pods that integrates public open space and/or preserved historical or environmentally sensitive features.

High Density Clustered Development
Development that concentrates tall buildings and infrastructure into a very compact development pod that is contained within public open space and/or preserved historical or environmentally sensitive features.



Objective 3.14 Develop hillside or shoreline appropriate standards.

Policy 3.14.1 Modify infrastructure standards to use terrain adaptive techniques for hillside projects by amending the Subdivision, Development and Servicing Bylaw to permit:

- i. Single-loaded travel lanes or one-way streets
- ii. Very low design speeds
- iii. Narrow and/or steep local roads with parking pull-outs
- iv. Very steep driveways
- v. Sloping boulevards with fewer sidewalks
- vi. Xeriscaped boulevard landscaping
- vii. Low-impact development drainage
- viii. Pumping for sanitary and storm services

Objective 3.15 Preserve the character of Langford's lakes.

Policy 3.15.1 Create lakefront-specific subdivision regulations, which may include:

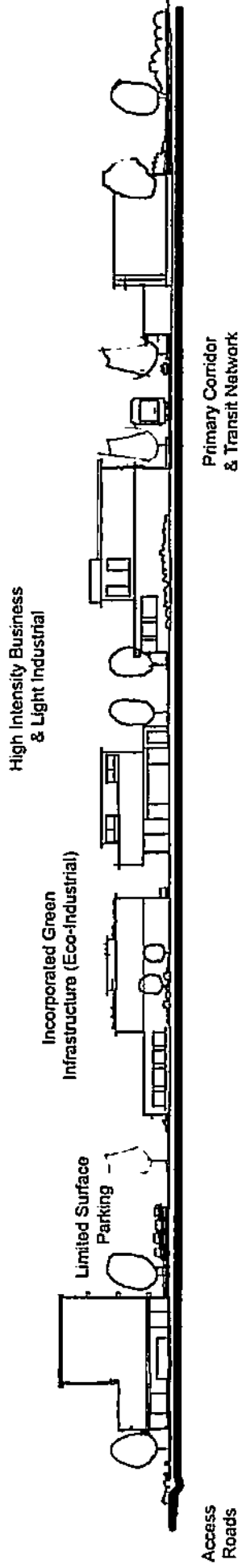
- i. Encourage park dedication to be taken adjacent to lakes, and encourage creation of trail access, boat launches, and viewing platforms, to provide public access to and for the enjoyment of the lakes.
- ii. Exclude streamside protection and enhancement area (SPEA) from minimum lot size calculation
- iii. Create larger minimum lot size for lakefront lots and allow slightly smaller minimum lot size where SPEA is gifted to City as park.

Business or Light Industrial Centre

Predominantly existing business parks and light industrial areas.

- Predominantly business and light industrial precinct that supports a range of business uses
- Parks, open spaces are integrated throughout the centre where appropriate to serve users and employees and green corridors (creeks, wildlife corridors, trails, etc.) that connect to other parts of the community where appropriate.
- Transit stops are located where appropriate

A Concept for Business or Light Industrial Centre



→ Business or Light Industrial Centres

Objective 3.16 Ensure long term supply of employment lands.

Policy 3.16.1 Develop and maintain an inventory of commercial and industrial floor space and remaining capacity to ensure that an adequate supply of employment lands is maintained.¹

Policy 3.16.2 Ensure long term availability of land for light industrial, commercial and/or institutional uses.

Policy 3.16.3 Promote high intensity business and light industrial development with smaller setbacks, smaller lots, multi-storey light industrial facilities.

→ Other Uses

- **Agricultural Strategy Lands:** Areas that are currently farmed and/or part of the Agricultural Land Reserve.²
- **Open Space:** Private and public open space.

¹ While designating lands alone will not foster economic growth and employment, it does allow for both the community to respond positively to future demand.

² Note: Most areas that are currently farmed in Langford are not in the Agricultural Land Reserve (ALR) and are not shown on Map 2.

→ Allowable Density for Centres

The following table identifies permitted density in terms of floor space ratio (FSR) for the areas designated as centres. Land use and density will be determined through zoning.

Allowable Floor Area Ratio		
City Centre	1.5	Greater densities will be permitted provided some of the following is provided as part of a development application and/or amenity package*: <ul style="list-style-type: none"> • Affordable Housing • Green building standards certification (ie. BuiltGreen BC, LEED-NC™ or LEED-ND™ or other) • Open space preservation and rehabilitation • Child care facilities • Park or public square development • Public art • Community safety initiatives • Agricultural land restoration, enhancement or designation • Community shuttle service • Community gathering places such as cultural facilities, meeting spaces • Other amenities as listed by Council policy
Village Centre	1.5	
Neighbourhood Centre	1.5	
Mixed-Use Employment Centre	2.0	
Business or Light Industrial Centre	1.75	

* These amenities may be provided unevenly on individual properties as long as they are achieved on an area wide basis as defined by a Neighbourhood Area Plan.

Part II: Community Objectives & Policies

4.0 Our Natural Setting, Parks & Open Spaces

A community's natural setting, parks and open spaces are form-makers for creating sustainable communities. Langford is located within the Coastal Douglas-fir zone, found only in the southwest corner of British Columbia. Major watersheds include the Colwood, Millstream and Bilston Creek watersheds. This unique bio-geoclimatic zone includes many ecosystems including mature Douglas fir and Garry oak forests and associated ecosystems. This breathtaking natural setting is also defined by the sea, mountains, lakes, creeks, streams, and community parks. The amount, proximity and quality of these places reinforce a unique identity and sense of place. Continued access to these places is a significant aspect of the livability in the region and a top priority of local residents. The biodiversity within these places is also a vital concern.

→ Challenges & Opportunities

- With the projected doubling of the community's population by 2028, not only must we meet the needs of current residents but we must plan for the next generation and newcomers.
- As the community grows, some unprotected green spaces, including environmental sensitive areas, will be not preserved. But development in these areas will create opportunities to designate significant parks, open space connections, and natural areas in community trust in perpetuity.
- The current parks and open space system lacks connection and integration internally and externally to the surrounding context.
- A desire for less formal outdoor recreation and recreation integrated with the day-to-day routines of residents has been expressed.
- Higher residential densities will require changes in the nature of the form and programming of our parks.
- Climate change will bring increased fire risk and drought, and we must address the *wildfire interface* while protecting natural areas.

→ OCP Strategy

Develop an interconnected network of parks and open spaces that support biodiversity and recreational uses that promote healthier lifestyles.



→ Natural Systems, Habitat & Biodiversity

Objective 4.1 Ensure the long term health of environmentally sensitive areas (ESA).

Development Permit Areas are defined in Part IV of this OCP (Bylaw 1200)

Policy 4.1.1 Maintain all ESAs as part of the ESA Development Permit Area (see Appendices).

Objectives and guidelines relating to site planning environmental protection and protecting development from hazardous conditions are located in Bylaw No 1201

Policy 4.1.2 Update mapping of ESAs as appropriate as part of ongoing planning and development initiatives as new information is obtained. ESAs should include, but not necessarily be limited to:

- i. Endangered and threatened habitats such as Garry Oak ecosystems and/or wildlife corridors
- ii. Riparian corridors, wetlands, shoreline habitats
- iii. Heritage landscapes

Policy 4.1.3 Update Development Permit Area guidelines, based on new technical and scientific data and best practice guidelines as they become available.

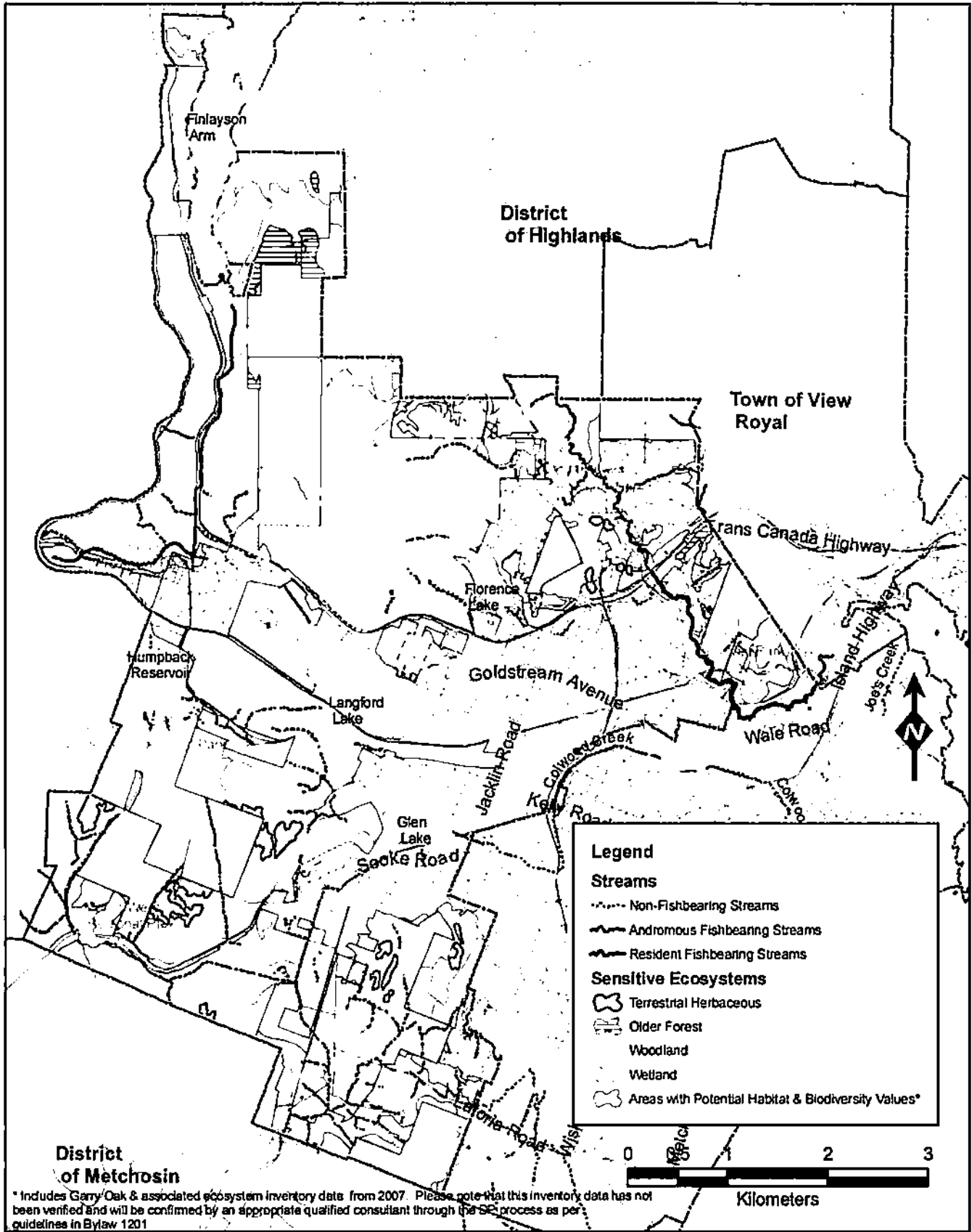
Policy 4.1.4 Limit or restrict access to ESAs through sensitive design. Where access is restricted, encourage development of appropriate interpretive media to explain why access is restricted and the ecological significance of the sensitive ecosystems.

Policy 4.1.5 Where development occurs in and/or around ESAs, negotiate targets with development proponents for ESA retention, enhancement (including removal of invasive species), connectivity and/or compensation. As much as possible, ensure retained ESAs on development parcels are placed into public trust or ownership.

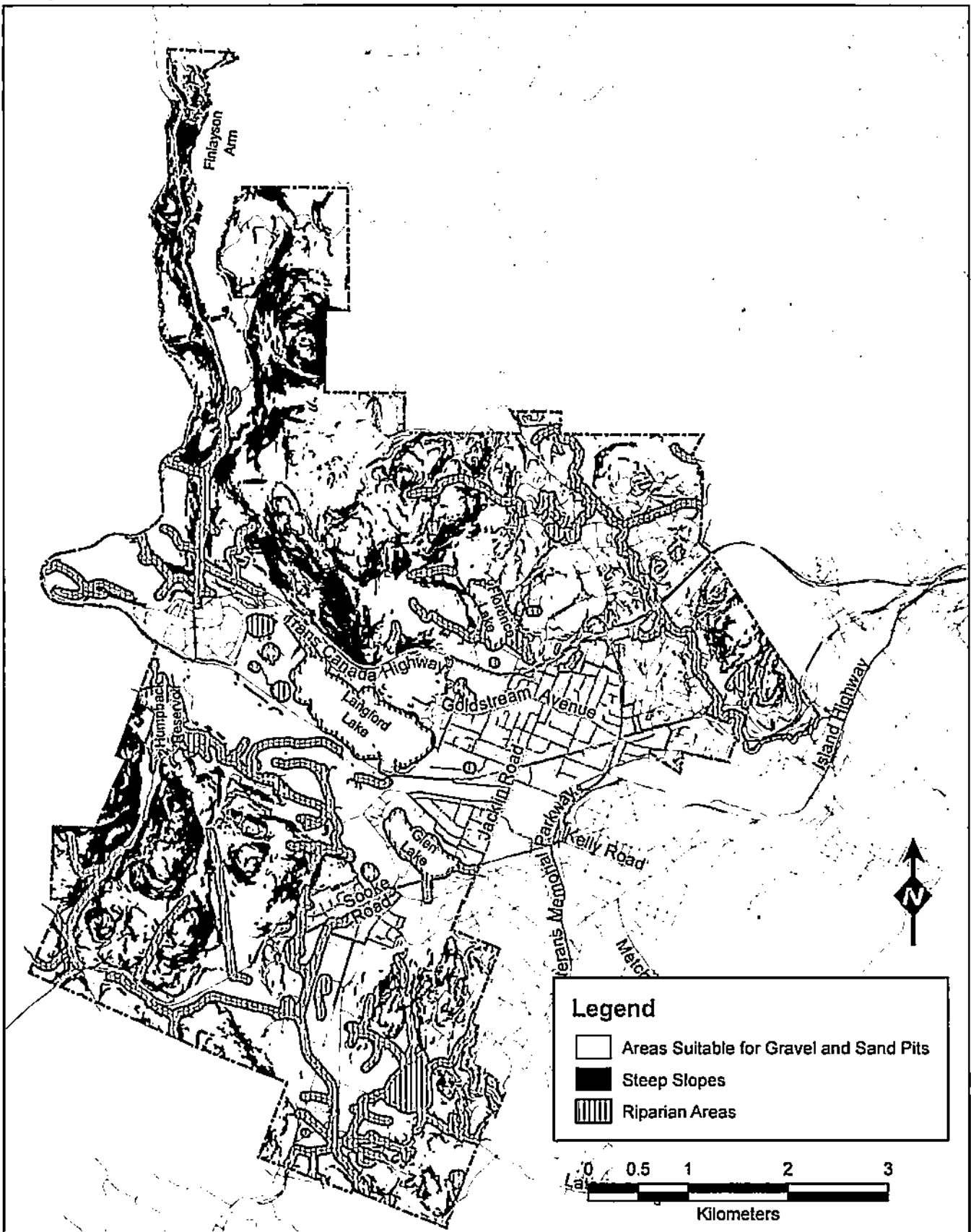
Policy 4.1.6 Continue to support the efforts of senior government agencies and community organizations to restore damaged habitat and sensitive ecosystems.

Policy 4.1.7 Continue to support and encourage individuals and community organizations to be involved in managing sensitive ecosystems, removing invasive species, restoring and enhancing native habitats, planting appropriate native vegetation, preventing erosion, and installing signs to inform and educate the public.

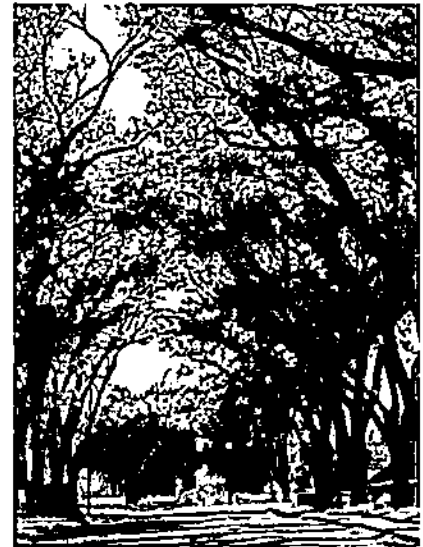
Map 3: Sensitive Ecosystems & Habitat Areas



Map 4: Natural Features



- Objective 4.2** **Maintain a healthy urban forest.**
- Policy 4.2.1** Set targets for canopy coverage in built areas that result from street trees, urban forests and/or trees on private property.
- Policy 4.2.2** Adopt policies for trees and forest maintenance and health in the public realm.
- Policy 4.2.3** Establish a tree management bylaw or policy that will guide decisions related to tree cutting, retention, replacement and planting for private developments.
- Policy 4.2.4** Enhance wildlife biodiversity within the urban forest and landscaping through choice of vegetative species, planting locations and landscaping cover and density.



Mature street trees contribute to character, shade buildings during summer months and provide shelter from the elements.

→ **Integrated Parks and Open Space System**

- Objective 4.3** **Ensure natural areas, parks and open space are connected in a network consistent with the *Colwood & Langford Natural Setting, Parks & Open Space Integration Strategy (Map 6)*.**
- Policy 4.3.1** Maintain a Parks Master Plan (PMP) in order to guide decisions related to property acquisitions, planning, development, design, and delivery of amenities, programs and services. Ensure integration of the Parks Master Plan with the work of the West Shore Parks & Recreation Society and other park planning bodies. The PMP should also define, but not necessarily be limited to:
- i. Standard of park area (ha) per 1000 people
 - ii. Proximity and walkability criteria (e.g. parks with 400m walk from all homes)
 - iii. A range of park sizes, experiences and future use potentials
 - iv. A parks & open space classification system
- Policy 4.3.2** Acquire parklands proposed within the Colwood & Langford Natural Setting, Parks & Open Space Integration Strategy. During acquisitions or dedications, meet separate targets for active parkland, environmental protection and commuter purposes.
- Policy 4.3.3** Work with the adjacent municipalities and other agencies to plan and develop a 'West Shore Greenbelt' that envelops the urban areas of Colwood & Langford. The greenbelt will be defined by a collection of connected open spaces that will include parks, natural areas, ESAs,

trails, agricultural lands as well as developed areas that have habitat and/or open space value.

Policy 4.3.4 Maintain density bonusing strategies as part of parkland dedications with the intent of optimizing all available land acquisition and designation instruments.

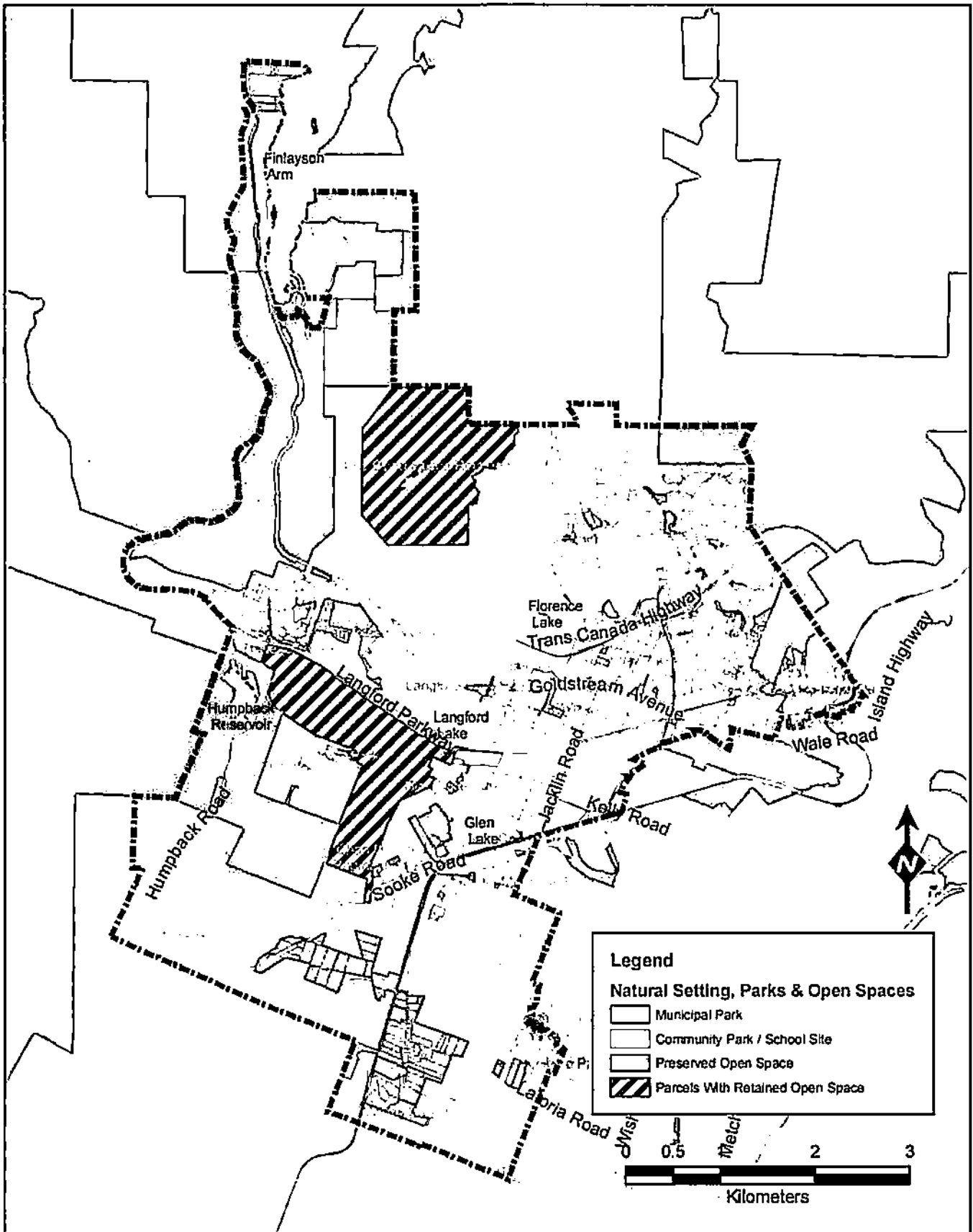
Policy 4.3.5 Ensure park planning and design remains integrated with broader community planning initiatives related to land use, residential development, transportation and provision of community amenities, including:

- i. Integrated social, celebratory and contemplative spaces within a rich fabric of public realm uses and programming.
- ii. Integrated public art (elements and programming) with other social and cultural services and programs (e.g. youth mural program)
- iii. Integrated interpretation of heritage, the environment, First Nations culture, veteran and local immigrant culture in the design of parks and public places.
- iv. Create a legacy of rich public gathering places (e.g. places for community celebration and civic engagement)

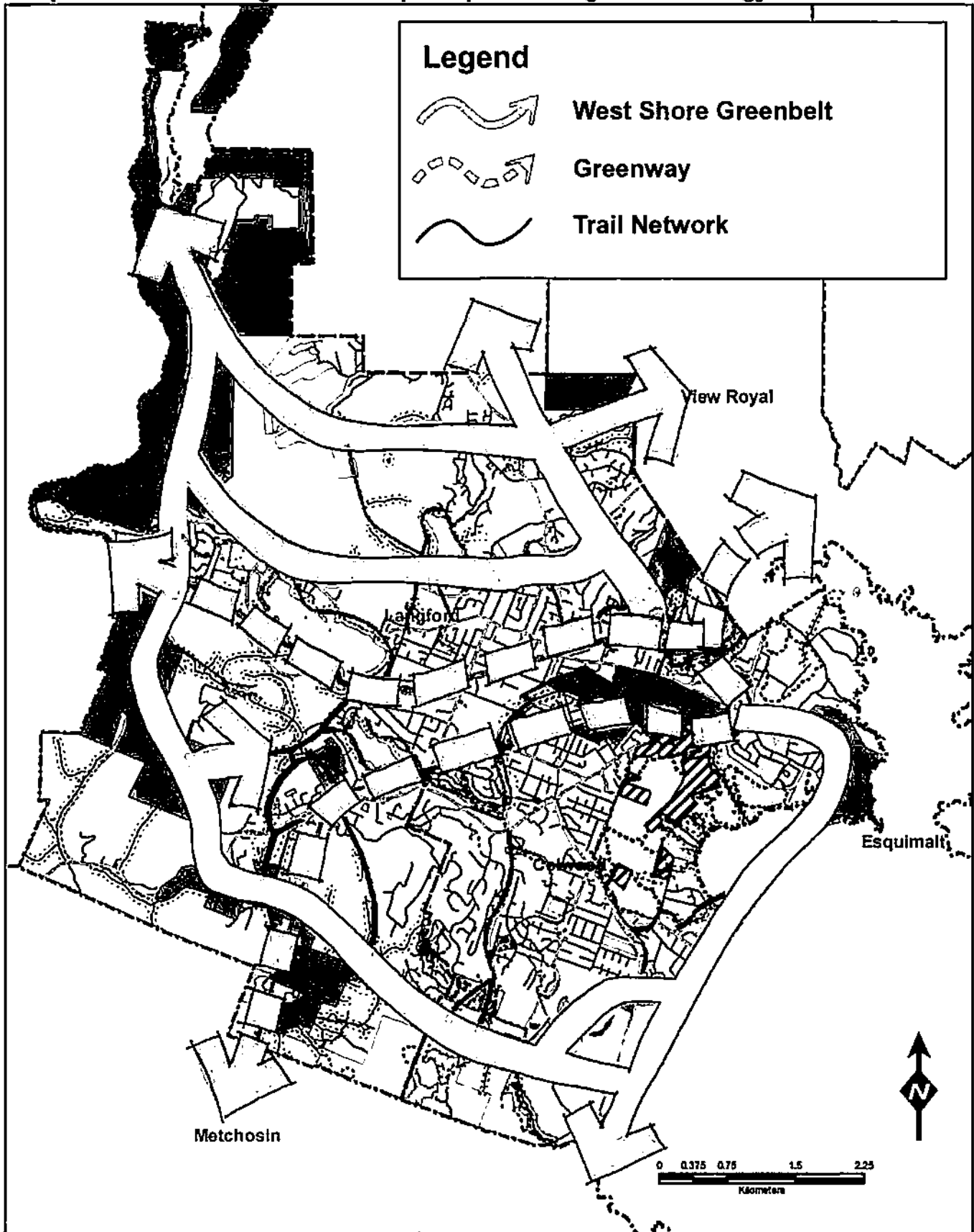
Policy 4.3.6 Ensure movement of wildlife within West Shore Greenbelt such that fencing, roadways, and development do not cause undue impediment.



Map 5: Natural Setting, Parks & Open Spaces Inventory



Map 6: Natural Setting, Parks & Open Spaces Integration Strategy



Objective 4.4 Develop greenways that integrate recreational opportunities and active transportation options.

Policy 4.4.1 Link parks and open space to natural, commercial and social amenities through greenways.

Policy 4.4.2 Exploit options for cross-jurisdictional greenways, habitat corridors, recreational, and cycling and pedestrian network connections.

Objective 4.5 Develop waterfront parks and/or ensure opportunities for public access.

Policy 4.5.1 Maximize public access to the ocean, lakes and creeks. Pursue city ownership wherever feasible. Where ESAs have been identified, limit or restrict public access as appropriate.

Policy 4.5.2 Universally apply requirements under the Land Title Act that stipulate access to water bodies at the time of subdivision (BC Land Title Act Section 75 (1) (c).)

Policy 4.5.3 Equally important to providing public access to ocean, lakes and creek is



The Galloping Goose

→ Landscaping

Objective 4.6 Convert to universal use of plant species indigenous to the area and region.

Policy 4.6.1 Maintain and adopt appropriate City policies to promote native habitat restoration, removal of invasive species and other sustainable landscaping strategies, including:

- i. Changing mowing and other maintenance practices
- ii. Integrated Pest Management strategies
- iii. Xeriscaping strategies
- iv. Urban forestry strategies

- Policy 4.6.2** When undertaking restoration of habitat areas, removal of invasive plant species will be undertaken. Species of plants indigenous to the area and region will be integrated into restoration practices.
- Policy 4.6.3** Promote use of species of plants indigenous to the area and region for private developments.
- Policy 4.6.4** Celebrate native plant species in parks and in the public realm (boulevards, medians, etc.).

5.0 Our Built Environment

Urban design of the built environment focuses primarily on the interface between public (streets, open spaces) and private realms (buildings, yards). We also look at the relationship of buildings to each other, in terms of site design, building form and character. The design of the built environment has a significant influence on the community's sustainability performance. Where distances between homes and workplaces or parks and shops are too great for walking or cycling, a car or transit must be taken which in turn influences a community's GHG emissions. Residential density largely determines the viability of local businesses and community services that serve residents such as schools and recreation facilities. Langford will continue to shift over time our pattern of development to provide better connectivity and mixed use in order for the community to meet sustainability objectives.

→ Challenges & Opportunities

- While newer development projects are being planned with a diversity of uses that will encourage walking, the bulk of residents remain in single use, auto-oriented neighbourhoods. Existing patterns, connections, and relationships between land uses, buildings, streets, and parks are generally such that walking and transit frequency is not convenient.
- New development is generally greenfield and brownfield in some areas. Infill opportunities should be continued to be supported in order to bring better services and amenities closer or within to existing neighbourhoods.
- New forms of higher density housing are causing concerns about height, location, character, etc. and other offsite impacts such as views and shadows. Responding to residents' concerns while educating them on the benefits associated with mixed use high density development will help optimize integration of higher density centres into the community.

→ OCP Strategy

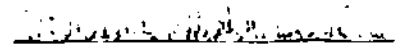
Create dense, compact, and complete centres that are walkable at all scales.

Centres will:

- *Support all modes of transportation;*
- *Be well-connected to all parts of the city for all modes and defined by fine-grained street, cycle and pedestrian networks;*
- *Ensure amenities, services, open spaces and jobs are within walking distance from homes.*
- *Provide for greater housing diversity*
- *Feature high quality, safe and pedestrian-scaled streets that contribute to positive social interactions and safe environments.*

→ Urban Design

- Objective 5.1** Ensure development in and around centres supports a nodal pattern of development.
- Policy 5.1.1** Support the focus of centres by locating high intensity development in and around centres and generally decreasing intensity away from them where topographical conditions permit.
- Policy 5.1.2** Ensure zoning for centres emphasizes appropriate building massing, density and form, and does not unduly restrict land use, in such a way that reinforces the nodal development pattern of centres.
- Policy 5.1.3** Relate all neighbourhood development and infill to the nearest centre through direct and/or improved connections, scale and character.
- Objective 5.2** Encourage the growth of business activities in all parts of the community as a mixed-use urban design objective.
- Policy 5.2.1** Define performance criteria that enables business uses in all parts of the city provided the use, design and siting of buildings and servicing needs are generally compatible with surrounding uses. Consider land efficiency, increased assessment, interface (e.g. buffers), scale and siting, integration, connectivity, and transportation and servicing.
- Policy 5.2.2** Promote the development of purpose-built live/work units that can support retail 'foot traffic' should the owner wish to operate a business on premises.
- Policy 5.2.3** Require a minimum amount of office development as part of mixed-use development projects in the City Centre.
- Objective 5.3** Centres will contribute to walkability at all scales of development.
- Policy 5.3.1** Define walksheds (pedestrian-oriented centres), where the 5 minute walk (approximately 400 m radius from the main street in the centre) supports a wide range of high intensity land uses and the 10 minute walk (approximately 800 radius from the main street in the centre) supports a



Development Permit Areas are defined in Part IV of this OCP (Bylaw 1200)

Objectives and guidelines relating to site planning and building form and character are located in Bylaw No 1201

wide range of residential uses with emphasis on ensuring housing diversity.

- Policy 5.3.2** Ensure new centres are planned and developed to be defined by finely grained and connected road and/or cycle/pedestrian networks to the extent that landscape conditions will allow.
- Policy 5.3.3** Promote walking and cycling in existing areas by developing long term strategies for increasing road, cycle and pedestrian network connectivity.
- Policy 5.3.4** Seek access easements or rights of way dedications at time of rezoning and/or subdivision for creating more finely grained street, pedestrian and/or cycle network connectivity.
- Objective 5.4** Create liveable streets through scale, form, building orientation and character elements.
- Policy 5.4.1** Encourage street-orientation of buildings along all roads and corridors where topographical conditions allow.
- Policy 5.4.2** Make extensive use of regulatory and non-regulatory tools to ensure that all streets in the community are designed to include public and design amenities such as:
- i. Street trees
 - ii. Sidewalks or pedestrian trails
 - iii. Street definition and enclosure defined by a strong street wall that is at a human scale
 - iv. Transparent storefronts with significant fenestration
 - v. Weather protection for pedestrians and furniture such as awnings and/or street trees.
 - vi. Street furniture such as seating areas, planters, garbage receptacles, bicycle racks and kiosk stands (for posters)

- Objective 5.5** **Ensure centres are transit-supportive.**
- Policy 5.5.1** Ensure that the design of primary roads in new developments which are conducive to fast and direct transit service.
- Policy 5.5.2** Ensure residential densities and the location of dense development increases viability and frequency of transit service.
- Policy 5.5.3** Ensure street design and private development integrates transit stops into or adjacent to the site and provides high quality transit-rider amenities such as street furniture, lighting, shelters, and retail opportunities where appropriate.
- Policy 5.5.4** Ensure cycling infrastructure is accommodated in development projects and street design near transit stops.
- Policy 5.5.5** Encourage the development of pedestrian pathways in areas that do not have direct walking links to transit through the road network. Examples include pathways at the end of a cul-de-sac that connects to another street or pedestrian connections through open spaces.
- Policy 5.5.6** Ensure transit stops are active and safe by locating them adjacent to local serving retail and mixed use development.
- Policy 5.5.7** Ensure location of development and choices about residential densities are supportive of and do not preclude implementation of rail-based transit along the E & N Railway.
- Policy 5.5.8** Higher density rezonings above the allowable density (see page 39) may be required to upgrade the local bus stop (within 200-400m walking distance) with amenities such as shelters, accessibility features or pedestrian lighting.

While every jurisdiction is different, most experts agree that a minimum overall density target for centres of 37 units per hectare (15 units per acre) and/or an employee density of 185 employees per hectare (75 per acre) will increase transit ridership significantly (Developing Around Transit" Urban Land Institute 2004)



*Transit Village, Plano, Texas:
Exemplary transit-oriented development near commuter rail. Pedestrian-oriented retail, office and residential village adjacent to a commuter rail line*

- Objective 5.6** **Ensure a range of public spaces are distributed and integrated across the city and in centres.**
- Policy 5.6.1 Ensure parks and open spaces are integrated into the built environment.
- Policy 5.6.2 Promote the development of high quality public squares or parks near or adjacent to civic, commercial or institutional buildings that can support formal and informal activities.
- Policy 5.6.3 Ensure integration of play spaces at all scales, to encourage both formal and informal play.
- Objective 5.7** **Ensure accessibility is considered for planning and design of buildings, the public realm and public parks.**
- Policy 5.7.1 Develop *universal design* guidelines for the public realm, and ensure these are consistent with our other urban design goals.
- Policy 5.7.2 Encourage universal design measures for private development in the public and private realms.
- Policy 5.7.3 Require adaptable design of all new multi-family residential housing units.
- Objective 5.8** **Ensure viability of sustainable infrastructure technologies.**
- Policy 5.8.1 Ensure residential densities and infrastructure planning can support green or efficient district energy systems at the time of development.
- Policy 5.8.2 Ensure buildings and infrastructure systems, including roads, allow for ease of adaptability and retrofitting.

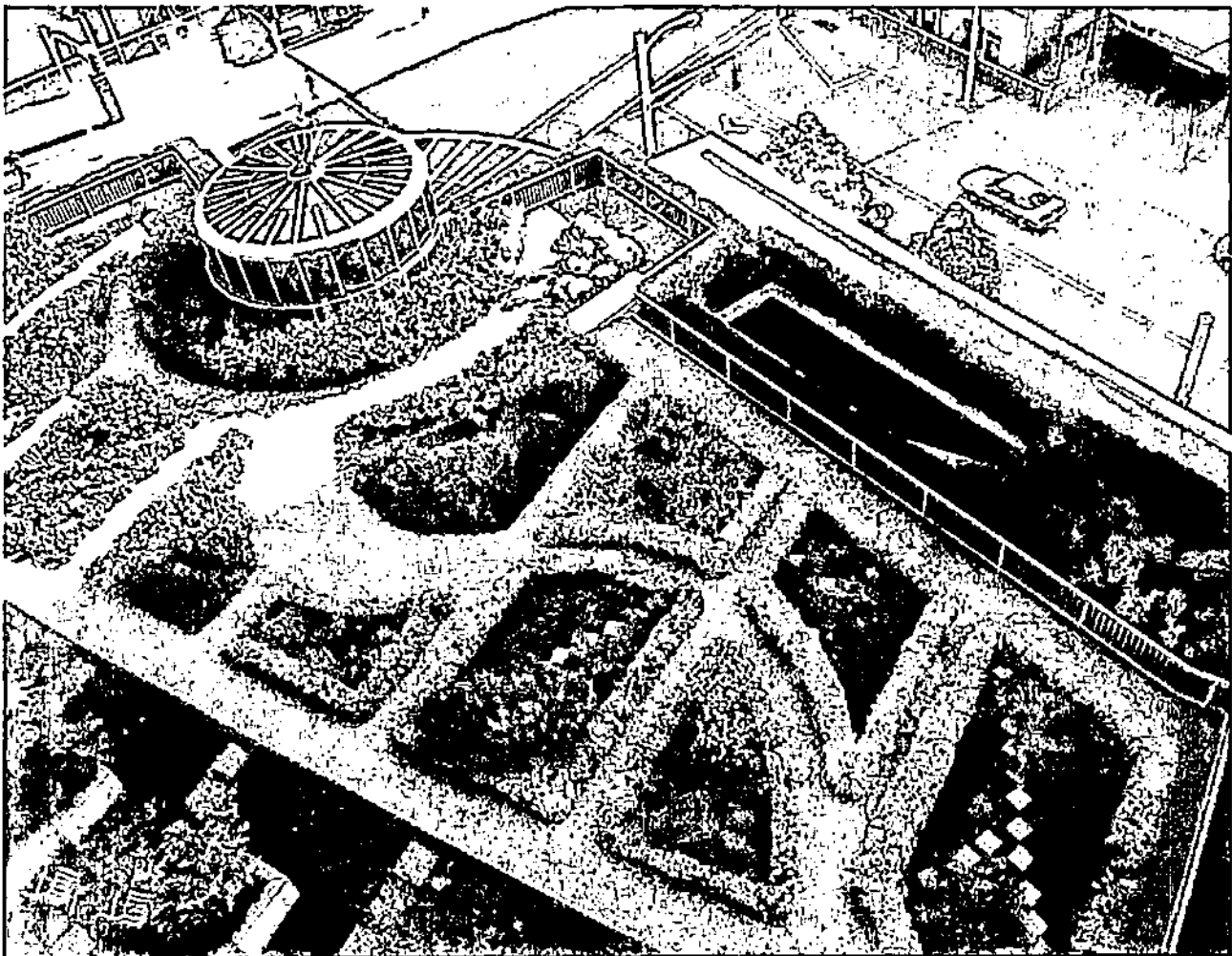
- Objective 5.9 Promote a greater sense of place that celebrates our community's unique setting and people.**
- Policy 5.9.1 Ensure architecture and landscape design reflects local climate, topography, and history.
- Policy 5.9.2 Encourage community input into strategies for creating and celebrating our identity and sense of place.
- Policy 5.9.3 Encourage adaptive reuse or preservation of historical land uses, historical sites or architecturally significant buildings.
- Policy 5.9.4 Promote a seasonal or temporary space (e.g. street closure) in the public realm that concentrates activities for the enjoyment of residents and visitors.
- Policy 5.9.5 Encourage the installation of public art on or within public buildings and property, including public waterfronts, parks, and streets. Public art may include permanent and temporary installations of statuary, murals and other visual art displays reflecting local culture.
- Policy 5.9.6 Enhance existing neighbourhoods to ensure they retain and/or enhance the community's character.
- Policy 5.9.7 Support the expression of unique cultural and sub-cultural groups (community gardeners, skateboarders, etc.) in the public realm.
- Objective 5.10 Promote urban agriculture in the built environment.**
- Policy 5.10.1 Create infrastructure for food preparation, outdoor eating, and special event areas that enable people to build community and celebrate food.
- Policy 5.10.2 Specify plants for landscape design that produce food or herbs wherever possible in planters, hedges, shrub beds, or trellises.
- Policy 5.10.3 Locate productive plants in areas that receive an appropriate amount of sun wherever possible.
- Policy 5.10.4 Promote tidy and attractive urban agriculture opportunities in highly visible, public, or semiprivate spaces.
- Policy 5.10.5 Create urban agriculture opportunities in places of education such as schools and community centres so that urban agriculture may foster a connection between

children and the process of growing, harvesting and eating fresh produce.

- Policy 5.10.6 Promote a wide range of urban agriculture practices that showcase food growing opportunities especially those that are relevant to private residential spaces that define the area (e.g. containers, rooftops, balconies and patios).
- Policy 5.10.7 Utilize vertical and rooftop growing spaces for urban agriculture.
- Policy 5.10.8 Design urban agriculture spaces so that they are accessible to all people.
- Policy 5.10.9 Design urban agriculture spaces so that enhance natural habitat for beneficial insects, birds, and natural predators of garden pests.

Grow food everywhere!

Food can be grown on roofs, balconies and in private and common yards and parks.



Rooftop herb garden at the Fairmont Hotel in downtown Vancouver

6.0 Our Healthy Community

Development has significant impacts on a community's social development and the health of its residents. Residents recognize that unprecedented levels of population growth are putting pressures on the community and leading to real and perceived concerns related to issues such as crime, health, and safety. We need to address growth benefits all members of the community equally without compromising certain groups such as youth, children or seniors. Ultimately, residents strongly value the "small town feel", family-oriented nature, and unique and diverse people that define the West Shore.

→ Challenges & Opportunities

- High levels of growth and a desire to maintain the "small-town" family-friendly feel in the community will demand participation amongst all groups to increase community acceptance of change.
- Ensure development choices support community social objectives.
- Ensuring strong social infrastructure and cohesion will be critical as the community continues to expand and meet new challenges.
- Our arts, culture and heritage initiatives largely lack focus, facilities and funding, despite strong formal and informal interest and community activity in this area.
- Housing affordability is related to the cost of housing and household income. We require unique strategies to address the needs of low income families and 'the working poor' to ensure their short and long term success in the community.

→ OCP Strategy

We will account for the needs of all members of the community with emphasis on children, youth, seniors and low income families.

The city will aim to:

- *Provide high quality services*
- *Ensure a safe and secure environment for all members of the community*
- *Develop and promote arts, culture and heritage to better define the community's unique people and sense of place in the region.*

→ Social Development

Objective 6.1 Increase social planning activities.

Policy 6.1.1 Establish a cultural and/or social planning council whose mandate will be to develop strategies, review future development projects and advise staff and council on social, arts, culture and heritage related opportunities for the community or as part of a broader working group representing West Shore communities.

Policy 6.1.2 Optimize and/or utilize existing administrative structures such as West Shore Parks & Recreation Society to work with local groups to develop and execute social development programming in new or existing facilities.

Policy 6.1.3 Support local agencies seeking to implement social improvement projects.

Objective 6.2 Promote community involvement and improve access to facilities, programs and/or services.

Policy 6.2.1 Work towards a West Shore strategy for social development.

Policy 6.2.2 Improve access to and availability of information regarding community services and volunteer opportunities for all residents.

Policy 6.2.3 Increase community access to public and school facilities (through joint use agreements or MOUs with the School District) and ensure that they are available at affordable rates.

Policy 6.2.4 Communicate, collaborate and advocate with other agencies to ensure that senior government social programs are adequately addressing local needs.

Policy 6.2.5 Enhance City policies with respect to supporting communication and cooperation with residents' associations and other grass root organizations.

- Policy 6.2.6** Help build partnerships, alliances and networks among community-based organizations and institutions in order to provide effective services on behalf of residents.
- Policy 6.2.7** Assist agencies and individuals to access grants or funding from senior levels of government to address social needs.
- Objective 6.3** Ensure the specific needs of children and youth are addressed in all aspects of community and social planning and development.
- Policy 6.3.1** Expand and improve inclusive recreational programs for children.
- Policy 6.3.2** Provide funding specific to children and youth leadership, counselling, development, sport, teambuilding programs.
- Policy 6.3.3** Support the development of playgrounds, skateboard parks, day camps, swimming lessons, beach and playground programs.
- Policy 6.3.4** Facilitate the development of childcare and preschool spaces in the community.
- Policy 6.3.5** Work with other agencies to promote and encourage employer-supported child care
- Policy 6.3.6** Work with community agencies and developers to develop housing and/or mixed use projects which include child care facilities and/or services.
- Policy 6.3.7** Ensure maximum potential for the provision of child care facilities in zoning bylaws.
- Objective 6.4** Develop a youth strategy.
- Policy 6.4.1** Develop new ways to involve youth in municipal decision-making processes (e.g. advisory councils, forums).
- Policy 6.4.2** Support community-based youth services and/or programs which focus on skills development, increased opportunities, and development of self-esteem. Ensure that programming is accessible and affordable, and involves youth in program development.
- Policy 6.4.3** Promote or support local service clubs undertaking the establishment of youth drop-in centres.

→ Recreation

- Objective 6.5** **Meet the recreational needs of residents.**
- Policy 6.5.1** Maintain an understanding of community recreational needs through a Parks Master Plan, community surveys, and/or collaboration with community groups.
- Policy 6.5.2** Support the role of the West Shore Parks & Recreation Society as a major provider of recreational services and programs in the West Shore, while adding local recreational opportunities within the City of Langford.
- Policy 6.5.3** Provide a range of recreation programs and services for teams and individuals of all ages in indoor facilities, and outdoor natural and programmed areas.
- Policy 6.5.4** Undertake recreational initiatives such as "Recreation 2010" to provide recreation in Langford.

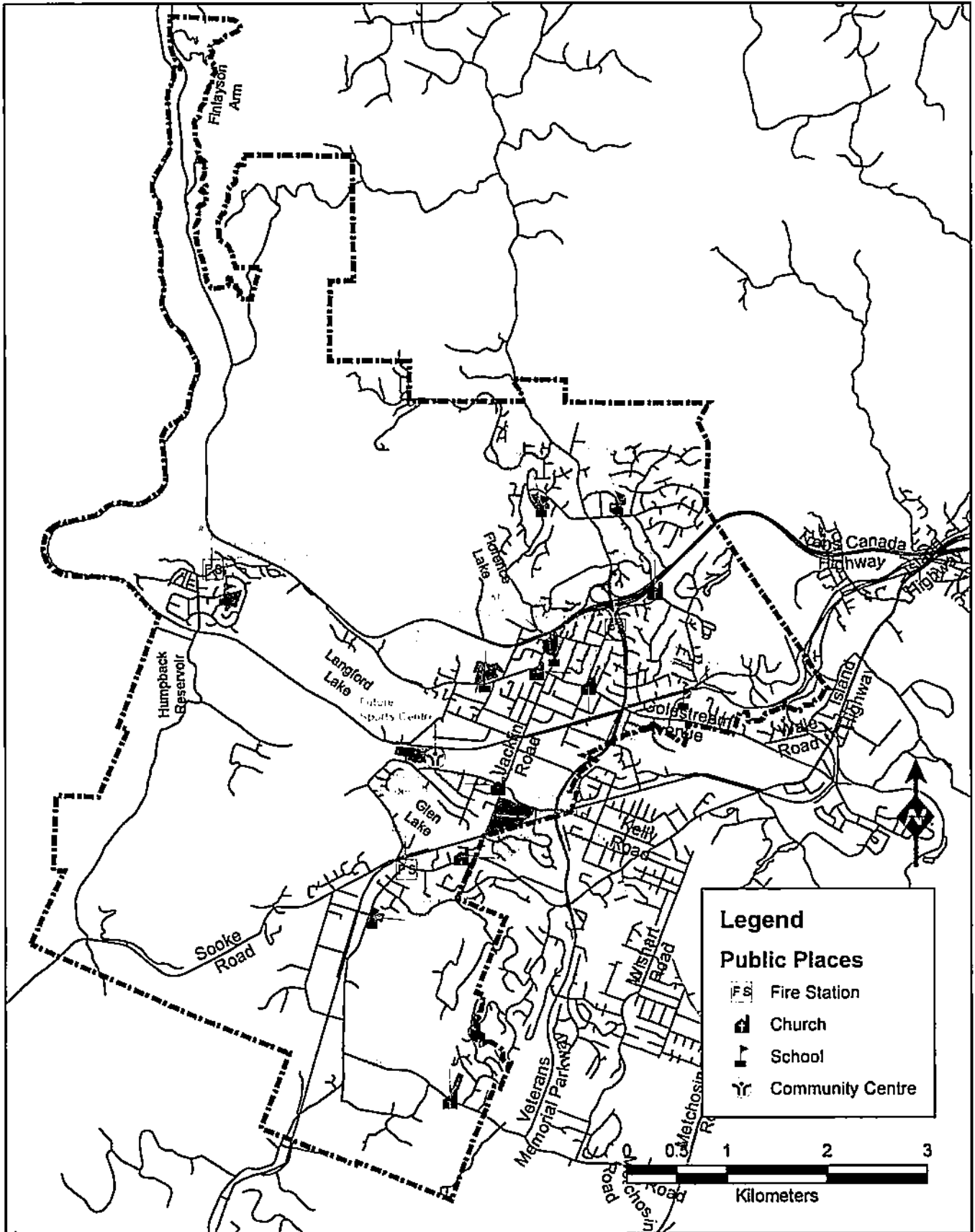
→ Archaeological Heritage

- Objective 6.6** **Ensure protection of recorded and unrecorded archaeological sites from land-altering activities.**
- Policy 6.6.1** Recognize the significance of archaeological findings, which detail the physical remains of past human activities, as a valuable heritage resource.
- Policy 6.6.2** Educate property owners and builders so that they are aware of recorded and potential archaeological sites.
- Policy 6.6.3** Partner with local First Nations and the Province of BC to identify archaeological sites, as well as First Nations traditional use areas and sacred sites.
- Policy 6.6.4** Actively foster a proactive approach with partners and industry in order to reduce site impacts to archaeological resources, as well as associated costs.

→ Childcare, Education & Skills Training

- Objective 6.7** Promote community-based education and skills development programming and facilities.
- Policy 6.7.1** Promote the concept of the West Shore Learning Commons with the principle that it will accommodate accredited and continued education opportunities.
- Policy 6.7.2** Support community groups in the ongoing development and operation of community programs and facilities.
- Objective 6.8** Ensure adequate supply of day care and pre-school space for young children.
- Policy 6.8.1** Support preschool programs and out-of-school programs.
- Policy 6.8.2** Locate new group childcare, public schools and senior serving facilities in close proximity to each other and promote joint programming and volunteer opportunities.
- Policy 6.8.3** Develop policies and targets for provision of daycare space in new large-scale development projects that reflect the needs of existing and new residents.
- Objective 6.9** Ensure life long learning opportunities for all citizens.
- Policy 6.9.1** Enhance bridging opportunities between high schools and post-secondary institutions by way of joint programming, mentoring, orientation sessions, etc.
- Policy 6.9.2** Improve information regarding post-secondary education, including career or program options, eligibility criteria, etc.
- Objective 6.10** Promote the delivery of skills training programs recognizing the need for a skilled workforce for supporting business growth in the community.
- Policy 6.10.1** Work with senior governments to implement local skills training programs for business activities in the community.
- Policy 6.10.2** Work with local business associations and industry groups to participate in mentorship, skills training and career development programs.
- Policy 6.10.3** Promote the development of local trades and technical schools in the community.

Map 7: Institutional Uses & Institutions Inventory



→ Arts & Culture

Objective 6.11 Promote local First Nations culture.

Policy 6.11.1 Create in partnership with local First Nations an inventory of cultural and archaeological sites the city to ensure they are properly protected or recognized, as appropriate.

Policy 6.11.2 Engage local First Nations artists to implement public art features as a part of public and/or private development projects.

Policy 6.11.3 Actively promote involvement of First Nations community members in public celebrations and gatherings as performers, exhibitors or attendees.

Objective 6.12 Provide funding for arts and culture groups, initiatives and programs for both regional and city-focused activities.

Policy 6.12.1 Contribute to the CRD Arts Service / Arts Development Fund to allow local non-profit arts groups to access funding and resources through the regional initiative.

Policy 6.12.2 Develop project grants accessible to local non-profit groups for arts, culture and heritage activities.

Objective 6.13 Promote arts & culture initiatives and activities.

Policy 6.13.1 Work with partners to develop and implement a joint arts and culture marketing initiative to better promote local activities, individuals, groups, events, and programs.

Policy 6.13.2 Highlight the activities of local artists and promote public involvement in arts and culture.

Policy 6.13.3 Continue to link the arts sector via beautification, and arts initiatives, public performance spaces (indoor and outdoor) with while supporting City Centre revitalization and development.

→ Community & Cultural Land & Facilities

Objective 6.14 Support the development and promotion of community facilities.

Policy 6.14.1 Work with community groups and residents to support the development of a centre that would service the entire West Shore as a means to showcase local individuals and groups, and encourage greater programming, performance and education.

Policy 6.14.2 Work with other West Shore communities and other partners to contribute funding to an arts and culture centre (dedicated use and/or multi-use).

Policy 6.14.3 Consider provision of municipal properties or proceeds from sales of properties as a contribution to fund a centre and / or operating endowment.

Policy 6.14.4 Work with School District No. 62 to explore opportunities associated with new and existing joint development / use agreements as a means to address the needs of the broader community in a cost and resource effective manner.

Policy 6.14.5 Work with developers, partners and stakeholders to define and create multi-use facilities such as community centres, performance / practice areas, and gathering places as part of development projects.

Objective 6.15 Ensure availability of affordable venues for artists, arts organizations and cultural groups.

Policy 6.15.1 Encourage the development of private performance and gallery spaces.

Policy 6.15.2 Develop public outdoor places that support programmed or spontaneous performance, art installation, and play.

→ Civic Engagement

Objective 6.16 Seek the participation of all sectors of the community in any decision-making or planning process.

Policy 6.16.1 Actively engage the public to participate and be involved in discussions pertaining to the future of the community.

Policy 6.16.2 Help shape a "civic consciousness" among residents by providing information and education about how the city works and how they can actively become involved.

- Policy 6.16.3 Use creative and flexible communication techniques to out to all parts of the community.
- Objective 6.17 Require development proponents to engage and gather input from the public.**
- Policy 6.17.1 Set goals for public consultation among development proponents which must be fulfilled prior to consideration of development approvals.
- Policy 6.17.2 Require development proponents to engage citizens and communicate how their proposals contribute to the broader Community Sustainability Goals (see Section 2.0).
- Policy 6.17.3 Ensure communications on proposed development from the City and from development proponents address community sustainability goals (see Section 2.0).

→ **Emergency Services**

- Objective 6.18 Provide appropriate emergency services.**
- Policy 6.18.1 To improve long-term fire fighting capacity, consideration will be given to development of appropriate fire hall sites in current and proposed major development areas of the city in cooperation with the Fire Department.
- Policy 6.18.2 Focus interdepartmental cooperation to reduce fire and emergency response times where possible.

7.0 Our Housing

While new projects are delivering newer forms of housing and diversifying housing choices in the community, many areas of Langford remain defined by predominantly single family homes. Greater housing diversity allows for *aging in place* and tenureship choice (own, rent, etc.) and creates economic, racial and family diversity that contributes to a vibrant community. Also, affordable housing is a growing need among residents across BC and especially on Southern Vancouver Island.

→ Challenges & Opportunities

- Escalating housing prices and lack of rental stock has led to very real needs related to overall housing affordability.
- Higher incomes are necessary to purchase homes, which creates a gap between existing and newer residents in relation to income and lifestyle.
- Diversifying housing choices in existing developed areas will ensure opportunities for aging in place, rental housing and greater choice.
- There is a lack of construction of new purpose-built rental housing and a gradual loss of existing affordable housing stock.
- Some 28% of renter households (13,080 households) in the Capital Region are defined as being in "core housing need" - meaning they do not live in, and are unable to access, housing which is affordable, in adequate condition, and of suitable size.
- Incomes have risen by 16% from 2000 to 2006, while housing costs rose 96% in the same period.

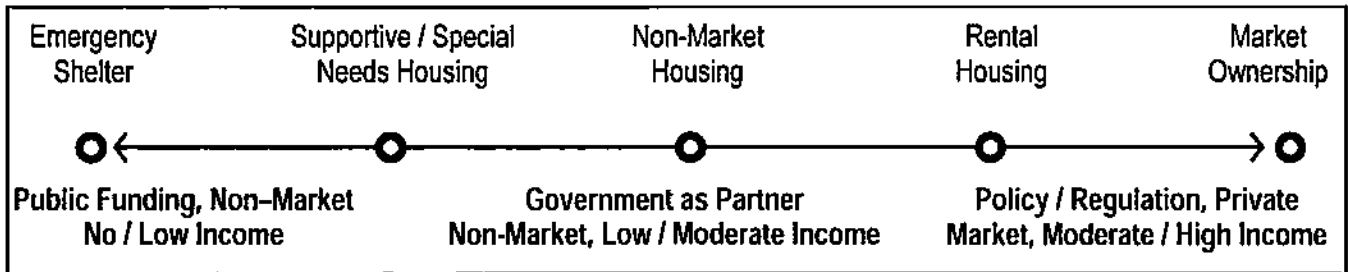
→ OCP Strategy

A range of housing choices along the housing continuum accommodate the needs of all incomes and ages in all parts of the community.

→ Defining the Housing Continuum

The housing continuum recognizes that the delivery of housing must acknowledge that people's housing needs change throughout their life cycle and through changing circumstances in their lives.

The Housing Continuum



→ Defining Affordable Housing

Housing affordability is a function of housing cost and household income.

Affordable housing is defined as housing which has a market price or rent that does not exceed 30% of income households which have income that is 80% or less than the median household income for the community.

Affordable housing can be provided by the private, non-profit, cooperative, and public sectors separately or through partnership models.

Affordable housing includes a variety of tenure models including ownership, rental, co-housing, cooperative and rental.

→ Affordable Housing

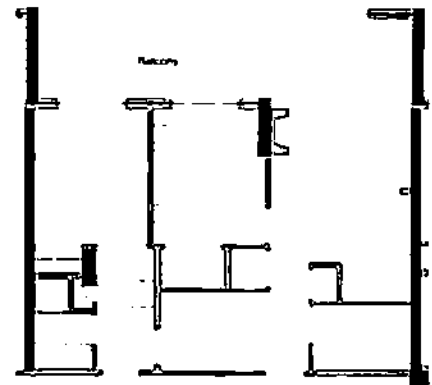
- Objective 7.1** **Preserve and increase the stock of safe and affordable housing.**
- Policy 7.1.1** Regularly maintain the affordable housing strategy which focuses on mechanisms and detailed policies for delivery of affordable housing.
- Policy 7.1.2** Regularly revise the disbursement strategy of funds in the Affordable Housing Reserve Fund.
- Policy 7.1.3** Maintain a density bonus strategy for delivering affordable housing.
- Policy 7.1.4** Form partnerships to gain access to other funding mechanisms and programs from senior levels of government. Maximize opportunities for joint initiatives or delivery of programs and services with other jurisdictions.
- Policy 7.1.5** Seek partnerships with local agencies and governments to access housing programs directed towards specific populations such as seniors, families with children, people with special needs and those at risk of homelessness.
- Policy 7.1.6** Continue to seek development agreements that secure commitments for development proponents to build affordable housing units where cash contributions are not provided.
- Policy 7.1.7** Continue and mandate development of housing with additional dwelling units as secondary suites in all parts of the community in all building types, including multi-family buildings.
- Policy 7.1.8** Streamline approvals process for affordable housing projects.
- Policy 7.1.9** Use municipal land and/or other resources and/or community organizations to deliver special needs housing.
- Objective 7.2** **Support residents in moving through the stages of the housing continuum and facilitating the delivery of a range of housing types along the continuum.**
- Policy 7.2.1** To meet needs of those residents needing emergency shelter and/or supportive housing, the city will work with local social service providers, culture or religious groups, and senior levels of government to locate emergency shelter facilities in the community on an as needed basis.

The city will work with partners to ensure appropriate supportive services are implemented.

- Policy 7.2.2** To meet the needs of those residents needing affordable rental housing, the City may:
- Permit or require secondary suites in new and existing single family housing, including small to housing and fee simple row housing.
 - Permit or require secondary suites in new and existing multi-family residential buildings in the form of 'lock-off' suites or 'mingles'.
 - Seek commitments from development proponents to develop purpose-built rental housing.
 - Work with non-profit groups, special needs groups and/or other governments to plan and develop rental housing.

Policy 7.2.3 For the delivery of affordable ownership housing, the City will take direction from its affordable housing policy.

Policy 7.2.4 To meet the needs of those residents needing market rental and ownership housing, the City will ensure ongoing efficient delivery of approvals so as to ensure housing supply can keep pace with housing demand.



A Lock-off suite (image courtesy of SFU University) is a form of additional dwelling unit in multi-family buildings that acts as a secondary suite. The unit is sold as a two bedroom unit where the blue shaded unit can act as 1.2 bedroom or as a separate one bedroom suite that can be rented if not used or as a mortgage helper. The lock-off suite has its own entrance to the main corridor or from the main suite.

→ Special Needs Housing

Objective 7.3 Identify the full scope of special needs housing in the community and develop strategies for delivery of special needs housing.

Policy 7.3.1 Undertake a community initiative to define special needs housing.

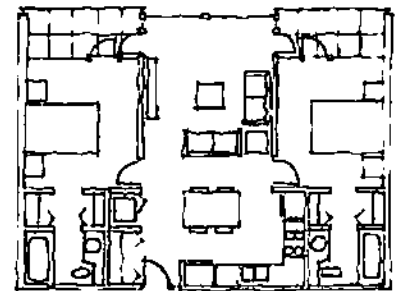
Policy 7.3.2 Use municipal land and other resources to deliver special needs housing.

Policy 7.3.3 Work with partners and/or community organizations to identify and deliver special needs housing.

Policy 7.3.4 Integrate special needs housing throughout the community, with emphasis on locating special needs housing in new and existing centres.

Objective 7.4 Pursue 'aging in place' and accessibility strategies for housing.

Policy 7.4.1 Ensure the needs of our aging population are met by increasing the diversity of housing options in all parts of



A Mingle Suite is another form of secondary suite in a multi-family building. The suite is sold as a two bedroom unit which includes two converted one-bedrooms that share a main living area and kitchen. Each bedroom can be rented if desired.

the community, with emphasis on integrating housing variety in existing and new centres.

Policy 7.4.2 Ensure accessibility and universal design features are an important aspect of housing that facilitates aging in place.

Objective 7.5 **Set targets for family and ground-oriented housing in high-density development projects.**

Policy 7.5.1 Recognizing the family orientation of the community, negotiate targets for family-oriented housing in high density development projects. Family-oriented housing is typically ground-oriented but can include two or more bedroom suites in multi-family apartment buildings.

Policy 7.5.2 To ensure “eyes on the street” and active street life, the City will negotiate targets for ground-oriented housing in high density development projects.

8.0 Our Multi-Modal Transportation System

Langford has long been a bedroom community to Greater Victoria. This characteristic results, in part, from relatively inexpensive housing prices, lack of good quality high paying jobs, and good connections to greater Victoria east of the Colwood interchange (when compared to other major urban centres in the region). As a result, Langford has historically developed as a low density, predominantly single family residential community with some supporting commercial development. Understanding this past is critical in developing strategies for improving conditions in the future. Langford is growing rapidly and is predicted to continue growing rapidly for at least the next 20 years. This growth is resulting in transportation pressures that are most evident through congestion at the McKenzie / Admirals intersection which is due, in part, to the large commute from Westshore to downtown Victoria and the Saanich Peninsula employment areas.

Congestion is also increasing in Langford (Sooke Highway, Veterans Memorial Parkway) despite relatively recent transportation system improvements. Congestion is increasing because the communities are growing faster than the transportation infrastructure is being improved. In some case, congestion is not necessarily a bad thing. Without congestion it would be much more difficult to attract people out of their cars and to use other modes of transportation such as buses, commuter rail, bicycles and walking. Therefore community sustainability goals need to be considered before congestion is addressed through improvements to infrastructure. Economic and environmental issues and public attitudes are also changing our perceptions towards automobile travel. The cost of fuel is rising and is predicted to reach historic highs in the near future. Greenhouse gas emissions are also an increasingly important public issue.

To address these challenges and others, the City of Langford and the Ministry of Transportation commissioned the West Shore Transportation Plan in 2006. The West Shore Transportation Plan included an evaluation of a range of transportation options and recommended a reasonable road network to support travel to, from and within the West Shore communities. Recent transit programs and funding from senior governments will affect transportation infrastructure, modal choices, and travel patterns in the community. The model used for the Plan included an upgraded conventional transit network but did not include rapid transit or commuter rail, concepts that have since become central in the planning of our community.

→ Challenges & Opportunities

- Auto-oriented, single use neighbourhoods that define much of the community promote motor vehicle use which in turn drives a need for more road improvements and parking. Also, provision of effective transit service is difficult and/or very expensive because lower densities result in lower ridership and higher per capita public transit. Resulting infrequent service will not entice people out of their cars and onto transit. Also, low density development is difficult to service without long walking distances to transit stops, another disincentive for people to change from automobiles to transit.
- Many major and arterial roads are congested at peak travel times and this congestion will get exponentially worse as the population grows unless more and wider roads are constructed or people are enticed out of their cars and onto a transit system, bicycle or walking paths.
- Land use decisions will drive the need for transportation infrastructure as people will have to travel to work, shop and play. High density mixed use nodes will provide an opportunity for those who live there to reduce travel and go about their daily lives within one self contained centre. High density mixed use nodes can be more effectively served with good frequent transit service including conventional buses, rapid transit and light rail.
- Transportation infrastructure will drive land use decisions. Any increases in the number or width of automobile focused roadways will attract people away from transit and as a disincentive for implementation of dense mixed use nodes. Building more roadways will only encourage people to live in single family units. For example, replacing the at-grade McKenzie / Admirals intersection with a full movement interchange would make it much easier for automobile users to get to and from downtown Victoria or the Saanich Peninsula for work trips and this could tend to be counterproductive to a strategy to move people onto bus rapid transit. On the other hand transit only improvements would give the transit system an efficiency and service advantage in relation to automobiles.
- Transportation infrastructure is expensive. Investments must be executed wisely and in a way that the community will benefit in accordance with broader community objectives.
- Increasing population within the West Shore will increase travel demand. Demand for automobile travel will increase despite the congestion and the economic and environmental challenges of supporting single occupancy vehicles. Effective transportation planning will reduce (not eliminate) automobile travel growth. The challenge is to create a balance where transit, bicycle and pedestrian modes thrive without the choking effect of major congestion. Some congestion is here to stay and will remain an effective tool that limits automobile travel demand over time.

→ Strategies

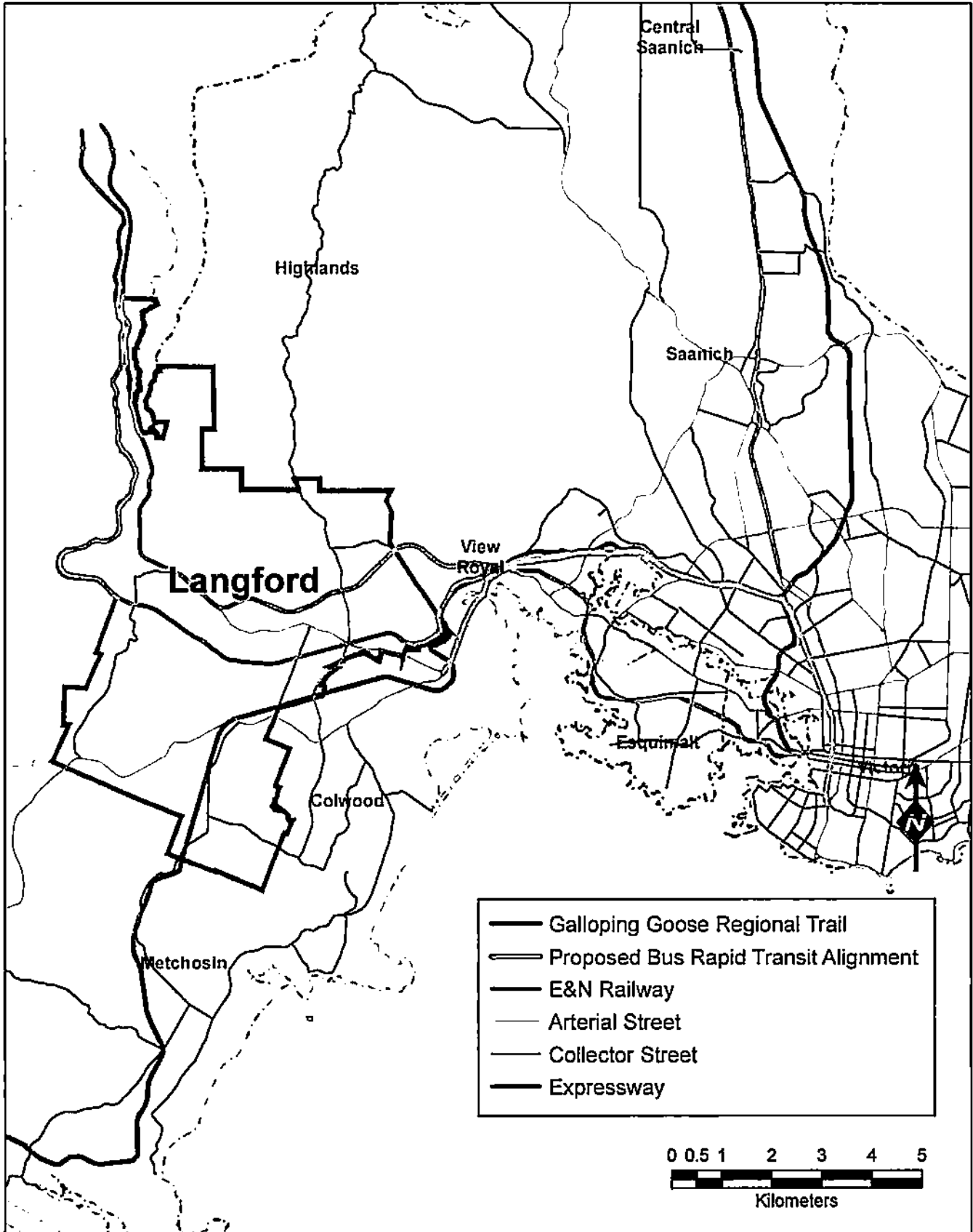
The transportation strategies are closely bound with the growth management and land use strategies. The transportation system needs will be closely allied with the achievement of:

- **Economic Development:** Good quality high paying jobs in Langford will tend to reduce the need to commute to areas east of the Colwood interchange (downtown Victoria and the Saanich Peninsula); (See Section 10.0)
- **Compact Complete Centres:** Dense mixed use (residential, office, light industrial, retail, recreation, entertainment) nodes which promote the use of transit (bus, commuter rail, light rail), cycling and walking (See Section 3.0 & 5.0)
- **Integrated Transportation Systems:** A transportation system that is integrated into the community in an aesthetically pleasing and complementary manner.

Specific multi-modal transportation strategies include:

- Maintain and improve regional connections in and through the community
- Accommodate the needs of local residents for all modes of transportation
- Climate-friendly active transportation (i.e. walking, cycling) will be a priority by developing safe and viable multi-modal transportation networks that link compact and mixed centres

Map 8: Regional Transportation System Context



→ Multi-Modal Transportation Network

Objective 8.1 Implement multi-modal travel corridors that move all users according to the *Multi-Modal Transportation Strategy (Map 9)*.

Policy 8.1.1 The following modal hierarchy will guide decisions about transportation infrastructure improvements and spending (in this order):

- i. Walking
- ii. Cycling
- iii. Transit
- iv. High Occupancy Vehicles (HOVs)
- v. Single Occupant Vehicles (SOVs)

Policy 8.1.2 Develop a “Complete Streets” strategy that increases the multi-modality of planned and current streets, and which:

- i. Ensures all roads are pedestrian-friendly by ensuring safe, attractive, comfortable pedestrian facilities that are buffered from fast moving motorized and non-motorized road users as much as feasible through the use of parking lanes, street trees, boulevards and/or street furniture.
- ii. Is consistent with the approved Multi-Modal Transportation Strategy
- iii. Ensures safe interactions between all modes along or adjacent to the public right of way and at intersections. Where volumes and conflicts are high, consider grade separations.
- iv. Implements traffic calming strategies consistent with the intended function of the facility. Local roads that could also be used by through traffic would allow for vehicle passage but limit speed consistent with its multi-modal use in order to discourage unwanted through traffic.
- v. Improves facilities for the greater comfort and convenience of pedestrians. Pedestrian facilities will be improved by such means as reducing unnecessary pedestrian barriers, increasing opportunities for crossing busy roads in safety, providing direct routes where practicable, and providing incentives for walking throughout the city and especially within residential neighbourhoods.
- vi. Identifies pedestrian priority areas in neighbourhood centres where pedestrian environments are to be especially encouraged.
- vii. Implements traffic diversion strategies that allow for pedestrian and cycle passage but not vehicles where required.

Policy 8.1.3 Where appropriate encourage and provide for separate corridors for specific transportation purposes such as the E&N Rail Corridor and the Galloping Goose Trail for cycling, walking and potential future light rail transit or commuter rail.

Policy 8.1.4 Implement a strategy that will protect the functional integrity of the transportation corridor by:

- i. Providing for a hierarchical street system where the function of the facility is matched with its form in a multi-modal context
- ii. Limiting or where possible eliminating local access to transportation corridors where the movement of traffic is the primary goal
- iii. Providing all movement access on local facilities where the movement of through traffic is not a goal but access to individual residences, businesses or buildings is a priority
- iv. Reducing conflicts between modes along high volume corridors by giving priority to the strategically dominant users or by constructing infrastructure that will eliminate the conflict

Policy 8.1.5 Assign resources and/or seek senior government funding to modify and/or expand capacity on roads for all modes of transportation.

Policy 8.1.6 Partner with other levels of government and the business community and continue to promote the development of improved rapid transit links with downtown Victoria including:

- i. Commuter rail service between new and existing centres in the community and downtown Victoria using the E & N railway with stations at appropriate locations adjacent to developing and planned high density nodes;
- ii. Ensure new developments are planned to enhance new transit initiatives.
- iii. Ensure new developments make provisions for transit rights-of-way where appropriate.
- iv. Avoid any development that precludes development of and future implementation of planned transit corridors.

Policy 8.1.7 Work with BC Transit to develop and promote high quality and frequent bus service between major development nodes within the combined municipalities of Colwood and Langford by:

- i. Designating bus corridors on high volume routes.
- ii. Providing, over time, as funding and development opportunities arise, properties required for the bus corridors.
- iii. Working with BC Transit to implement transit service in a proactive manner between high density development nodes.
- iv. Working with BC Transit to implement transit priority measures to provide quick and efficient service. Measures may include bus bulges, queue jumpers and bus priority at traffic signals. Bus-only lanes will be provided on selected routes if and when they become warranted by the increase in vehicle traffic.

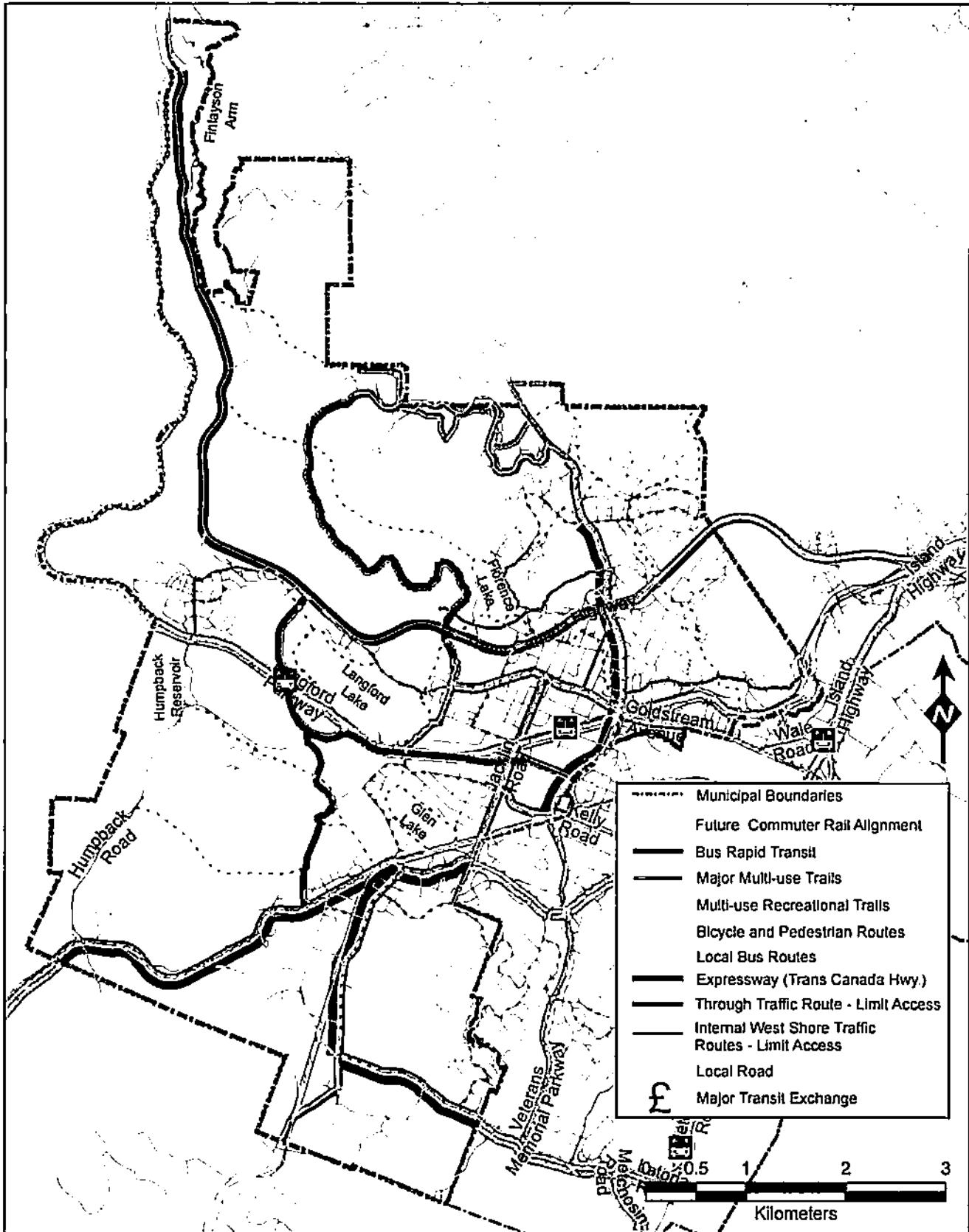


Woonerf Street - Woonerf is a Dutch word that translates to 'Street for living'

Woonerfs are designed to accommodate the needs of automobile drivers but also integrate the needs of other users such as pedestrians, cyclists, and playing children. Woonerfs sustain lower traffic speeds through utilisation of chicanes, or integrated traffic calming and intensive landscaping that forces frequent short turns

- Policy 8.1.8** Work with development proponents to improve connectivity of the multi-modal network through dedication of public right of ways, easements, and development agreements.
- Objective 8.2** Support the use of alternative transportation modes such as walking or cycling.
- Policy 8.2.1** Improve the existing Galloping Goose Trail corridor by supporting paving initiatives, safe street crossings and grade separation at major road network crossings only if needed.
- Policy 8.2.2** Support the implementation of the E & N Rail Trail along the E&N Railway corridor by integrating it into the bicycle and pedestrian network plans.
- Policy 8.2.3** Improve connections between the Galloping Goose Trail and E & N Rail Trail and our community's pedestrian and cycle networks.
- Policy 8.2.4** Incorporate new pedestrian and cycle connections in existing areas, and ensure their connection to new and existing centres.
- Policy 8.2.5** Where appropriate, implement Woonerf streets that allow for vehicle passage but use design techniques to reduce traffic speeds while simultaneously providing for safe access and passage of other users. Ensure street design features allow for temporary street closure as a means to support community activities.

Map 9: Multi-Modal Transportation Strategy



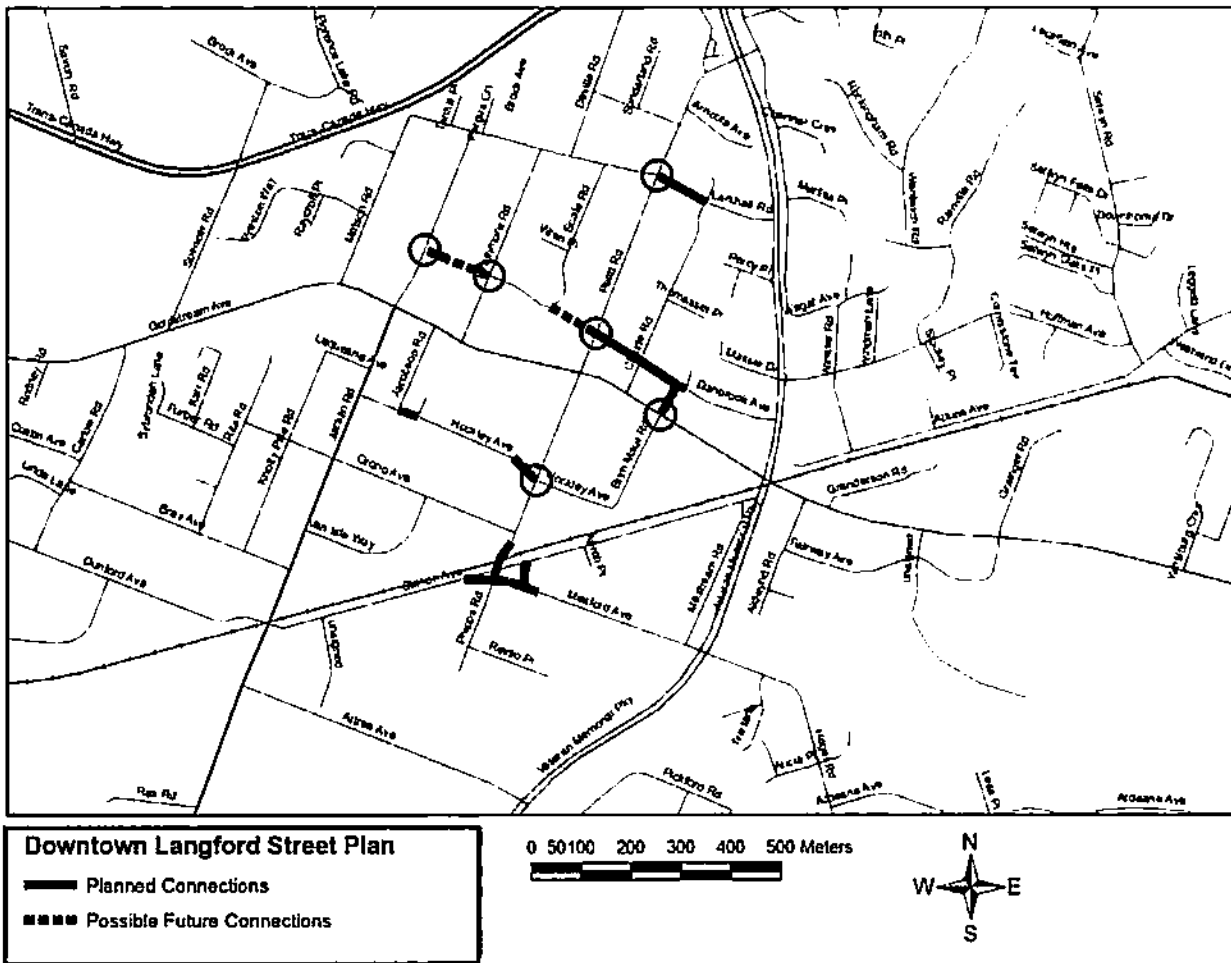
→ Road Network

Objective 8.3 Develop more connected and integrated City Centre road networks.

Policy 8.3.1 Develop a long-term network plan for the City Centre that defines a finely-grained and connected road and block network. A fine-grained road and block network will permit greater densities to be developed and will improve circulation in the centre.

Policy 8.3.2 Seek or procure public right of way dedications at the time of rezoning and/or subdivision in the City Centre that is consistent with the City Centre network plan.

Policy 8.3.3 Seek funding for capital improvement projects that work towards implementing the City Centre network plan from development proponents and/or senior levels of government.

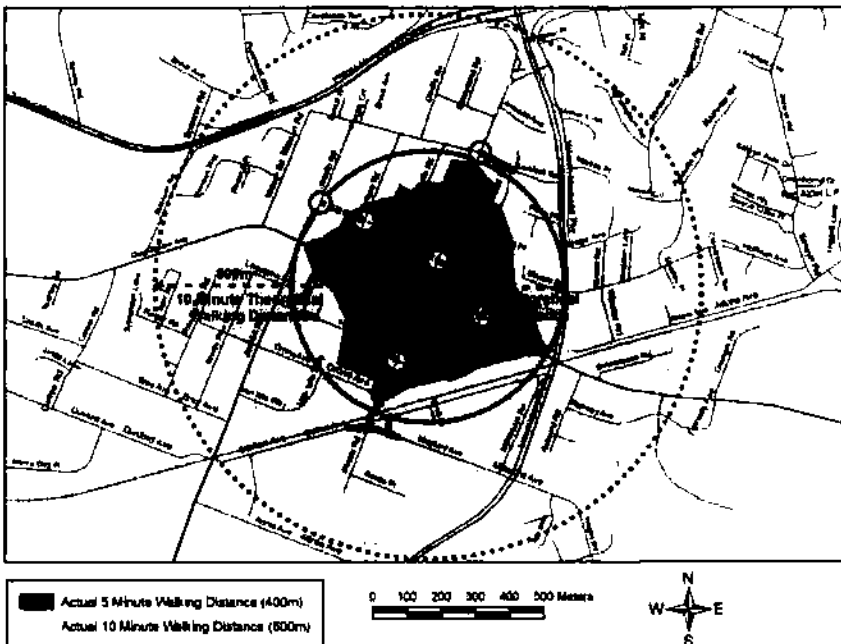
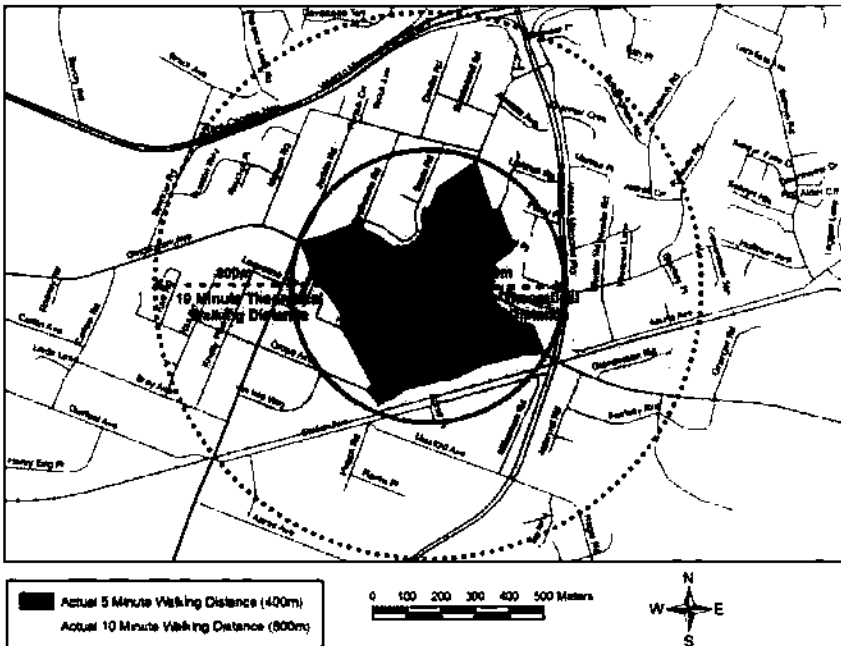


A connected City Centre road network will create a more walkable Downtown.

A healthy person can comfortably walk 400m in about 5 minutes. It has been shown that provided with an attractive pedestrian environment complemented by a range of uses, many people will walk this distance rather than drive. The 10 minute walk, or 800m, is also important to consider since many people will also walk this distance given the right conditions.

The first diagram demonstrates how far one can walk from the corner of Goldstream Ave. & Peatt Rd. in 5 & 10 minutes along existing streets. The theoretical 5 & 10 minute walks are shown as circles.

The second diagram demonstrates how a more integrated and connected road network will contribute to walking in the City Centre by allowing people to get further in the same time



→ *Transportation Demand Management (TDM)*

Objective 8.4 **Develop TDM strategies for the West Shore.**

Policy 8.4.1 **Develop a TDM strategy for Westshore with specific and achievable TDM goals and objectives for new developments and existing developed areas.**

Policy 8.4.2 **Require new development proponents to develop site-specific TDM strategies and ensure implementation of agreed TDM measures.**

Policy 8.4.3 **Obtain funding for, develop and implement TDM measures for existing developed areas.**

Objective 8.5 **Implement parking management programs that promote reduced ownership of cars and/or use of alternative modes of transportation.**

Policy 8.5.1 **Develop parking management strategies that encourage reduced car ownership and/or usage. Parking management strategies are encouraged when the following conditions are in place:**

- i. **Housing within 400m of bus service that is in operation and with a minimum 15 minute service frequency.**
- ii. **Mixed-use development projects and/or in established centres.**
- iii. **Parking spaces for car share vehicles and purchase of share cars (e.g. for every one share car and dedicated parking stall that is provided, 2 parking stalls can be eliminated)**
- iv. **Community transit passes or community shuttles are provided**

- Policy 8.5.2 Provide preferred (e.g. close to entrances) or dedicated parking stalls for share cars and/or hybrid vehicles.
- Policy 8.5.3 Provide free parking for share cars and/or hybrid or highly efficient vehicles on city-owned parking facilities.
- Policy 8.5.4 Review parking standards in zoning bylaw.
- Objective 8.6 Implement programs, services and/or facilities that promote greater transit use.**
- Policy 8.6.1 Ensure transit service is no more than a 5 minute walk (approx. 400m) from homes and ensure that safe and comfortable transit stops with shelters are integrated throughout the community.
- Policy 8.6.2 Work with stakeholders and transit agencies to implement transit pass programs for workplaces, schools (all levels), and/or residential buildings.
- Policy 8.6.3 Expand ride-sharing program and situate Park 'n' Ride parking lots near transit facilities wherever possible.
- Policy 8.6.4 Work with local and regional employers to implement alternative work hours and/or telecommuting strategies.
- Objective 8.7 Educate the public about the benefits of active transportation and transit usage.**
- Policy 8.7.1 Work with partners or seek funding to implement programs that raise the awareness of health and environment benefits associated with active transportation and transit usage.
- Policy 8.7.2 Work with partners to implement a social marketing program that promotes active transportation and the use of transit.
- Objective 8.8 Promote car sharing in the community.**
- Policy 8.8.1 Work with development proponents to include share cars as part of development projects.
- Policy 8.8.2 Work with share car service providers to dedicate vehicles for the West Shore that can be used by existing and new residents.

9.0 Our Sustainable Buildings

Buildings have a profound effect on the environment. In North America, buildings account on average for 39% of total energy use; 12% of total water consumption; 68% of total electricity consumption; and 38% of total carbon dioxide emissions on average. Green building and building energy efficiency strategies are effective ways to minimize social, environmental and economic impacts. Green building is the practice of increasing the efficiency of buildings and their use of energy, water, and materials. Green buildings also reduce building impacts on human health and the environment, through better siting, design, construction, operation, maintenance, and removal.

→ Challenges & Opportunities

- Buildings represented 47% of community GHG emissions in Langford in 2004. Addressing the energy performance of new and existing building stock will be critical for reducing GHG emissions.
- Global demand for resources is leading to escalating costs for building materials such as steel, concrete and wood products.
- Air quality issues related to materials and finishes used in buildings has been recognized as a factor in the poor health of some members of the community.

→ OCP Strategy

We must require improved energy efficiency and/or use of renewable energy in buildings as a minimum due to commitments to climate protection (BC Climate Action Charter).

Green building practices that are healthier for residents and promote efficient use of materials and resources are also a priority for new development projects.

Owners of existing building will be encouraged to retrofit or renovate buildings so as to, as a minimum, increase overall energy efficiency and environmental performance.

→ Existing Building Stock

- Objective 9.1** Improve the energy efficiency and environmental performance of existing buildings through retrofits or redevelopment.
- Policy 9.1.1** Work with senior levels of government to actively promote grant or incentive programs on our website, city advertising, city mail-outs (i.e. tax invoice) and / or other city literature.
- Policy 9.1.2** Develop incentive programs (e.g. tax or permit discounts, density bonusing) possibly structured around other incentive programs (e.g. senior level government incentive or grant programs) for promoting green renovation and/or energy efficiency retrofits of existing buildings for all uses.
- Policy 9.1.3** At the time of rezoning or development permit stage for rehabilitation, addition to or replacement of existing buildings, negotiate energy efficiency and/or renewable energy targets.
- Policy 9.1.4** Work with community-based groups such as industry organisations, business groups, or community groups to better understand their needs and develop strategies to enable green or energy efficiency upgrades or retrofits.
- Policy 9.1.5** Evaluate opportunities for incentive programs that can be offered through local agents and/or businesses for purchasing of energy efficient household appliances.
- Policy 9.1.6** Engage business and industry groups to develop energy baselines, set targets for energy efficiency, develop action plans and/or participate in third party programs (e.g. BOMA Go Green program; a national environmental recognition and certification program for existing commercial buildings). Activities could be supported by senior government literature, reports, programs including grants / incentives programs.
- Policy 9.1.7** Use Local Improvement Charges (LIC) as a means to finance the capital costs of specific improvements to buildings on a cost-recovery basis. An LIC shows up as an additional line item on the property owner's municipal taxes. LIC's associate repayment of the cost of improvements over time with the property rather than with the building owner.

→ New Buildings

- Objective 9.2** Encourage energy efficiency and green building development for new buildings.
- Policy 9.2.1** Amend infrastructure policies and standards to enable the development of green and energy efficient buildings and use of renewable energy; tools to be considered include (but not limited to):
- i. Varied development cost charges
 - ii. Density bonusing
 - iii. Service Area Bylaw(s)
- Policy 9.2.2** Use tools to encourage development of green, energy efficient buildings and use of renewable energy such as:
- i. Mandatory completion of a Sustainability Checklist at rezoning and/or development permit application
- Policy 9.2.3** Establish comprehensive development (CD) zones as a means to tailor development requirements that will deliver on green building / energy efficiency objectives.
- Policy 9.2.4** Obtain green building commitments and increased building energy performance requirements at the time of rezoning in the form of a covenant or development agreement.
- Policy 9.2.5** Amend the City of Langford Zoning Bylaw to describe the conditions that, if met, will entitle the developer to additional density as a public environmental / health benefit in order to promote green building and increased building energy performance requirements.
- Policy 9.2.6** Set policy for discounted Building Permit or other municipal fees for selected buildings for implementation of green building measures.
- Policy 9.2.7** Develop and implement a performance-based approvals process (e.g. expedited development/building permit application processing for developer / builder commitment to third party building labelling programs such as *LEED™* or *BuiltGreen™*) if green and/or energy efficient measures are provided for private developments.
- Policy 9.2.8** Develop and implement a revitalization tax exemption bylaw for green and/or energy efficient buildings.
- Policy 9.2.9** Use Local Improvement Charges (LIC) as a means to finance the capital costs of any additional cost of building

to the highest levels of efficiency (e.g., LEED Gold or R2000) on a cost-recovery basis.

- Policy 9.2.10** Promote the development and implementation of alternative financing strategies and mechanisms to address financial barriers associated with additional costs for green buildings, energy efficiency and/or use of renewable energy. Options include, but are not limited to:
- i. Municipal financing incremental costs of green building and/or energy efficient measures on a cost recovery basis.
 - ii. Fostering the development of energy efficient mortgages with local mortgage lenders
 - iii. Establish a revolving loan fund to provide grants and loans for undertaking special projects to advance significant emission reduction results or green buildings.
 - iv. Foster the development of strata energy mortgages to finance high performance, energy efficient equipment and materials.
- Policy 9.2.11** Support development industry capacity building and education to build knowledge and confidence in energy efficient and/or green building practices.
- Policy 9.2.12** Develop and implement demonstration projects through partnerships and/or incentive / grant programs.
- Policy 9.2.13** Use city-owned land as a means to promote the development of showcase green and/or energy efficient buildings through a land disposition process that incorporates sustainability objectives.
- Objective 9.3** Develop sustainability guidelines and/or user resources.
- Policy 9.3.1** Provide assistance on marketing of green buildings and neighbourhoods as part of the overall sustainability marketing / brand of the community.
- Policy 9.3.2** Require a 'green operating manual' for homes and/or strata corporations to increase capacity of homeowners to choose and manage green technologies / approaches.

- Objective 9.4** Lobby the Federal and Provincial Governments for funding or regulatory changes
- Policy 9.4.1** Lobby the federal government to commit to long term funding for building energy efficiency activities for new and existing buildings directly or through other agencies such as the Union of BC Municipalities (UBCM) and/or the CRD.
- Policy 9.4.2** Lobby the provincial government to continue to expand the range of energy efficiency activities or equipment eligible for a PST exemption.

10.0 Our Local Economy & Jobs

This OCP elevates the importance and need for supporting expanded local economic development as a critical component for creating a sustainable community. Creating jobs locally is part of the solution to current and future transportation challenges in the West Shore in that it will reduce commuting trips. Creating local jobs is therefore a key strategy for reducing common air contaminants and greenhouse gas emissions that result from transportation activities. Recent economic development trends and positive market conditions for additional commercial and industrial development are creating the supportive conditions for achieving municipal economic development objectives.

→ Challenges & Opportunities

- Ongoing maintenance and further development of the city's jobs-labour force ratio.
- Quality of life in a community attracts new businesses.
- Strong demand exists for commercial, industrial, and office land with diminishing land resources and designated areas for these uses in the region.
- Increased property tax revenues and more local jobs are much needed to make progress towards sustainability.
- The City is maturing and has the opportunity to be seen as a more complete community with important regional employment centres.

→ OCP Strategy

Encourage existing businesses to stay and grow in and welcome new businesses into the community

Key strategies include:

- Increase the ratio of jobs for every member of the resident labour force to 0.7 - 0.9.
- Ensure a short and long term supply of employment lands.
- Promote the community as a business-friendly environment.

→ Economic Development

Objective 10.1 Retain and encourage the growth of existing businesses and attract targeted new businesses to locate in the planning area.

Policy 10.1.1 Work with local business associations to support expansion of local and regional serving businesses located in the community.

Policy 10.1.2 Periodically review the business enabling environment to ensure the city remains a regionally (and provincially) competitive, attractive business destination.

Policy 10.1.3 Permit home-based business in all parts of the community in all dwelling units.

Policy 10.1.4 Explore or encourage the development of a home-based business support centre to support or promote home-based businesses.

Policy 10.1.5 Develop a West Shore Economic Development Strategy with local business associations and other communities in the West Shore.

Objective 10.2 Promote the City Centre as a regional growth area for knowledge-, education- and creative-based industries.

Policy 10.2.1 Ensure sufficient and appropriate floor space capacity to meet local and regional demands for the planning period.

Policy 10.2.2 Direct new office development to the city centre where practical and feasible.

Policy 10.2.3 Expand existing live-work zoning in areas in and around centres.

Policy 10.2.4 Identify sites for office development and work with development proponents to attract 'anchor' office/business tenants through proactive development partnerships.

Policy 10.2.5 Support those businesses whose needs cannot be met within the city centre in other locations in the city.

Objective 10.3 Improve the ratio between the resident labour force and jobs.

Policy 10.3.1 Establish a progressive jobs-labour force target and reevaluate floor area capacities based on this target to

CITY OF LANGFORD
BYLAW NO. 1201

A BYLAW TO AMEND BYLAW NO. 300, LANGFORD ZONING BYLAW, 1999

The Council for the City of Langford, in open meeting assembled, enacts as follows:

- A. Langford Zoning Bylaw No. 300, 1999 is amended by adding as Appendices "A" to "R" the Appendices attached to this Bylaw, and by adding the following as Section 2.14:

2.14 Development Permit Guidelines

2.14.01 Appendices "A" to "R" are adopted pursuant to Section 919.1(3) of the *Local Government Act* as guidelines pertaining to the development permit areas designated in the Langford Official Community Plan.

- B. This Bylaw may be cited for all purposes as "Langford Zoning Bylaw, Amendment No. 232, (Design Guidelines), 2008".

READ A FIRST TIME this day of, 2008.

PUBLIC HEARING HELD this day of, 2008.

READ A SECOND TIME this day of, 2008.

READ A THIRD TIME this day of, 2008.

APPROVED BY THE MINISTRY OF TRANSPORTATION this day of , 2008.

ADOPTED this day of, 2008.

MAYOR

CERTIFIED CORRECT
(Clerk-Administrator)



City of Langford

Bylaw No. 1201

***Appendices No. "A" to "R"
to Zoning Bylaw No. 300***

***Design Guidelines
and
Development Permit Areas***

April 21, 2008

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List of Maps

<i>Map</i>	<i>Description</i>	<i>Follows</i>	<i>BW or C</i>	<i>C or I</i>
4A	200-Year Floodplain	Page A11	BW	I
4B	Steep Slopes	Map 4A	BW	I
4C	Drainage Concern	Map 4B	BW	I
5A	Riparian Zones	Map 4C	BW	I
5B	Sensitive Ecosystems	Map 5A	BW	I
5C	Areas With Potential Habitat and Biodiversity Values	Map 5B	BW	I
3A	Development Permit Area Design Guidelines for Auto-Oriented Business Area (CanWest)	Page B7	BW	C
D1	Olympic View	Page D6	C	I
E1	Goldstream Meadows – Comprehensive Development Plan	Page E5	BW	C
E2	Goldstream Meadows – Land Use Plan	Map E6	C	C
K1	Hull's Field	Page K4	BW	C
7	Land Use Concept Plan (Bear Mountain Resort)	Page M18	C	I
N1	Interface Fire Hazard Plan	Page N4	C	I
10	Valley View Land Use Neighbourhood Plan	Page Q2	C	C
	South Langford Neighbourhood Plan	Page R16	C	C

Appendix A: Development Permit Area Guidelines

Applicants are required to provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason shall be made. Council may diverge from these guidelines where a compelling rationale which preserves the qualitative intent of the guidelines is supplied. If, during a consultative process, Council receives a compelling reason for modifying a guideline, the applicant shall be requested to consider amending the proposal.

Development Permit and Development Approval Information Areas – Designations, Justifications, and Objectives

All areas identified on Maps 4a, 4b, and 4c as being floodplain, steep slopes, or areas of drainage concern are hereby designated as Development Permit Areas for the purposes of protecting, development from hazardous conditions. The justification and objectives of this designation are to:

- a) ensure that people and property are safe from floodwaters;
- b) ensure that development in steep slope areas is designed and engineered to provide a high level of protection from ground instability and/or slope failure;
- c) ensure that drainage is managed in a way that does not negatively impact the community.

All areas identified on Maps 5a, 5b, and 5c as being environmentally sensitive are hereby designated as Development Permit Areas and as Development Approval Information Areas for the purpose of protecting the natural environment, its ecosystems and biological diversity from development. In relation to any areas shown on Map 5a comprising or connected to a ravine, the Riparian Development Permit Area includes, despite the appearance of the area on Map 5a, an area defined by the following criteria, and for that purpose a "ravine" is a narrow valley with an average grade on either side greater than 3:1 measured between the natural boundary of the water body contained in the valley and the top of the valley bank, and the top of the valley bank is the point nearest the water body beyond which the average grade is less than 3:1 over a horizontal distance of at least 15 metres measured perpendicularly to the water body:

- a) For a ravine less than 60 metres wide, an area on both sides of the water body measured from the natural boundary of the water body to a point that is 30 metres beyond the top of the ravine bank; and
- b) For a ravine 60 metres wide or greater, an area on both sides of the water body measured from the natural boundary of the water body to a point that is 10 metres beyond the top of the ravine bank.

The justification for this designation is to ensure that Council has the ability to secure the necessary information and is able to establish conditions on developments such that rare, endangered, and sensitive ecosystems and wildlife are protected and development impacts mitigated.

The objectives of this designation are to:

- a) preserve and enhance the majority of riparian values and prevent slope instability and erosion in the areas adjacent to all water bodies;
- b) preserve and enhance the sensitive ecosystems whenever possible;
- c) preserve and enhance general habitat, biodiversity and natural environment regimes in undeveloped (green field) sites in excess of five acres in size in areas designated for development (New Neighbourhoods and Existing Residential).
- d) Protect fish habitat in accordance with the Provincial Ministry of Environment's Riparian Areas Regulation enacted under the *Fish Protection Act*.

All areas on Maps 4a, 4b, 4c, 5a, 5b, and 5c and those areas identified as Town Centre, Commercial, New Neighbourhoods, Residential, and Business Park/Service Commercial on Maps 2 and 3 are designated as Development Approval Information Areas.

The justification and objective for this designation is to ensure that council has the ability to secure necessary information for considering development applications in order that traffic and Storm Water resulting from development is well-managed, and rare, endangered, or sensitive ecosystems, plants, and animals are protected.

Development Permit Area Guidelines

1. Within the designated Development Permit Areas, Council may, by resolution, issue development permits.
2. Every application for a development permit shall be accompanied by plans indicating the following:
 - a) location of all buildings and structures;
 - b) siting of parking areas, driveways, and loading docks;
 - c) the extent and nature of landscaping, including details of trees to be maintained or proposed to be planted;
 - d) the exterior finish, materials, and colour of buildings and roofs; and
 - e) location, number, and type of signage
3. Notwithstanding the Development Permit Area designation, a development permit is **not** required under the following conditions:
 - a) For areas designated within any Multi-family Housing, Commercial, Industrial, and Business Park/Service Commercial classification and in the Downtown Langford Development Permit Area and the Bear Mountain Estates Development Permit Area, in the case of:
 - i) minor alterations to the exterior of a building or structure that do not change the form or character of the development; and

- ii) for minor changes to design, finish, or landscaping.
 - iii) Accessory, other minor buildings, and additions which are consistent with the architectural theme (form, character, materials and colour scheme) of the proposed or existing building provided that the additional structures comprise less than 10% of the total square footage of buildings (as approved in a Development Permit) on the site and provided that they are not in an environmentally sensitive area or floodplain.
 - iv) Temporary buildings or structures that are erected either for offices for construction or marketing purposes for a period that is not expected to exceed the duration of such construction, or one year, whichever is less.
 - v) Propane tanks may be placed on properties with appropriate zoning provided that they are not located in required front yards and are screened with landscaping (designed and installed by landscaping company) or fencing from adjacent residential properties).
 - vi) For the purpose of this Section, "minor" is defined as a change which does not:
 - alter the siting more than 5% of the approved siting (based on the site coverage of all buildings);
 - change the colour scheme or materials of building exteriors;
 - increase site coverage more than 5% of the approved coverage;
 - increase any bylaw non-conformities;
 - Constitute more than \$10 000 in value to construct; or,
 - Comprise more than 2,000 square feet of gross building area.
 - vii) The erection of temporary tent structures, provided that the structure(s):
 - Are not erected, or allowed to be standing on site before April 15th or after September 15th of any calendar year;
 - That a building permit has been issued with respect to the tent structure;
 - That the tent structure is accessory to a commercial, business park, or industrial use occurring on the same property;
 - That the applicant has deposited with the City of Langford a bond, to the satisfaction of the City Planner, sufficient to allow for the removal of the structure;
 - That the structure does not remain in use for a period of more than 14 days.
- b) For areas designated within the Environmentally-Sensitive Area and Hazardous Area classification, with the exception of Riparian areas, a development permit is **not** required under the following conditions:
- i) for subdivisions which are lot line adjustments, subdivisions for park purposes, or for consolidation;
 - ii) for single-family dwellings or two-family dwellings on lots created by a plan of subdivision for which a development permit was previously issued by Council;
 - iii) for accessory buildings not exceeding an area of 10 m² (107 ft²);

- iv) Where a building is legally non-conforming in the designated floodplain, it may have a one time only expansion/addition at the original floor elevation up to, but not exceeding, 25% of the non-conforming habitable area on the main floor of the building existing at the time of enactment of flood proofing requirements provided that:
 - there is no increase in non-conformity in respect to setback requirements;
 - no fill is brought onto the site the addition satisfies the floodplain elevation; and
 - the foundation allows flood waters to pass through.
- v) Where a building is legally non-conforming in the designated floodplain, it may have expansion/addition in excess of 25% provided that:
 - the existing building and the addition are raised and placed on a new foundation which satisfies the required floodplain elevation and allows flood waters to pass through;
 - any expansion or addition complies with all other zoning and building bylaw regulations;
 - there is no increase in non-conformity in respect to setback requirements; and
 - no fill is brought onto the site.
- vi) Habitat improvement activities undertaken by the City of Langford or by Provincial or Federal agencies.
- vii) Trail projects approved by the City of Langford.
- viii) The trimming or removal of vegetation/trees 3 m either side of driveways or fire accesses.
- ix) The removal of dead or hazardous trees in accordance with a recommendation from a certified arborist.
- x) For minor additions or renovations to buildings or structures, provided that:
 - a) There is no increase in any bylaw non-conformities;
 - b) All zoning requirements are satisfied;
 - c) There is no further encroachment in the designated development permit area in a horizontal plane; and
 - d) Site coverage is not increased by more than 5% of the existing coverage.
- xi) The removal of vegetation in orchards and domestic or commercial gardens.
- xii) Existing land uses and buildings, provided that the area occupied by the land use is not increased or the area occupied by the building is not expanded.
- xiii) For areas designated as "Terrestrial Herbaceous," "Older Forest," or "Woodland" development permit areas on *Map 14: Sensitive Ecosystems* of OCP Bylaw No. 1200, no development permit is required if a registered professional biologist determines

through a detailed field assessment that no sensitive or rare plants and ecosystems, or significant habitat features are located on the property in question.

- c) For areas designated within the Riparian Development Permit Area, a development permit is not required under the following conditions:
 - i) For the removal of trees in the Riparian Development Permit Area, up to 30 m (100 ft) of the natural boundary of the adjacent water body if;
 - a) The land being cleared is separated, from the water body from which the Riparian Area is measured, by land that is of a higher elevation than the land to be cleared and the water body; and
 - b) The water body from which the Riparian Area is measured contains standing or running water for less than six months per year.
 - ii) For all lands taxed as agricultural that are within the Agricultural Land Reserve.
 - iii) The removal of dead or hazardous trees in accordance with a recommendation from a certified arborist and an impact report prepared by a qualified environmental professional.
 - iv) Notwithstanding the development permit area boundaries identified on Map 5A, a development permit is not required if the property owner submits a letter from a qualified environmental professional that certifies that the entire development site is outside of the riparian development permit area, which is measured as 43 m (141 ft) from the natural boundary of any watercourse, to the satisfaction of the City Planner.

4. Within the designations specified below the following development permit area guidelines apply:

Town Centre Pedestrian Commercial

- TC1 The emphasis of buildings shall be on street frontage, with buildings incorporating a substantial amount of fenestration.
- TC2 Where existing commercial centres are expanded, the building orientation shall be towards the street, incorporating small retail and office units, and shop windows at grade level to encourage pedestrian activity.
- TC3 Roof mounted signs are prohibited. The location and size of signs shall be architecturally integrated with the overall design of the buildings and any theme that may be established for the area.
- TC4 The use of decorative lighting on buildings within the designated areas is encouraged.
- TC5 The use of canopies, awnings, or similar cover shall, where feasible, be incorporated to provide pedestrian protection. Pedestrian passage along the face of buildings, under a continuous system of awnings or canopies is preferred.
- TC6 All landscape areas fronting a street shall incorporate ornamental trees, where practical.

- TC7 Surface parking areas shall be primarily located to the rear of buildings, and be screened from view by good quality landscaping. Fences are not desirable and will be approved only for limited storage areas. Chainlink and/or perimeter fencing will not be permitted.
- TC8 Where street furniture forms a component of a project, consistent treatment shall be required, in order to maintain a community identity.
- TC9 All parking areas shall be provided and maintained as approved by the City. Where possible, parking coordination with adjacent lots shall be provided, with reciprocal access agreements to ensure street efficiency.
- TC10 Any cut and fill areas within new sites are to be treated in an appropriate manner so that they do not cause erosion and drainage problems.
- TC11 All access cross-overs and access roads are to be designed so that the locations, dimensions, and grades are functional and safe.
- TC12 All parking areas are to be paved, drained, and appropriately screened.
- TC13 The City will require security to ensure the installation and maintenance of landscaping in compliance with Council's adopted Landscape Policy.
- TC14 All exterior on-site utilities, including but not limited to: drainage systems; sewers; gas lines; water lines; and electrical, telephone and communication wires and equipment; shall be installed and maintained underground.
- TC15 Land owners should provide electrical outlets at appropriate locations to provide electricity for Christmas lights on site and along frontages spaced at approximately every 20 m.
- TC16 In order to provide for efficient and safe vehicular access to commercial properties and between commercial properties, the design of parking areas and vehicle maneuvering aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

Town Centre General Commercial

- TG1 Large retail complexes and free standing major stores shall have substantial landscaping along all perimeter areas, including planting of trees in clusters at several locations within the perimeter.
- TG2 Parking areas must include several substantive landscape islands to break the expanse of parking areas.
- TG3 Buildings facing major access roads must incorporate adequate fenestration and decorative elements to minimize the building bulk.
- TG4 Roof signs are prohibited.
- TG5 Pedestrian movement between shops should be direct, sheltered, and in a pleasant setting.

- TG6 All access cross-overs and access roads are to be designed so that the locations, dimensions, and grades are functional and safe.
- TG7 All parking areas are to be paved, drained, and appropriately screened.
- TG8 The City will require security to ensure the installation and maintenance of landscaping in compliance with Council's adopted Landscape Policy.
- TG9 Fences are not desirable and will be approved only for limited storage areas. Chainlink and/or perimeter fencing will not be permitted.
- TG10 All exterior on-site utilities, including, but not limited to: drainage systems; sewers; gas lines; water lines; and electrical, telephone and communication wires and equipment; shall be installed and maintained underground.
- TG11 Land owners should provide electrical outlets at appropriate locations to provide electricity for Christmas lights on site and along frontages spaced at approximately every 20 m.
- TG12 In order to provide for efficient and safe vehicular access to commercial properties and between commercial properties, the design of parking areas and vehicle maneuvering aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.
- Specific Design Guidelines for the Auto-Oriented CanWest development permit area are in Appendix B.

Town Centre Residential - Multi-Family Low-Density, Multi-Family Medium-Density, and Cluster Housing

- TL1 Where new developments are introduced in areas traditionally comprised of single-family housing, the new project shall provide harmony and continuity with the existing neighbourhood.
- TL2 The design of buildings shall reflect a comprehensive design, giving thorough attention to the massing of buildings, building scale, character, architectural style, entrances, materials, and roof design.
- TL3 Where low rise residential buildings face a major road, careful attention shall be given to the street linkage, through appropriate treatment of building exteriors, pedestrian entrances, and walkways.
- TL4 The roof treatment of low buildings should be developed with substantial attention to the streetscape, to create a pleasing overall environment.
- TL5 Where a multiple-family residential development is to be constructed adjacent to a property with a single-family building or an area not designated for multiple-family housing, the new multiple-family housing design shall provide sufficient buffering in terms of screening, fencing, berming, and landscaping to reduce any potential land use conflicts.
- TL6 Substantial landscaping shall be provided and maintained to screen parking lots. Retention of mature trees within the overall landscape treatment is encouraged. The City will require

security to ensure the installation and maintenance of landscaping in compliance with Council's adopted Landscape Policy.

- TL7 Where a higher-density multiple-family residential development is to be constructed adjacent to a property with a non-residential or lower density residential use, the new residential design shall provide sufficient buffering in terms of screening, fencing, berming, and landscaping to reduce potential land use conflicts.
- TL8 To the extent practical, the massing of buildings shall be oriented on a site in such a manner that pre-development view corridors are not impeded.
- TL9 Land owners should provide electrical outlets at appropriate locations to provide electricity for Christmas lights on site and along road frontages spaced at approximately every 20 m.

Business Park/Service Commercial/Industrial

- SC1 Where practical, and approved by the City, frontage roads are to be used for ingress and egress. Where a frontage road parallels a major arterial, the frontage road shall be separated from the arterial by either a strip of land sufficient to permit building development, or a landscaped boulevard not less than 5 metres (16 ft) in width.
- SC2 Where an abutting property is not zoned industrial or commercial, a minimum landscaped setback of 5 metres (16 ft) shall be provided between the principal or accessory building and the parcel not zoned industrial or commercial. This landscaped strip shall include trees and planting materials which provide a substantial buffer between uses.
- SC3 No outdoor storage or parking shall be permitted within 5 metres (16 ft) of a street or an abutting parcel not zoned industrial or commercial.
- SC4 Roof signs are prohibited.
- SC5 Where physically practical, the majority of parking and loading areas shall be situated at the rear or side of buildings.
- SC6 All portions of a site not covered by buildings, structures, and circulation space shall either be retained in a natural condition, or landscaped. The City will require security to ensure the installation and maintenance of landscaping in compliance with Council's adopted Landscape Policy.
- SC7 In order to provide for efficient and safe vehicular access to commercial properties and between commercial properties, the design of parking areas and vehicle maneuvering aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

Specific development permit area guidelines may be established for each principal geographic Business Park/Service Commercial area.

ENVIRONMENTALLY SENSITIVE AND HAZARDOUS AREAS

General

G1 Environmental Impact Studies may be required in accordance with the designation made in the Official Community Plan.

An Environmental Impact Study means a professionally prepared report that identifies wildlife, plants, and plant communities, wildlife corridors, aquatic animals and high value habitat and recommends:

- a) development patterns and servicing to minimize impact on rare, endangered or sensitive wildlife and plants;
- b) mitigation and enhancement strategies; and
- c) storm water management plans that maintains predevelopment water quality and quantity.

G2 Development within the areas identified on Maps 5a, 5b, and 5c should be in accordance with the mitigation and enhancement strategies recommended in the Environmental Impact Studies provided to the City of Langford.

Riparian

R1 Within environmentally sensitive areas, and where not inconsistent with the environmental quality of the area, an emphasis shall be placed on providing public open space and public access to these lands and watercourses.

R2 To ensure development does not negatively impact wetlands and other sensitive plant communities, a buffer area may be required to set back buildings and uses.

R3 R4 Public trails may be developed in riparian corridors provided that porous, non-polluting trail/tread surfaces are used.

R5 Council may require environmental enhancement and restoration for applications involving riparian areas.

R6 Council may require the dedication of water bodies as a condition of development permit approval.

R7 Development shall incorporate measures to contain sediment and control erosion during and after construction.

R8 Existing trees and vegetation may not be disturbed except as allowed in a development permit approved by the City of Langford. Replacement trees and shrubs should be native to the region.

R9 All development proposed within a Riparian Development Permit Area is subject to the Development Permit process and no vegetation removal, land alteration or construction of buildings or structures, unless specifically exempted, may occur unless authorized by Development Permit.

- R10 Development may occur inside the Riparian Development Permit Area subject to compliance with these guidelines. Any development occurring within the Riparian Development Permit Area is encouraged to exceed the minimum standards of the Riparian Areas Regulation.
- R11 For any development within a Riparian Development Permit Area, the applicant, at their own expense, shall retain a qualified environmental professional to prepare an assessment report pursuant to Section 4 of the Riparian Areas Regulation. The qualified environmental professional must certify that he or she is qualified to carry out the assessment, and that the assessment methods in the Schedule to the Riparian Areas Regulation were complied with, and further provide their professional opinion that:
- a) the development will not result in any harmful alteration, disruption or destruction of any natural features or functions that support fish life processes within the Riparian Development Permit Area; or
 - b) the streamside protection and enhancement area identified in the assessment report will be protected from the effects of the development by measures identified in the report, with the result that the development will not result in any harmful alteration, disruption or destruction of any natural features or functions that support fish life processes within the Riparian Development Permit Area.
- R12 The development permit shall not authorize any development activities within the streamside protection and enhancement area designated in the assessment report.
- R13 Notwithstanding Guideline R12, the City Planner may issue a Development Permit authorizing development within the streamside protection and enhancement area (SPEA) if the Minister of Fisheries and Oceans issues a Section 35 permit that authorizes the work, and may approve the identification and protection of a SPEA that does not comply in every respect with the assessment methods under the Riparian Areas Regulation, provided that:
- a) The land uses and densities permitted on the parcel in question by Zoning Bylaw No. 300 cannot otherwise be developed;
 - b) The overall area of the SPEA is not reduced from the area it would have if identified and protected strictly in accordance with the assessment methods, and the SPEA forms a single contiguous area;
 - c) No portion of the SPEA boundary is less than 5 m (16.4 ft) from the natural boundary of the water body;
 - d) The qualified environmental professional has expressly considered the location, quality and function of existing riparian vegetation when determining the extent and location of the adjustment to the SPEA boundary, and the adjustment of the SPEA boundary does not result in the loss of the only large trees in the portion of the parcel that lies within the Riparian Development Permit Area; and
 - e) Geotechnical stability is not likely to be comprised as a result of any development authorized by the Development Permit, and for this purpose a geotechnical assessment may be required at the applicant's expense.
- R14 Any measures recommended or required by the assessment report to preserve, protect, restore or enhance the SPEA shall be included in the Development Permit as conditions.

R15 The City Planner may approve variances for the setback of a building from a watercourse otherwise required by Zoning Bylaw No. 300, if the variance is recommended in an assessment report.

R16 The use of impervious surfaces will be permitted in accordance with the City of Langford Stormwater Management Guidelines.

Sensitive Ecosystems

SE1 The preservation of wild flower communities and wooded areas is encouraged.

SE2 Where an environmentally sensitive area comprises part of a parcel being proposed for development, such environmentally sensitive lands may not necessarily be included for the purposes of calculating the number of permitted dwelling units. Clustering of density is encouraged as a means for preserving environmentally sensitive areas.

SE3 Unnecessary disturbances to the Sensitive Ecosystem shall be avoided.

SE4 Drainage should not be altered in a way that increases or decreases the amount of surface or groundwater or the quality of water available to the sensitive ecosystem.

SE5 Planting of non-native plants common to the sensitive ecosystem is discouraged.

Undeveloped Green Field Sites

GF1 Pollutants shall be prevented from entering lakes and watercourses through control of surrounding drainage.

GF2 Before issuing a Development Permit for any area identified on Map 5c, the City may require the applicant to provide, at his/her expense, a report, certified by a qualified consultant, to assist in identifying:

- a) sensitive ecosystems;
- b) rare or endangered plants and animals;
- c) wildlife corridors; and
- d) significant biodiversity values.

The report should also provide recommendations for protecting and enhancing significant environmental values on the site. The recommendations and information in the report may be used to set conditions in the Development Permit.

Steep Slopes

SS1 When land is altered, due regard shall be given to maintaining the normal drainage system, and regulating storm water run-off. Exposed soil on steep slopes subject to erosion shall be re-vegetated or otherwise protected from run-off erosion.

SS2 Development will not be permitted on hillsides of 30% or greater slope with poor soil stability or susceptibility to erosion unless engineered to resolve the hazard.

Floodplain

- FP1 No significant excavation or filling shall be undertaken nor any building or structure shall be constructed unless permitted by a development permit. The development permit should insure that the work:
- a) does not negatively impact the floodplain; and
 - b) is flood-proofed in accordance with City of Langford standards.
- FP2 Minimum habitable elevation levels will be the 1 in 200 year flood levels as defined by the Langford Storm Water Master Plan (prepared by Reid Crowther and Associates) plus .3 m (1 ft) for surge and .3 m (1 ft) freeboard along the main stem of creeks and tributaries and lakes/wetlands.

Drainage Concern Areas

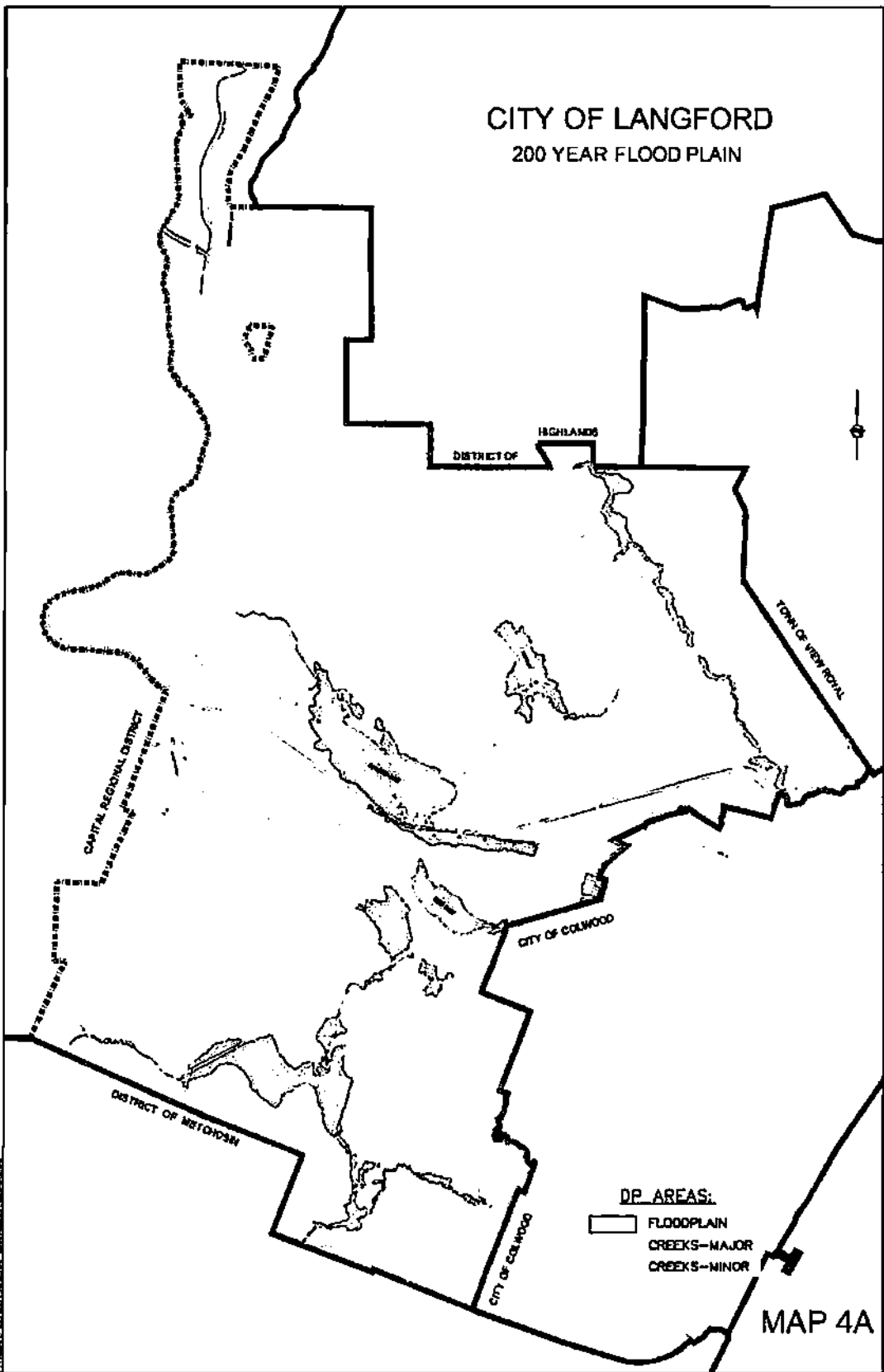
- DC1 No significant excavation or filling shall be undertaken nor any building or structure shall be constructed unless permitted by a development permit. The development permit should insure that the work:
- a) does not negatively impact the area of drainage concern; and
 - b) is flood-proofed in accordance with City of Langford's standards.




Development Permits

Council has the authority to issue a Development Permit which may be used to vary bylaw provisions other than the use or density of land or floodplain specifications.

Council may consider requests for relief of bylaw specifications (e.g., height, siting of buildings and parking requirements) where such change is appropriate from an environmental perspective or represents a community benefit.

CITY OF LANGFORD
200 YEAR FLOOD PLAIN

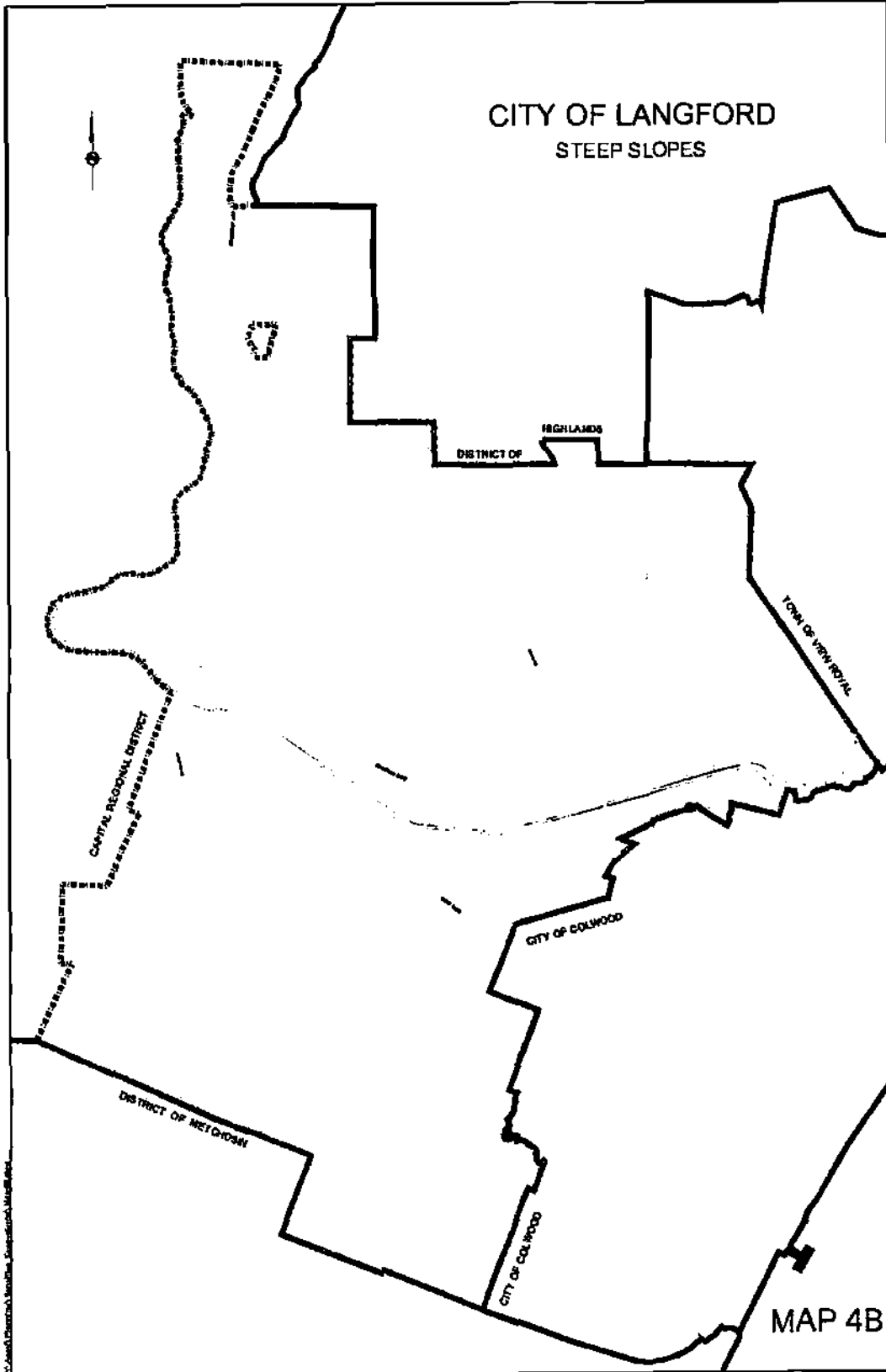


- DE AREAS:
-  FLOODPLAIN
 -  CREEKS-MAJOR
 -  CREEKS-MINOR

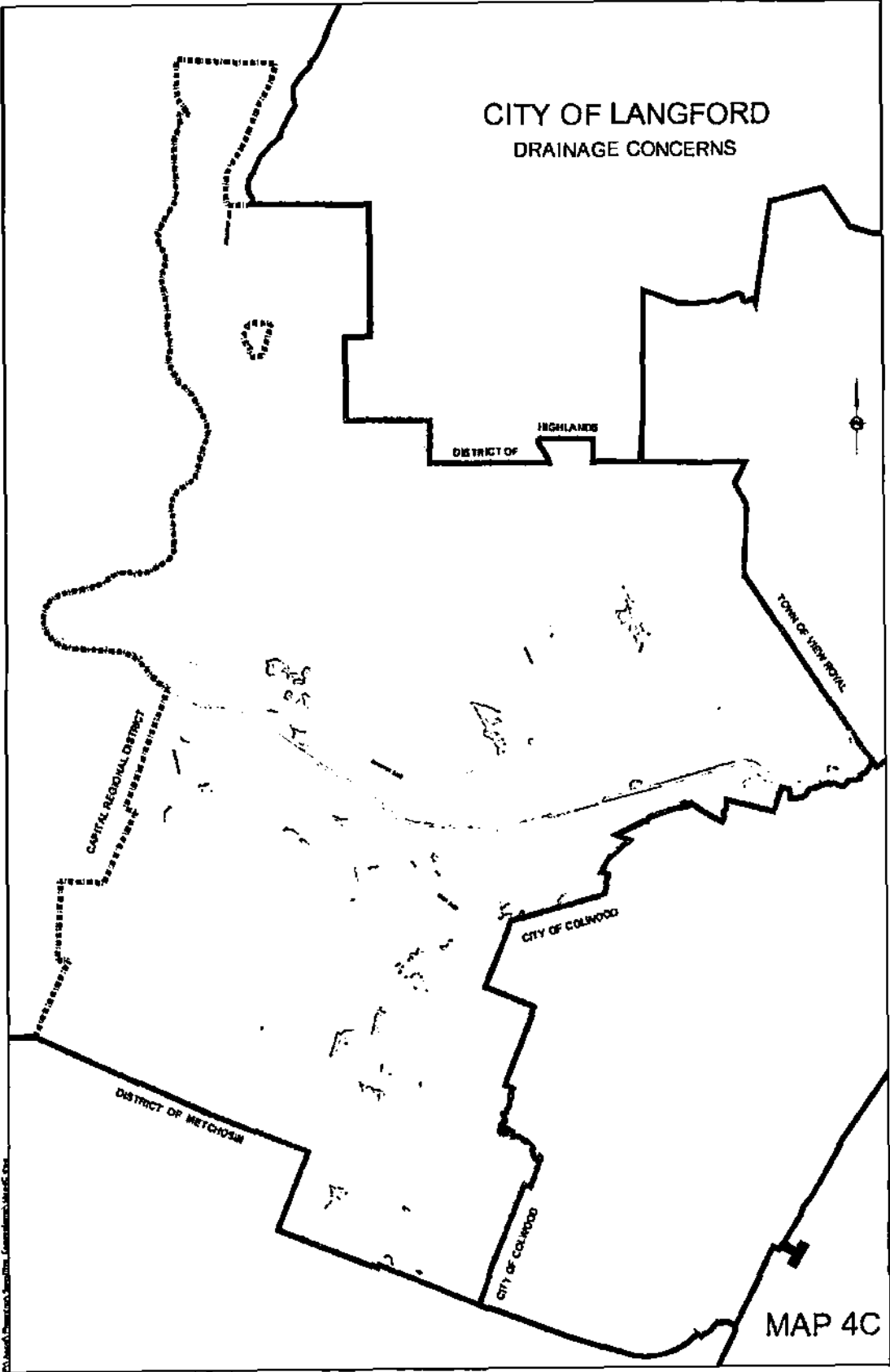
MAP 4A

Lang Release Sept. 27, 2008

CITY OF LANGFORD STEEP SLOPES



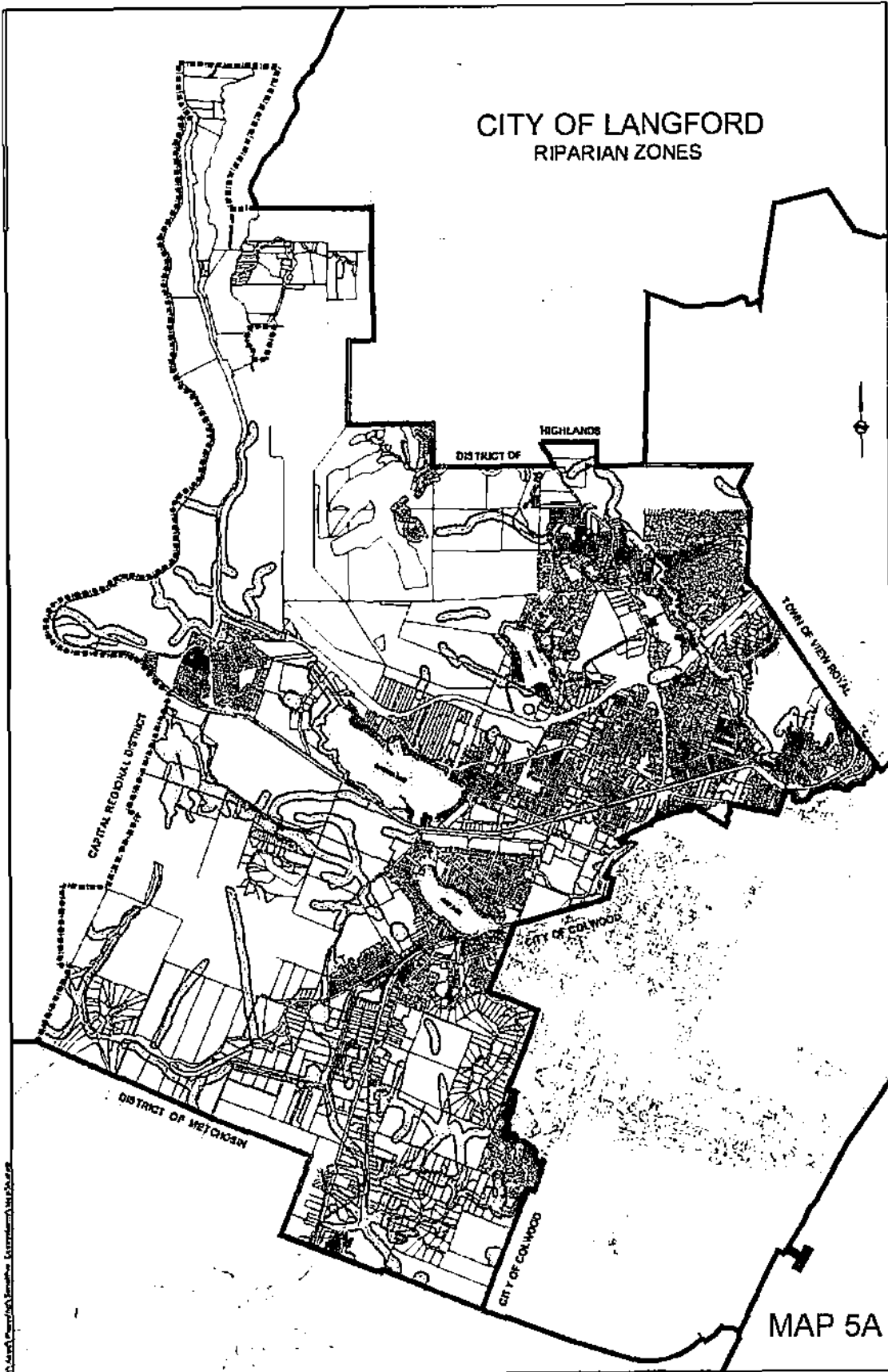
CITY OF LANGFORD DRAINAGE CONCERNS



MAP 4C

Let's Review Sep. 07, 2006

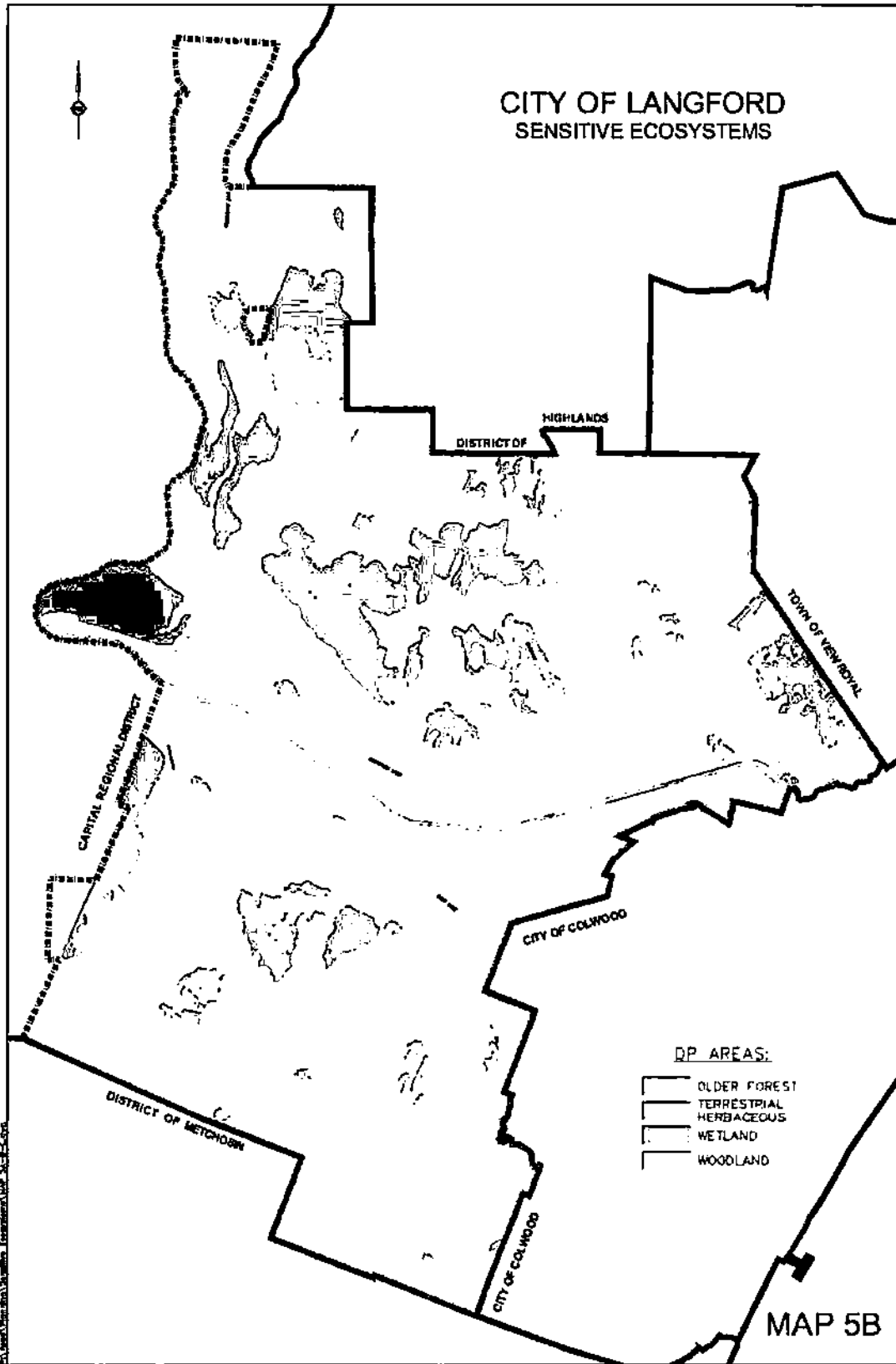
CITY OF LANGFORD RIPARIAN ZONES



MAP 5A

Law Periodic Sup. 04, 2004

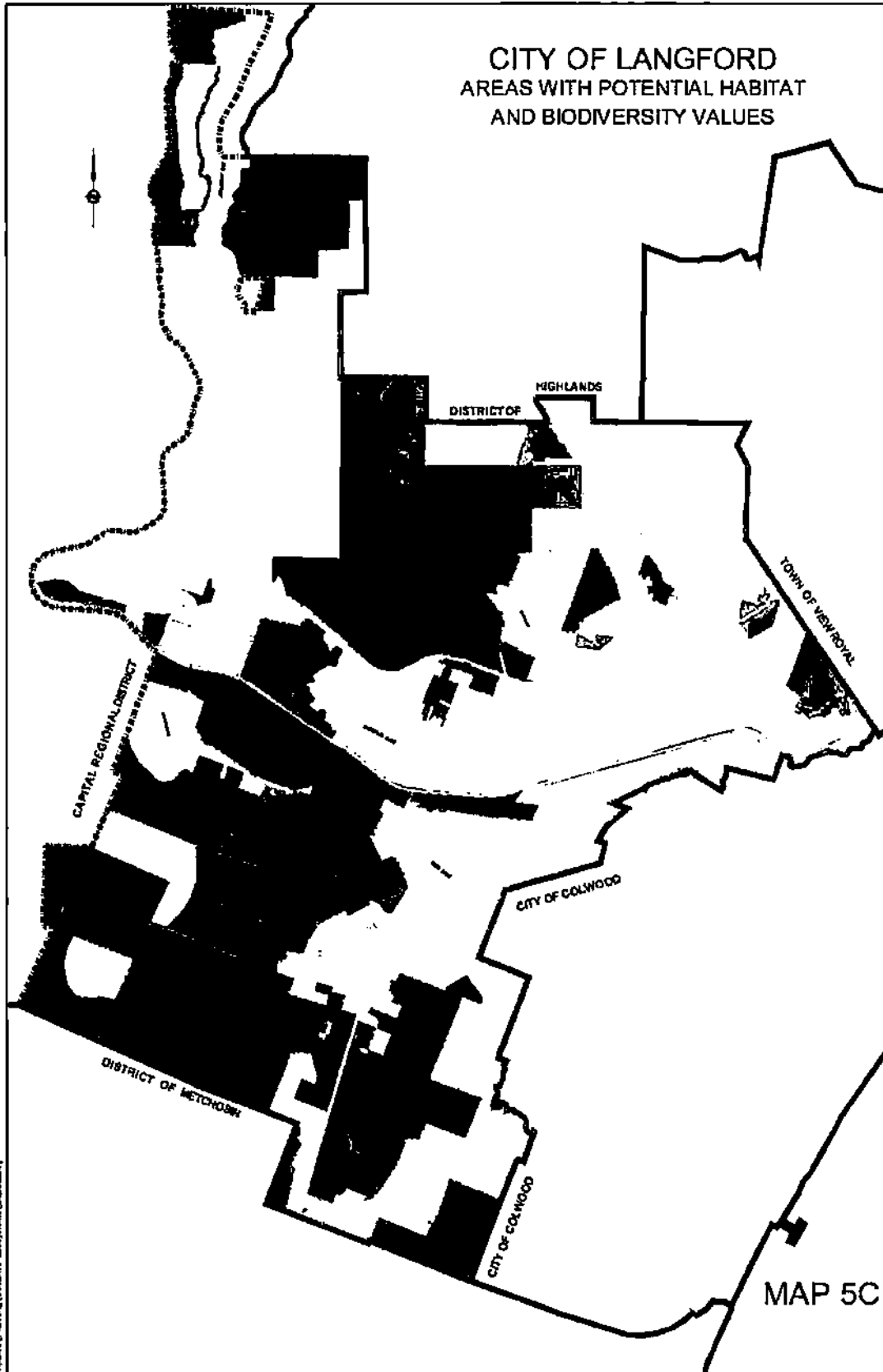
CITY OF LANGFORD SENSITIVE ECOSYSTEMS



City of Langford Planning & Development Services

Last Revised: Sept 06, 2008

CITY OF LANGFORD
AREAS WITH POTENTIAL HABITAT
AND BIODIVERSITY VALUES



C:\Users\jmartin\Documents\Langford\Map5C.dwg

Last Revised: Sept. 04, 2000

MAP 5C

Appendix B: Development Permit Area

GENERAL

Applicants are required to provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason shall be made. Council may diverge from these guidelines where a compelling rationale which preserves the qualitative intent of the guidelines is supplied.

DEVELOPMENT PERMIT AREA

Lands in the vicinity of the Millstream Connector, Attree Avenue, Kelly Road, and Jacklin Road as set out in Map 3A.

DESIGN GUIDELINES

1. **Layout**
 - 1.1 Development shall present an active, attractive face to the exterior. Observers on the Millstream Connector, Kelly Road, Jacklin Road, and Attree Avenue should perceive "fronts" of commercial retail units, extensive landscaping, and entries to the interior.
 - 1.2 The "main" entrance to the primary site, the CanWest Mall lands, shall be off Attree Avenue in alignment with Phipps Road.
 - 1.3 Pedestrian access shall lead directly to well demarcated entries to buildings. Building entrances shall be visible to drivers entering the site.
 - 1.4 Buildings shall be grouped and organized into a co-ordinated commercial village.
 - 1.5 Significant building elements shall be located in close proximity to boundary roads. Setbacks shall be incorporated at vehicular entrances to facilitate appropriate traffic sightlines.
 - 1.6 Buildings more than two storeys in height shall be set back from roads by a distance equal to at least twice their height. Where a proposed one- or two-storey building element at least 3 m (10 ft) high faces an external street, the setback of taller building elements can be reduced to a dimension equal to the height.
 - 1.7 Service areas shall be screened from public view or shall incorporate materials, geometry, and features such as door canopies which are aesthetically integrated with more public areas. Service areas shall incorporate measures such as lights and view corridors which will discourage vandalism and criminal activity.
 - 1.8 Generally, all utilities shall be underground along the extent of any fronting road and on the subject site. Kiosks, connection boxes, and meters shall be either underground or screened with landscaping to a degree which renders them indistinct, but functionally accessible.

- 1.9 The Kelly Road/Millstream Connector intersection shall be emphasized as both an entry to Langford and to the Auto-Oriented Business Area. This should be done by special landscaping on all four corners, the use of a monument, symbol or sign and the opening of a vista as vehicles and pedestrians enter Langford. The Galloping Goose Regional Trail should be diverted to this intersection for both symbolic and safety reasons.
- 1.10 Building siting and design and the orientation of windows designed to allow visibility from the interior to the exterior should afford views of surrounding hills and mountains and the open horizons offered by lowlands.

2. Design

- 2.1 New development shall use materials such as bricks and coloured glass which distinguish present development. Materials shall be durable and of good quality.
- 2.2 All new developments are encouraged to incorporate river rock as a façade treatment, and as an element in freestanding signage.
- 2.3 Design shall reflect the heritage of Langford. Applicants are advised that Langford has a history which includes mining, logging, firewood cutting, farming, and transport. The use of materials, forms, and artifacts to provide a character reflecting the heritage of Langford is recommended.
- 2.4 Development within the Development Permit Area shall include at least one building mass such as a clock tower or spire which will be visible from a substantial distance on approaches to the subject Development Permit Area. Primary landmark shall be on the CanWest site with lesser landmarks visible at lesser distances on other commercial sites.
- 2.5 Intense colours shall be used sparingly and for architectural effect such as demarcating entrances. Continued use of warm tones and materials similar to those used in existing development which accept rain gracefully are encouraged.
- 2.6 Long horizontal elements such as canopies or parapets shall be interrupted by projections, changes of orientation or other such device. The spacing of these devices should relate to recesses, windows, buttresses, or other elements used to mitigate long wall planes as set out in item 3.4.
- 2.7 Extensive roof planes which are overlooked from the exterior of the site should be avoided. Roof visibility at the north east corner of the site from the Millstream Connector is likely. Use of gabled roofs or building height sufficient to obscure flat roofs from exterior grade is recommended. Height is subject to provisions of other guidelines. Applicants are encouraged to place active uses on large extents of flat roof such as plazas, sports facilities, and gardens.
- 2.8 Roof mounted and exterior mechanical units shall either be of architectural character or obscured from public view by architecturally integrated elements.
- 2.9 Outdoor storage, loading bays, large refuse containers, and recycling facilities shall be screened from public view in accordance with item 5.2.

- 2.10 The City Planner may approve variances to allow signage constructed using tubular neon, provided that the signage is complimentary to the form and character of the commercial building, is in keeping with surrounding commercial development, and does not negatively impact neighbouring residential areas.

3. Scale

- 3.1 The overall character of the scale of development shall be horizontal building masses with entrances and other appropriate landmarks indicated by vertical elements.
- 3.2 Generally, development should be limited to two storeys in height. However, where building massing is architecturally related to lower elements, both in proportion and character of form and finishes, taller building elements may be used. Taller buildings shall incorporate soffit lines of surrounding one- or two-storey buildings as a three-dimensional architectural feature.
- 3.3 Development shall be expressed by individual building elements. Smaller elements shall be located toward the exterior of the site, larger elements toward the interior and partially obscured by the smaller elements.
- 3.4 Where the horizontal extent of any wall is more than three times its height, some architectural device such as a recess, window, or buttress shall be incorporated into the wall.

4. Traffic and Parking

- 4.1 Design shall accommodate all modes of customer and service movement. This includes domestic cars, delivery trucks, transit (buses, future LRT), pedestrians, wheeled devices such as bicycles, wheelchairs, and prams.
- 4.2 Vehicle lanes shall be clearly indicated. Access to and from bordering streets shall be designed to be visible and to provide adequate car stacking lanes.
- 4.3 Provision for pedestrian movement from the exterior of the site and from parking areas to entrances shall be clearly indicated.
- 4.4 Where muscle powered traffic and engine powered traffic cross, the continuum of paving materials shall emphasize pedestrian movements. The use of slightly elevated pavers is recommended to slow vehicles and provide positive drainage from paths.
- 4.5 Primary vehicular entrances to the site shall incorporate bicycle lanes. Bicycle lanes shall be designed to avoid conflict with other vehicles and shall be separated from pedestrian alignments.
- 4.6 Secure parking for bicycles shall be provided at each building entrance serving either several commercial retail units or any single commercial unit exceeding 4 500 m² (50 000 ft²).
- 4.7 Parking for vehicles shall be divided into units accommodating no more than 120 vehicles. Units shall be separated by either the mass of a building, a pedestrian open space, or a significant landscape screen consisting of both continuous planting between 0.6 and 1.0 m

(2 to 3 ft) in height and trees at most 6 m (20 ft) on centre having a canopy at least 2 m (6 ft) high.

- 4.8 Provision should be made to encourage transit use to, from, and on the CanWest site.
- 4.9 In order to provide for efficient and safe vehicular access to properties and between properties, the design of parking areas and vehicle manoeuvring aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.
- 4.10 The City Planner may approve Development Permits with variances for parking standards, where the request for such variances are supported by a satisfactory study prepared for the City of Langford by a qualified professional.

5. **Landscaping**

- 5.1 The perimeter of the Development Permit Area shall be fully landscaped so as to present a pleasing aspect to the exterior. Landscaping design shall emphasis entrances.
- 5.2 Extensive landscaping shall be used to screen "passive" building elements (i.e., those which do not incorporate storefronts, extensive windows to publicly accessible interiors or publicly accessible entrances). Initial planting size shall be at least one-third of the height of the building element being screened. Plant material is preferred over fencing for screening.
- 5.3 Fences or free standing walls shall incorporate two distinct materials and shall include integrated planting. A fence or freestanding wall shall have a significant change in plane for any horizontal dimension which exceeds eight times its height.
- 5.4 Landscaping which is close to the perimeter of the site shall incorporate vegetation such as native or commonly used species which relate to the existing character of Langford. Faster growing species shall be used to provide "interim" planting where slow growing species are incorporated.
- 5.5 A registered Landscape Architect or Arborist shall prepare a plan which will incorporate plant species suitable for the particular microclimate.
- 5.6 Landscape design shall incorporate a variety of materials which will provide seasonal interest, pleasant fragrances, and colour highlights.
- 5.7 Landscape design shall incorporate either low maintenance provisions or appropriate measures, such as irrigation and maintenance undertakings, to ensure initial plantings will flourish.
- 5.8 Development permits shall incorporate means, such as bonding, by which the establishment of initial plantings is assured for at least one full growing season.
- 5.9 Street furniture such as benches, lamps, and refuse containers shall be incorporated in the landscape design. These shall be required to be consistent, similar, or identical in character to the architectural character of the development and identified by type and source in Development Permit documentation.

5.10 Open spaces acting as “cross-roads” or as sites of public assembly shall incorporate special landscape features such as fountains, floral displays, or monuments as focal elements.

5.11 Interim landscaping shall be provided, to the satisfaction of the City Planner, on every part of a commercial development site that is not immediately developed according to the ultimate landscape plan included with a Development Permit.

6. **Open Spaces**

6.1 Any building incorporating exterior access shall provide a pedestrian walk at least 2 m (6 ft) wide related to its frontage.

6.2 Where pedestrian traffic will be focused during seasonally busy periods, the shape and width of sidewalks should anticipate their usage.

6.3 Intersections of pedestrian walkways, other than simple corners, shall incorporate additional width.

6.4 Buildings or building complexes with floorplates exceeding 4 500 m² (50 000 ft²) shall provide exterior open spaces suitable for public assembly, temporary markets and the presentation of entertainment equal to 10 percent of the area of the floorplate. If parking areas are used for this purpose, they shall be distinguished by special paving and planting from ordinary parking areas. Such open spaces may be associated with entries to buildings, but should be designed to allow for both external activities and unfettered access.

7. **Signage and Lighting**

7.1 Applications for Development Permit shall include a signage and lighting program.

7.2 Multi-tenant projects shall be identified by one large sign per frontage which emphasizes the name of the development. Where individual tenancies are identified on such a sign only the shape of the name lettering or logo shall be incorporated; all shall be of a uniform neutral colour and all shall occupy a field less than 20 percent of any aspect of the sign.

7.3 Individual tenancies shall be identified by one sign incorporated in the facade of the tenant building and visible from the exterior of the site.

7.4 Signage may be incorporated below the soffit of a roof or on a canopy, but not on both and not in any other location (such as above the soffit or parapet).

7.5 Provision for temporary signage shall be identified in the Development Permit. This includes any kind of “non-permanent” signage such as banners, portable signs, inflatables, lights, luminous projection, etc. The development permit shall identify mounting brackets, locations, and any other provision necessary to display temporary signs. Signs which do not comply with these provisions are not permitted.

7.6 Volumetric or three-dimensional signs incorporating the architectural theme and materials of the development and illuminated from the outside are encouraged. Any wall-mounted sign not incorporated in a pre-designed sign band shall use freestanding three-dimensional letters and logos.

- 7.7 Murals of artistic merit where commercial identification occupies less than 5 percent of the surface area are permitted.
- 7.8 Lighting fixtures co-ordinated with those currently used at CanWest Mall shall be used through the Development Permit Area. Illumination design shall ensure adequate visibility in vehicular and pedestrian areas.
- 7.9 Lighting suitable for proper functioning and security of service areas shall be installed.
- 7.10 Measures to prevent glare reaching the exterior of the site, including horizontally and vertically, shall be taken.

8. **Acoustical Design**

- 8.1 Sources of continuous or intermittent sound likely to cause a nuisance to lands beyond the site such as refrigeration units or unpacking areas shall be identified and mitigating measures incorporated in the design.

9. **Unenclosed Storage**

9.1 **Setback**

- 9.1.1 Unenclosed storage is not permitted in any required front or exterior side yard setback.

9.2 **Parking**

- 9.2.1 Commercial and business park developments are encouraged to identify, at the time of a Development Permit for the principal use building, areas on site that will be used for seasonal unenclosed storage. These areas should be available for parking when not in use for unenclosed storage.

9.3 **Pedestrian and Vehicular Circulation**

- 9.3.1 Unenclosed storage should not impede either vehicular or pedestrian traffic;
- 9.3.2 Unenclosed storage should be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and
- 9.3.3 Unenclosed storage should not interfere with sight lines for either pedestrian or vehicular traffic.

9.4 **Screening**

- 9.4.1 All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;
- 9.4.2 The use of chain link fencing is prohibited; and
- 9.4.3 The use of temporary wire fencing is prohibited.

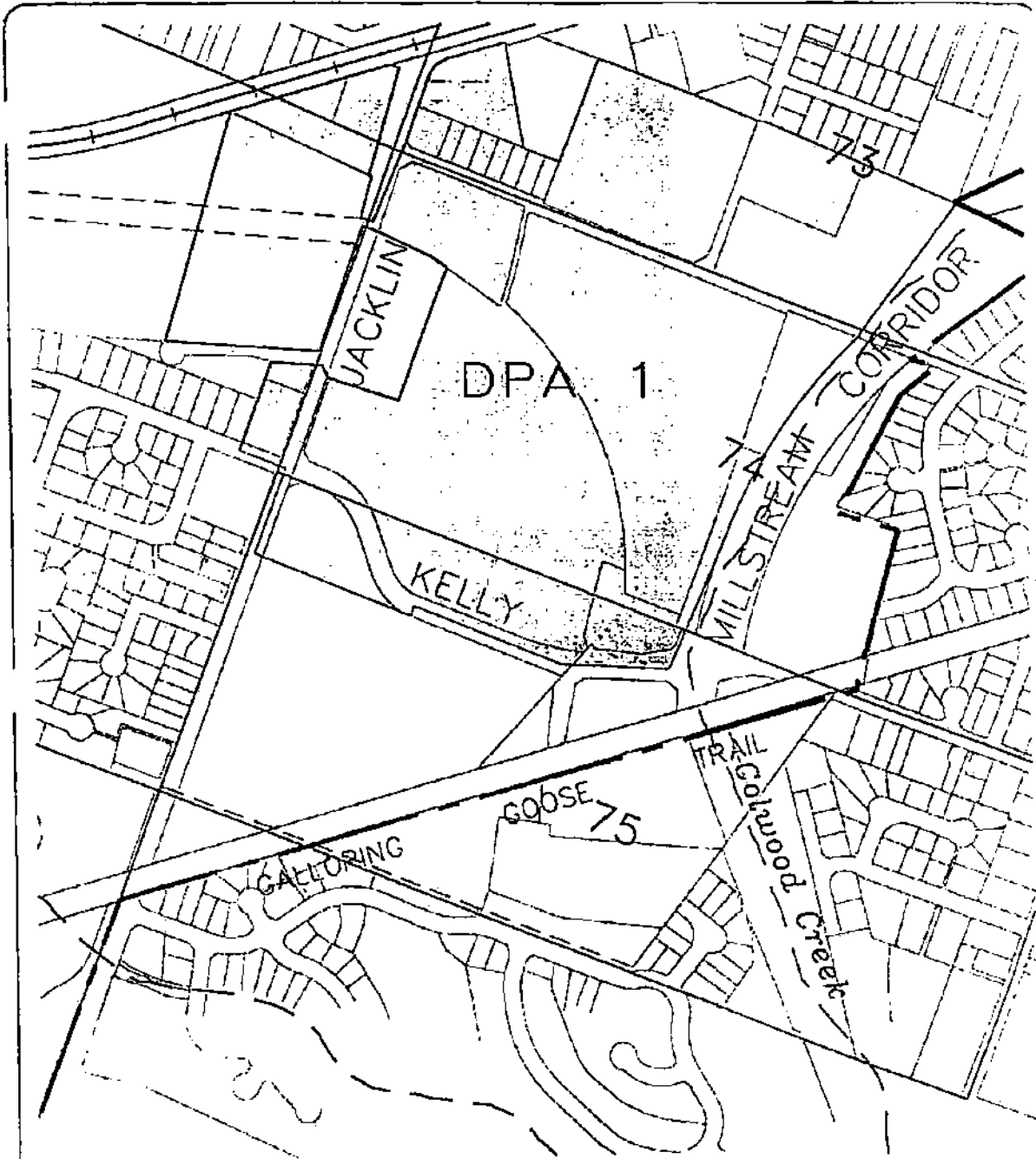
9.5 Storage of Combustible or Potentially Hazardous Material

9.5.1 Storage areas for toxic, combustible or potentially hazardous material such as liquid petroleum products, fertilizers, herbicides and pesticides must not be sited outside buildings.

9.6 Landscaping

9.6.1 Unenclosed storage is not permitted within any required landscape screening area; and

9.6.2 Unenclosed storage is not permitted in any landscape area, unless integrated with the landscaping in a manner that is unobtrusive, does not deteriorate the plantings and landscape material within the landscaped area; and does not interfere with sight lines.



MAP 3A
DISTRICT OF LANGFORD

I HEREBY CERTIFY THIS TO BE A TRUE
 COPY OF PLAN No.2 AS DESCRIBED IN
 SECTION 1e. OF BYLAW No.165.

Feb 3 / 97
 ADOPTION

[Signature]
 CLERK ADMINISTRATOR

Appendix C: Business Park Development Permit Area Guidelines

GENERAL

01. Compliance or Justified Alternative

Council regards the concepts presented in the Langford Business Park Design Guidelines as a starting point and welcomes the opportunity to review innovative or creative development that may not directly meet the design criteria noted in the guidelines. If the applicant does not comply with the Design Guidelines, a justification stating the reason for diverging from the Guidelines should be made.

DESIGN GUIDELINES

1. Layout

1.1 Buffer Storage and Parking

Outdoor storage areas and parking areas should be contained in a 5 m (16.4 ft) wide, 2 m (6.6 ft) high landscaping strip or solid decorative fence where development abuts or faces residential land uses or lands zoned or designated in the Official Community Plan (OCP) for residential use.

Outdoor storage areas and parking areas should be contained in a 1.5 m (5 ft) wide, 2 m (6.6 ft) high landscaping strip or solid decorative fence where development abuts or faces commercial or service commercial land uses or lands zoned or designated in the OCP for commercial or service commercial uses.

In addition, outdoor storage areas must be screened by fencing and landscaping in accordance with relevant guidelines. Buffers may be reduced or eliminated where deemed appropriate.

1.2 Parking and Loading Away From Front

Where practical, outdoor parking and loading areas relating to operations of the business should be located to the sides and rear of buildings. Any parking visible from a fronting or flanking street should be screened with plants or fences at least 1 m (3.28 ft) high on a 3 m (9.8 ft) wide landscaped area (except pedestrian or vehicular crossings). Planting should be located so as to reduce the visual extent of a parking area. The City Planner may approve Development Permits with variances or parking standards, where the request for such variances are supported by a satisfactory study prepared for the City of Langford by a qualified professional.

1.3 Landscape Unused Space

Any areas, such as setbacks, which may not be used for business purposes should be landscaped within the developed area.

1.4 **Outside Amenities**

Outdoor patios or amenity areas of adequate dimensions should be encouraged for people working on site. Suggested standards are 1 m² (10 ft²) per employee or per 200 m² (2 153 ft²) of building area, whichever is more.

1.5 **Screen Garbage Containers**

Garbage containers and areas for assembly and garbage handling should be screened and gated. Screening should be constructed of a similar design, fashioned from similar materials and finished with similar colours to those used in the primary building. Garbage containers must be located behind the front face of the buildings.

2. **Design**

2.1 **Contemporary Design**

Developers are encouraged to utilize contemporary designs. The design should express the intended use. Office areas, manufacturing areas, storage areas, and other building parts should be given distinctive expression by the use of appropriate scale; volumes which express internal activities; proportion of openings; and the choice of building materials, finishes, and signage.

All blank walls (e.g. those walls without fenestration) are to be treated, either with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building, or with a painted mural or other artwork approved by Council, and that a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.

2.2 **Design Assessment**

Langford will assess designs through an approval process. The goal of this process is to compare the submitted design with these guidelines and to consider the rationale documented in accordance with Guideline 0.1. The assessment process is not the design process; which is the responsibility of the developer.

2.3 **Use Appropriate Materials**

Structures should be constructed of durable materials. Materials should be chosen for ease of maintenance. Except where a visual accent is warranted, muted colours chosen from a palette representative of Langford's natural surroundings should be used. All new developments are encouraged to incorporate river rock as a façade treatment, and as an element in freestanding signage.

2.4 **Scale**

A human scale should be respected where buildings and other structures face a street. This is expressed in window size, sill heights, door shelters, and soffit height. Designs should capitalize on dramatic elements such as towers or mechanisms relating to use or processes associated with the business.

2.5 Screen Mechanical Units

Ancillary mechanisms should be integrated in the design or screened from public view by structures relating to the building design.

2.6 Locate Utilities Discreetly

Utilities, connections, junctions, and meters should be placed in underground vaults or screened from public view.

3. Access

3.1 Create Frontage Roads

Minimize access to the main roads with such measures as frontage roads and shared driveways.

3.2 Appropriate Access

Access should be designed to accommodate vehicles contemplated to service the business without obstructing flow of traffic on public roads. For example, lay-bys and room to turn trucks on-site should be provided where a business relies on semi-trailer trucks.

3.3 Pedestrian Accommodation

Pedestrian access to site should be well marked. The connection between front doors and public streets should be made by paved sidewalks.

3.4 Bicycles

Where warranted by number of employees and customers, bicycle parking should be provided on-site.

3.5 Reciprocal Access

In order to provide for efficient and safe vehicular access to properties and between properties, the design of parking areas and vehicle manoeuvring aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

4. Fences

4.1 Fences

Chain link fencing should be screened from the street and non-industrial or Business Park uses.

4.2 Fences and Landscaping

Fences visible from the street should be set back and supplemented with landscape materials. The setbacks should be inversely proportional to the initial planting size of vegetation. Where vegetation is the same height as the fence, the fence should be setback 0.5 m (1.6 ft).

4.3 **Fence Design**

Where fences are visible from the street or non-industrial neighbours, they should consist of two materials and be articulated by horizontal changes in line of at least 0.5 m (1.6 ft). Examples of "different materials" include brick piers with wood panels or wood panels with lath lattice along the top. The lesser used material should be used for at least 15 percent of the face of the fence visible from the street or non-industrial neighbours.

5. **Landscape**

5.1 **Perimeter Buffer**

The buffer referred to in Section 1.1 shall be landscaped.

5.2 **Trees in Unused Parts**

Undeveloped areas should be left in a natural state, if there are substantial existing vegetation or landscaping. "Substantial existing vegetation" means a plantation of trees at least 2 m (6.5 ft) tall with under-storey forming a recognized ecological succession community.

5.3 **Major Existing Trees**

Trees are a significant element of the character of Langford. By preserving or replacing these landmarks, developers can lessen the impact which new development often has.

5.4 **Native Vegetation**

Landscaping which is close to the periphery of the site should incorporate vegetation such as native or commonly used species which relate to the existing character of Langford. Faster growing species should be used to provide interim planting where slow growing species are incorporated.

5.5 **Visible Intersections**

Landscape design should accent intersections to improve visibility and assist with navigation.

5.6 **Pavement and Planting**

Parking areas should be paved except as specifically exempted in the Development Permit and should incorporate landscape elements to provide breaks in rows of automobile parking stalls. A suggested standard for this is a landscaped area at least one half the width of a parking stall between each row of ten adjacent parking stalls. In considering this guideline, Langford Council will take specific siting circumstances into account.

5.7 **Ease of Maintenance**

Maintenance should be considered in design. Means of irrigation, access by appropriate tools and disposal of waste should be included.

5.8 **Height v. Setback**

As noted elsewhere, density, and height of initial planting may substitute for setback.

5.9 **Interim Landscaping**

Interim landscaping shall be provided, to the satisfaction of the City Planner, on every part of a business park development site that is not immediately developed according to the ultimate landscape plan included within a Development Permit.

6. **Signage And Lighting**

6.1 **No Roof Signs**

All signage should be located below the roof line.

6.2 **Coordinated Signs**

Signage should be considered and coordinated in the design expressed in the Development Permit. Three types of signage are contemplated:

6.2.1 **Identification Sign**

One sign visible from any location outside a development which primarily identifies the development. This sign may be freestanding in a landscaped area or incorporated in the design of the building. It should be no higher than the first storey of the office portion of the primary building on the site it identifies. A freestanding sign should incorporate at least one material, finish, and colour of the main building.

6.2.2 **Information Signs**

Signs occupying co-ordinated locations relating to each tenancy or providing directional information. These should be located on fascias no more than 1 m (3 ft) wide near or over doors and windows intended for public access. If awnings carry messages, no other signs are permitted.

6.2.3 **Temporary Signs**

Provisions for temporary signage should be made in the Development Permit. No other temporary signs are permitted.

6.3 **Lighting**

Lighting should be designed to foster security, but not to shine onto adjoining lands. Entries should be illuminated and visible from the street. Security lighting should have armoured housings.

6.4 **Lighted Entries**

Vehicular and pedestrian access to the site should be illuminated in a manner which facilitates visibility and entrance movements from the adjacent road.

6.5 **No Glare**

On-site lighting and signs should not produce glare on neighbouring roads, properties, or the sky.

7. **Acoustical Design**

7.1 **Avoid Nuisance Noises**

Processes likely to be the source of repetitious or very loud noises which will create a nuisance on adjacent properties and must be accommodated in the design of the development.

7.2 **Obscure Noise Sources**

Outdoor work areas should be shielded from non-industrial neighbours by buildings or significant structures capable of attenuating incidental noise.

7.3 **Sound Mitigation**

The City Planner may require a sound mitigation plan, prepared by a qualified acoustical engineer, and sound mitigation in accordance with any such plan, for any use that would likely generate noise that could be a nuisance to neighbouring residential uses.

8. **Unenclosed Storage**)

8.1 **Setback**

8.1.1 Unenclosed storage is not permitted in any required front or exterior side yard setback.

8.2 **Parking**

8.2.1 Commercial and business park developments are encouraged to identify, at the time of a Development Permit for the principal use building, areas on site that will be used for seasonal unenclosed storage. These areas should be available as additional space for parking when not in use for unenclosed storage.

8.2.2 The City Planner may approve Development Permits with variances in off-street parking requirements, where the request for such variances is supported by a Parking Demand Study for the proposed use, prepared for the City of Langford by a qualified professional. The Parking Demand Study may consider proximity to bicycle routes and public transit as mitigating factors in determining the demand for off-street parking.

8.3 **Pedestrian and Vehicular Circulation**

8.3.1 Unenclosed storage should not impede either vehicular or pedestrian traffic;

8.3.2 Unenclosed storage should be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and

8.3.3 Unenclosed storage should not interfere with sight lines for either pedestrian or vehicular traffic.

8.4 **Screening**

8.4.1 All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;

8.4.2 The use of chain link fencing is prohibited; and

8.4.3 The use of temporary wire fencing is prohibited.

8.5 **Storage of Combustible or Potentially Hazardous Material**

8.5.1 Storage areas for toxic, combustible or potentially hazardous material such as liquid petroleum products, fertilizers, herbicides and pesticides must not be sited outside buildings.

8.6 **Landscaping**

8.6.1 Unenclosed storage is not permitted within any required landscape screening area; and

8.6.2 Unenclosed storage is not permitted in any landscape area, unless integrated with the landscaping in a manner that is unobtrusive, does not deteriorate the plantings and landscape material within the landscaped area; and does not interfere with sight lines.

8.6.3 The City Planner may approve Development Permits with variances in the required width or depth of landscaping where appropriate landscaped separation and screening are provided between roads and buildings, including landscaping on a municipal boulevard.

GUIDELINES FOR INDIVIDUAL AREAS

9. **Millstream Road - North of the Trans Canada Highway**

In addition to the above general guidelines, developments in the Millstream Business Park area should:

9.1 Preserve significant trees especially on the western perimeter.

9.2 Enhance the entrance to the residential area at the north end.

9.3 Minimize accesses to Millstream Road by devices such as frontage roads.

9.4 Provide supplemental landscaping between buildings within the development.

10. **Dunford Avenue - East of Langford Lake**

In addition to the above general guidelines, developments in the Dunford Business Park area should:

- 10.1 Use fences, hedges, and planting to screen open storage from adjacent residential areas.
- 10.2 Place buildings in a landscaped setting.
- 10.3 Provide an attractive entrance feature.

11. **Business Park 1A – Millstream Road East**

- 11.1 Council's general Development Permit design guidelines for multi-family and intensive residential form and character apply to this area. In the event of a conflict between guidelines, the multi-family and intensive residential form and character design guidelines shall prevail for multi-family and intensive residential development, otherwise the business park design guidelines shall prevail.

Appendix D: Guidelines for the Development of Olympic View

1. Olympic View Area Plan

A new comprehensive development plan for the Olympic View area was submitted to the City in 2001. The plan was submitted by Bullock Baur Associates on behalf of the owners, GolfBC Ltd. The Olympic View lands straddle the Colwood-Langford border with approximately 56 ha (140 ac) in the City of Colwood and 52 ha (130 ac) in the City of Langford.

OBJECTIVE WITH RESPECT TO LANDS WITHIN CITY OF COLWOOD

As the proposed Olympic View neighbourhood includes lands in both Colwood and Langford and as the entire neighbourhood is accessed and serviced through Colwood, it is the objective of Langford to liaise with Colwood in the comprehensive planning for and creation of the new neighbourhood

OLYMPIC VIEW CONCEPT DETAILS

This plan proposes a new Olympic View neighbourhood, a mixed development which in Colwood would include up to 456 dwelling units, together with 25 hotel rooms, a village centre with 2250 m² (7382 ft) of commercial/office space, parks and open space in Langford. The total number of residential dwellings in both Colwood and Langford would total 917. Attached as Map D1 is the land use concept plan for the proposed development.

In designing this new neighbourhood, it is proposed to preserve and augment the character of the landscape, and to give a strong social focal point in the form of the Village Centre. Residential densities will be consistent with the desire to retain and translate the natural character of the site into a diverse, lush and prestigious community.

A mix of housing types and lot sizes is proposed. These will be influenced by the ability to provide access, neighbouring uses, and the topography, which ranges widely, with relatively level areas, low lying wetlands, moderately hilly areas, and steep and rocky outcrops.

POLICIES

The policies set out in this section pertain only to those portions of the Olympic View area that are within Langford.

1. This Area Plan is intended to give a conceptual picture of how the lands may be developed, provided that the necessary environmental and planning studies are satisfactorily completed and the necessary approvals are in place. Designation boundaries and numbers are necessarily approximate and will be refined by further studies and the subdivision approval process.
2. Discussions are to be commenced with regard to an inter-municipal servicing agreement between the landowner, Langford, and Colwood.

3. An inter-municipal servicing plan (sewer, storm water, and roads) is to be prepared for the site with a technical and environmental approach to include guidelines for the protection and enhancement of Latoria Creek and other creeks and specific servicing standards are to be developed to apply to the project.
4. The need for off-site service infrastructure is to be reviewed together with means of implementation, to ensure that appropriate development cost charges or other means of implementation are in place.
5. Transport planning, together with the possibility of a transit exchange, will be considered in more detail within either future planning for the project, the subdivision process, or a Development Permit process.

2. Design Guidelines for Environmental Protection

JUSTIFICATION

The City of Langford has designated environmentally sensitive areas as development permit areas for the purpose of preserving and enhancing sensitive ecosystems wherever possible, and preserve and enhance general habitat, biodiversity and natural environment regimes across the entire Olympic View area.

OBJECTIVE

The objective of these Development Permit Guidelines for environmental protection area to supplement the Development Permit Area Guidelines found in Appendix "A". These Development Permit Area Guidelines are also specifically for the development of the property commonly known as "Olympic View". These design guidelines are intended to ensure that:

1. Environmentally sensitive areas are preserved and enhanced;
2. General habitat, biodiversity values and natural environment regimes are preserved and enhanced; and
3. That the majority of riparian values are preserved and enhanced, and that the slope instability and erosion are minimized in areas adjacent to water bodies.

When there is a conflict between these guidelines and the general development permit area guidelines found in Appendix "A" of this Plan, these guidelines will supersede the General Development Permit Area Guidelines.

OLY1. RIPARIAN AREAS

- 1.1 Although development within 43 m (141 ft) of water bodies and watercourses is discouraged, the City Planner may grant variances, through a Development Permit, for work within the riparian area, subject to the applicant providing, at their expense, an appropriate and acceptable report, prepared by a qualified biologist, that informs the City Planner of the riparian values that may be disturbed, damaged or destroyed by intrusion into the riparian area, and provides a plan for the mitigation of loss of, or damage to riparian values;

- 1.2 There shall be no development of any golf course in or around any riparian area until such time as an appropriate landscape and golf course management plan has been devised, and registered on title. This plan should include strategies for limiting incursion into the riparian areas, strategies for irrigation, and strategies to limit the potential hazard, to the riparian area, of leachates from pesticides, herbicides, fertilizers or landscape materials (e.g.: bark mulch, gravel).

OLY2. SENSITIVE ECOSYSTEMS, WILDLIFE HABITAT AND BIODIVERSITY

- 2.1 Rock outcrops should be preserved and integrated into site design;
- 2.2 Every effort should be made to preserve veteran trees and snags, and to identify these in a conspicuous manner so that the public may be made aware of their present and future value, and the reason for their preservation;
- 2.3 The hardhack fen, identified on the concept plan found at the end of this appendix as Map D1, shall not be disturbed, damaged or destroyed, and a setback of 43 m (141 ft) shall be maintained from the edge of this fen area. The City Planner may grant variances for work within this setback area, subject to the applicant providing, at their expense, an appropriate and acceptable report, prepared by a qualified biologist, that informs the City Planner of the wildlife and habitat values of the hardhack fen area that may be disturbed, damaged or destroyed by intrusion into the prescribed setback.

3. *Design Guidelines for Form and Character of the Olympic View Development*

JUSTIFICATION

The City of Langford has designated all commercial and Multi-Family Residential Development as a Development Permit Area pursuant to the provisions of the *Local Government Act*. The justification of this designation is to ensure that Council has the ability to secure the necessary information and establish conditions on developments such that the form and character of new commercial and multi-family residential development are of a high quality, and best suited to both the surrounding properties and the vision of Langford as expressed in the Langford Plan.

OBJECTIVE

The objective of these Development Permit Guidelines is to supplement the General Development Permit Guidelines for Commercial and Multi-Family Residential Development (found in Appendix "G" and "H" respectively) specifically for the development of the property commonly known as Olympic View. These design guidelines are intended to ensure that development is:

1. Constructed to high standards, both material and aesthetic;
2. Complementary to the social and environmental goals of this plan; and
3. Compatible with other surrounding land uses.

The essential nature of the Olympic View development is a high quality mix of single- and multi-family residential and commercial uses. The form and character of the development and the design of the landscaping and pedestrian systems are an important means to achieving this.

When there is a conflict between these guidelines and the general guidelines, these guidelines will supersede the General Design Guidelines.

A concept plan showing Areas "1" and "2" may be found at the end of this appendix as Map D1.

OLY3. STREETSCAPE

3.1 The overall character and appearance of the streetscape is to be developed through the harmonious inter-relationship of buildings. Individual buildings should complement both existing buildings and the streetscape as a whole. Buildings should not be identical, but should reflect a common architectural style, and should relate to the desired and existing residential character of the area in terms of:

- a) articulation and massing of the building;
- b) type and location of windows and openings;
- c) type and colour of building materials;
- d) setbacks
- e) porches and entry; and
- f) garages and carports.

3.2 Dwelling siting, window placement and landscaping should minimize visual impact or avoid acoustic impact on neighbouring windows or yards;

3.3 High walls that overshadow neighbouring yards should be avoided; and

3.4 Views in and out of neighbours' property should be considered before cutting or planting trees.

OLY4. BUILDING ORIENTATION AND SITING

4.1 Orientation and siting for any building should meet the following objectives:

- c) collect summer breezes;
- d) provide protection from winter wind and rain;
- e) work in harmony with the topography and minimize intrusive or radical site works;
- f) preserve, wherever possible, the natural vegetation, and in particular significant trees;
- g) take advantage of distant and local views;
- h) ensure maximum privacy;
- i) allow for usable yard space;
- j) allow access to site for construction and vehicles, while minimizing tree removal and rock blasting.

OLY5. ENTRANCES

- 5.1 Entrances or routes to buildings should be clear and accessible from the street frontage;
- 5.2 The front entry of a building should be defined with adequate overhangs for weather protection;
- 5.3 The design of entrances or routes to buildings should not hinder sightlines for surveillance of visitors and intruders; and
- 5.4 Garage doors should not be the dominant feature of a building's front façade. Garage doors should be sited to one side, slightly behind, or at right angles to the main street frontage.

OLY6. APPROPRIATE BUILDING STYLES

- 6.1 Appropriate building design is necessary to create and maintain the desired character of this neighbourhood;
- 6.2 Ideally, building design should be influenced by the natural setting, by climate, by strong relationships between indoor and outdoor living areas, and by the honest expression of structure and materials. The desired design approach is through a thoughtful response to site and landscape; and
- 6.3 The use of styles or elements of styles which are specific to another country, another climate or a largely urban street-based environment are discouraged. Log homes, pioneer buildings, and buildings which employ pastiche, revival, Tudor, Spanish or classical elements are discouraged in favour of a fresher, more inventive response to the local conditions.

OLY7. MATERIALS AND COLOURS

- 7.1 Materials and colours should be selected that are compatible and reflect the surrounding natural landscape, textures, and colours;
- 7.2 Pastels, 'tropical' colours, and brightly coloured roofs are not acceptable;
- 7.3 Desirable materials include stone walls, stained wood siding, cedar shakes, fibre-cement roof tiles in 'natural colours', metal roofs in natural colours, natural slate, river rock, and timber detailing; and
- 7.4 Materials will also be required to be of good quality suitable for damp conditions and the desired neighbourhood image.
- 7.5 All new developments are encouraged to incorporate river rock as a faced treatment and as an element in freestanding signage.

OLY8. PARKING

- 8.1 Car parking for residential and non-residential lots alike is to be visually screened from sidewalks and neighbours, and prevented from dominating the streetscape;

- 8.2 In the resort village centre or cluster housing developments, parking lots should be broken into smaller areas with landscape dividers or other elements. Ideally, an unbroken parking area should be no larger than about 6 to 8 spaces in the village centre, and 4 spaces in residential areas;
- 8.3 The use of a consistent height of tree canopy throughout larger lots will help provide weather protection and visual screening for overlooking buildings;
- 8.4 Access driveways should be designed to use a minimum of paving. Desirable materials include brick, concrete pavers, gravel, or crushed cemented rock to complement the building materials requirements and natural environment;
- 8.5 Free-standing parkades will not be allowed except where they form a basement in the village centre or in a cluster housing development; and
- 8.6 Provisions for secure bicycle parking must be made in all public areas.

OLY9. FENCES

- 9.1 In order to foster a safer, more community-oriented environment, tall fences that extend forward of the front building face are not encouraged;
- 9.2 If a fence is necessary for privacy or noise control, a partially voided, articulated fence in combination with planting and natural materials in keeping with the environment and the building, is encouraged;
- 9.3 Low rock walls and hedges are encouraged; and
- 9.4 Chain link fencing is strongly discouraged.

OLY10. OUTDOOR AREAS

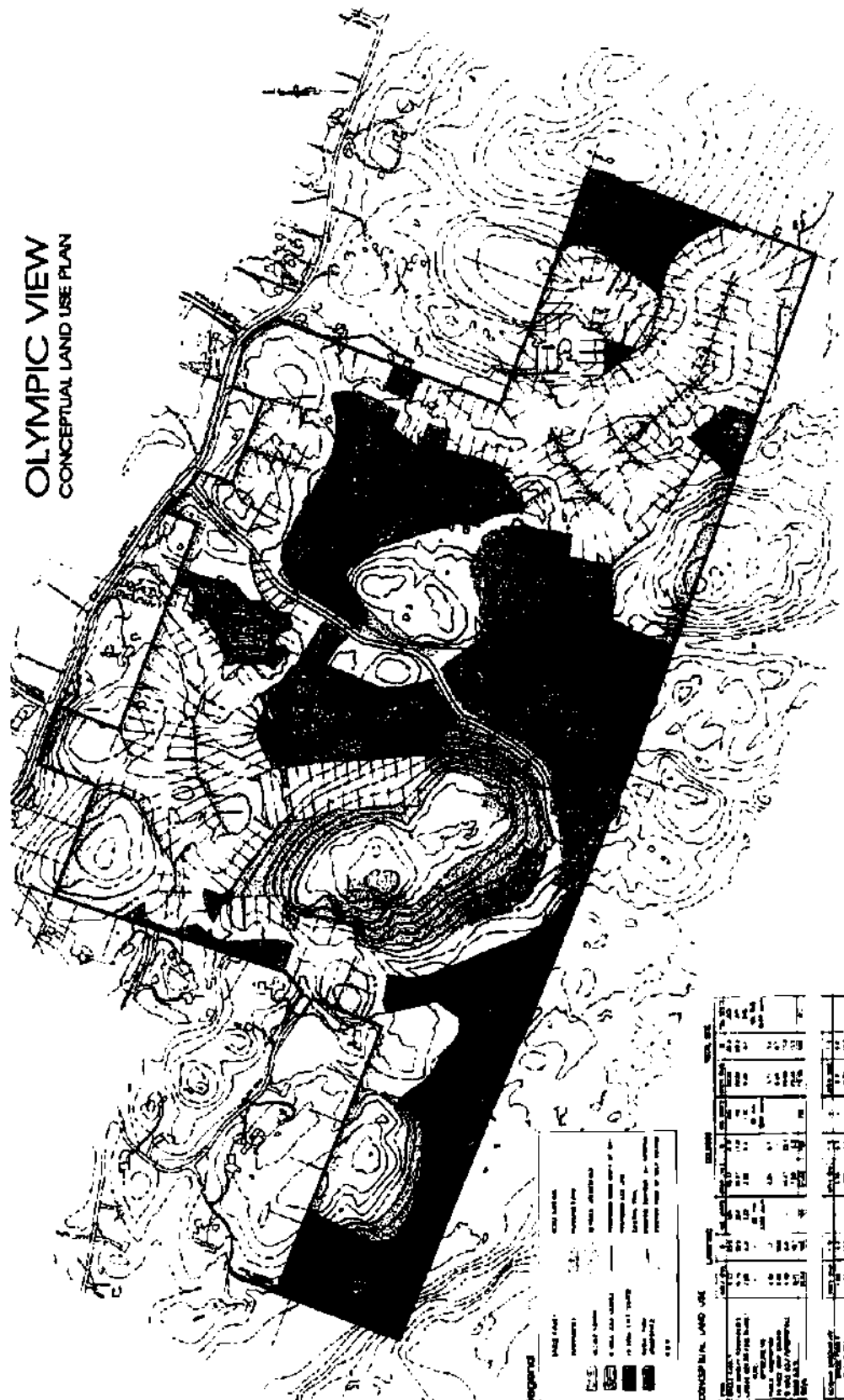
- 10.1 Buildings shall be sited and oriented to provide usable space for entertaining, utilities, storage, play areas, and views;
- 10.2 Unsightly storage and utility areas or kiosks shall be screened. Refuse containers, in particular, must be enclosed and fully-screened from the main streets and from other buildings; and
- 10.3 Accessibility from indoor areas, ability to capture sunlight and summer breezes, aural and visual privacy, and safety shall be considered when providing outdoor private or semi-private space.

OLY11. ENERGY EFFICIENCY

- 11.1 Siting and orientation of buildings in a manner that will increase the energy efficiency and subsequently reduce the cost required for heating, cooling, and lighting is encouraged;

- 11.2 Where possible, main living/retail spaces and windows should be located on the south side for natural light, winter warmth and summer shade for energy cost savings. The south side should be the longest side of the building for greatest passive solar gain. Shade the south, east, and west windows with eaves and deciduous plants;
- 11.3 Openings on the west sides of the building to catch the westerly summer breezes and on the opposite side of the building to assist in cross ventilation area encouraged; and
- 11.4 Higher openings or roof ventilation designed to expel summer heat is encouraged.

Map D1 – Olympic View Comprehensive Development Plan



OLYMPIC VIEW
CONCEPTUAL LAND USE PLAN

bullock baur

Legend

Symbol	Description
[Cross-hatch]	General Use
[Solid Black]	High Density Residential
[White]	Low Density Residential
[Dotted]	Commercial
[Diagonal Lines]	Industrial
[Wavy Lines]	Open Space
[Dashed]	Utility
[Thick Solid Line]	Major Road
[Thin Solid Line]	Minor Road
[Thin Dashed Line]	Proposed Road
[Thin Solid Line]	Waterway
[Thin Dashed Line]	Proposed Waterway
[Thin Solid Line]	Boundary
[Thin Dashed Line]	Proposed Boundary

CONCEPTUAL LAND USE

Category	Area (Acres)	Percentage of Total	Notes
General Use	1,200	45%	
High Density Residential	800	30%	
Low Density Residential	1,000	35%	
Commercial	200	7%	
Industrial	100	3%	
Open Space	500	18%	
Utility	50	2%	
Total	2,650	100%	

Olympic View Development
Scale 1:5,000

Appendix E: Goldstream Meadows Area Plan

1.0 GOLDSTREAM MEADOWS AREA PLAN

A comprehensive development plan for the Goldstream Meadows area was submitted to the City in 2003. The Goldstream Meadows comprise approximately 117.8 acres of land.

This plan proposes a mixed business park and residential development which would include business park uses as well as townhouse, manufactured and modular housing, together with active recreation opportunities, trails, conservation areas, parks and open spaces.

Attached as Map E-1 is the land use concept plan for the proposed development.

2.0 PLANNING PRINCIPLES AND POLICIES

- 2.1 The basic concept of the Goldstream Meadows neighbourhood includes multi-family townhouse, manufactured and modular housing at densities of between eight and ten units per acre, business park uses, active and passive recreation opportunities, trails, conservation areas, park and open spaces.
- 2.2 The comprehensive development plan is intended to give a conceptual picture of how the lands may be developed. Land use boundaries and numbers are approximate.
- 2.3 Greenways are to be provided and design criteria for all parks, open spaces and greenways are to be provided as a condition of development permit and subdivision.
- 2.4 The proposed parkland may include conservation areas, active recreation uses and passive recreation uses.
- 2.5 Public rights-of-ways or dedications for pedestrian trails shall be provided in the development in accordance with Map E-2.
- 2.6 The development of Goldstream Meadows must be serviced by sanitary sewers.
- 2.7 Storm drainage deposited directly to the lake shall not be permitted. Storm drainage to the glacial kettle located on the site shall not be permitted.
- 2.8 A 25 m right-of-way for West Shore Boulevard shall be protected.
- 2.9 Interim landscaping shall be provided, to the satisfaction of the City Planner; on that part of a comprehensive development site that is intended to be developed for commercial use, and is not immediately developed according to the ultimate landscape plan included within a Development Permit.

3.0 DEVELOPMENT PERMIT AREAS AND GUIDELINES

3.1 Justification

The Goldstream Meadows area includes certain sensitive ecosystems identified by the B Conservation Data Centre. Further, there are many wetland areas and areas which may have potentially significant environmental values which should be protected where possible. The Goldstream Meadows area also includes areas where business park commercial, multi-family residential, manufactured and modular intensive residential development may occur. The City of Langford has designated environmentally sensitive areas, as well as all business park, multi-family residential and all manufactured and modular housing within the Goldstream Meadows area as development permit areas, pursuant to provisions in the *Local Government Act*. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on development within the Goldstream Meadows area such that the form and character of development is of a high quality, and best suited to both surrounding properties and the vision of Langford as expressed in the Langford Plan. The justification for this designation is also to ensure that Council is able to secure the necessary information and is able to establish conditions on development within the Goldstream Meadows area for the purpose of protecting the environment from development.

3.2 Objectives

The objectives of the business park commercial development permit area designation is to ensure that business park development of Goldstream Meadows:

- (a) Is compatible with surrounding land uses;
- (b) Complements the social and environmental goals of this plan; and
- (c) Is constructed to high standards, both material and aesthetic.

The objectives of the multi-family and intensive residential development permit area designation is to ensure that multi-family and intensive residential development of Goldstream Meadows:

- (a) Provides a healthy, safe and liveable environment for residents;
- (b) Minimizes the impact of development on the natural environment;
- (c) Provides for vehicular as well as pedestrian needs in a safe manner;
- (d) Is complementary to surrounding land uses;
- (e) Complement the social and environmental goals of this plan; and
- (f) Is constructed to high standards, both material and aesthetic.

The objectives of the environmentally sensitive development permit area guideline is to ensure that the development of Goldstream Meadows occurs in such a manner that:

- (a) Significant environmentally sensitive areas are preserved and enhanced where possible;
- (b) General habitat and biodiversity values and natural environment regimes are presented and enhanced; and
- (c) Riparian areas are preserved and enhanced, or replaced with more valuable riparian area enhances the natural environment, its ecosystems and biological diversity.

Council's general Development Permit Guidelines for environmental protection, commercial, business park, multi-family and intensive residential form and character apply to this area. In the event of a conflict between guidelines, the Goldstream Meadows guidelines shall prevail.

The Goldstream Meadows Plan provides for business park commercial, multi-family residential, conservation and recreation land uses. Map E-1 (Land Use Concept for Goldstream Meadows) identifies the general layout of open spaces, residential, business park commercial uses.

3.0 ENVIRONMENTAL GUIDELINES

3.1 Riparian Guidelines

- 3.1.1 No land clearing or development may occur within the lands until such time as the riparian areas have been identified, leave strips protected, and a development permit authorizing any alteration of land is granted. The City may grant variances to setbacks to watercourses if the majority of riparian values are retained in accordance with a report from a registered professional engineer or a professional biologist.
- 3.1.2 All natural wetlands are encouraged to remain in their current state unless specific plans are presented and accepted by the City of Langford for the relocation and enhancement of wetland areas. The goal is to have the overall value of riparian areas at a higher level than what they were prior to any site disturbance.
- 3.1.3 The City of Langford may require enhancements on-site may in order to mitigate any impact caused by the development of the lands and to enhance the values of those water bodies.

3.2 Wildlife Habitat and Biodiversity Guidelines

- 3.2.1 Native planting should be used within private landscaping areas and municipal boulevards where appropriate.
- 3.2.2 Veteran trees and snags should be preserved where their presence does not constitute a hazard.
- 3.2.3 Natural features should be protected from infrastructure and the City may grant variances to road widths if such variances assist in preserving environmentally significant features.
- 3.2.4 Sidewalks should meander in order to avoid significant natural features and to add to the neighbourhood aesthetics. Prior to any land clearing or construction, detailed environmental inventories of significant features in areas proposed for development shall be conducted. Site development shall minimize impact on existing sensitive ecosystems. This can be done by either clustering of residential units, varying infrastructure standards, or preserving significant features by way of non-disturbance Section 219 covenants or parkland dedication.

- 3.2.5 Habitat areas shall be provided with connectivity to allow for the natural migration of wildlife.
- 3.2.6 Prior to any land clearing or construction, a sediment and erosion control plan must be provided to the satisfaction of the City Engineer and security to ensure compliance may be required.
- 3.2.7 A wildlife corridor under West Shore Boulevard shall be provided.

4.0 BUSINESS PARK AND COMMERCIAL DESIGN GUIDELINES

All commercial and business park developments shall be developed according to the following design guidelines, and in addition to either the City of Langford's commercial design guidelines (as found in Appendix "G" of this bylaw), or the City of Langford's business park design guidelines (as found in Appendix "C" of this bylaw), depending on the intended use of the property. Where there is a conflict between these guidelines and the general guidelines, these will prevail.

- 4.1 Developments should be designed so as to minimize the impact of lighting and sound to adjoining non-commercial or business park properties. The applicant for a Development Permit may be required to engage qualified professionals to provide a plan for controlling noise and light pollution from the site. The requirement for this plan shall be at the request of either the City Engineer or the City Planner, and shall be provided to the satisfaction of the City Engineer or the City Planner. Any recommendations of such a report may be included as required conditions of any development.
- 4.2 Exterior storage must be enclosed by a concrete panel fence of at least 2 m (6.6 ft) in height and not more than 3.5 m (11.5 ft) in height. Exterior storage may not exceed a height of 3.5 m (11.5 ft).

5.0 INTENSIVE RESIDENTIAL DESIGN GUIDELINES

All intensive and multi-family residential development shall be developed according to the following design guidelines, and in addition to either the City of Langford's intensive residential design guidelines (as found in Appendix "L" of this bylaw) or the City of Langford's multi-family residential design guidelines (as found in Appendix "H" of this bylaw), depending on the intended use of the property. Where there is a conflict between these guidelines and the general guidelines, these will prevail.

5.1 Community Amenities

- 5.1.1 Where practical, amenities, such as gardens, recreational facilities or restaurants, provided as part of an assisted living multi-family residential development, shall have a community focus, and be designed for by residents of the neighbouring community.
- 5.1.2 Community amenities shall be located in prominent locations, incorporating the basic principles of CPTED (Crime Prevention Through Environmental Design).

5.2 **Height and Massing**

5.2.1 New MFR buildings, including assisted living apartment, may be more than one storey higher than the permitted maximum height of surrounding uses, provided that an 8 m (26 ft) buffer, including landscaping to the satisfaction of the City Planner, is provided, and provided that the building is stepped back from the front building plane of the ground storey at each successive storey.

5.3 **Location of Parking**

5.3.1 Parking for multi-storey, multi-family residential development should not be directly adjacent to other residential uses (i.e., manufactured or modular housing, or townhouses) unless a 1.8 m (6 ft) high solid fence within a 2 m (6.6 ft) landscaping strip is provided to give effective screening.

5.4 **Landscaping**

5.4.1 Landscaping should be provided with the objective of:

- (a) Providing landscape screening, where practical to protect the privacy of occupants of adjacent properties;
- (b) Providing low-height vegetation between adjacent driveways to mitigate the visual impact of paved surfaces; and
- (c) Providing some effective screening at the time of planting.

5.4.2 The use of drought resistant plant species is encouraged in all landscaping.

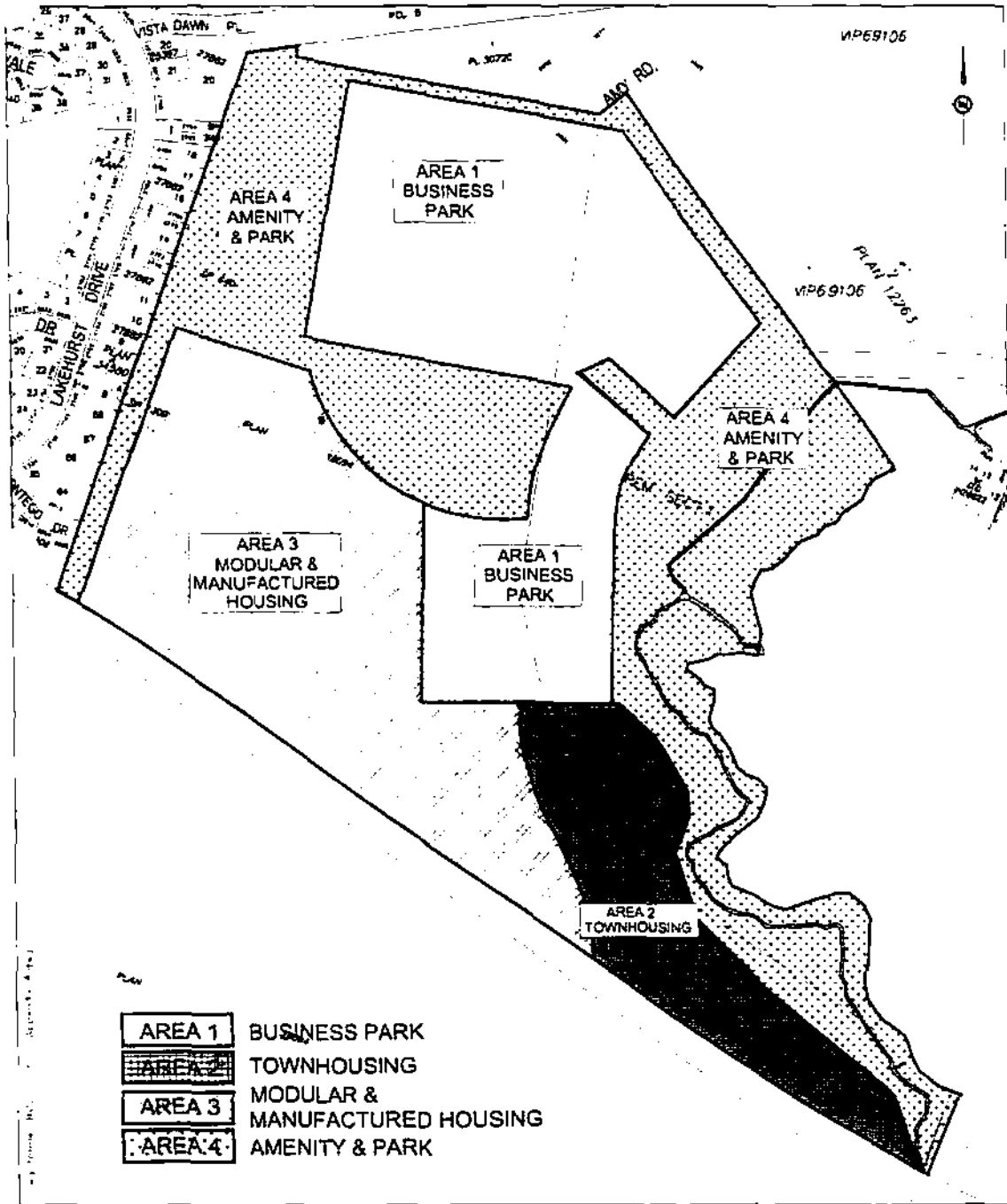
5.4.3 Street trees should be planted along internal drives and in the road frontage or right-of-way (to Bylaw No. 500 standards) after construction of house and driveway.

5.4.4 Development should, where practicable, provide a common green space for use by residents of the development.

5.4.5 A landscaped feature, integrated with signage (if required) shall be provided at every road or driveway entrance to the development permit area, unless the driveway entrance is intended to serve only a single dwelling.

5.4.6 Any common accessory buildings shall be either screened from public view, and only to the extent that screening does not compromise the principles of CPTED (Crime Prevention Through Environmental Design), or enhanced with landscaping that is effective at the time of planting.

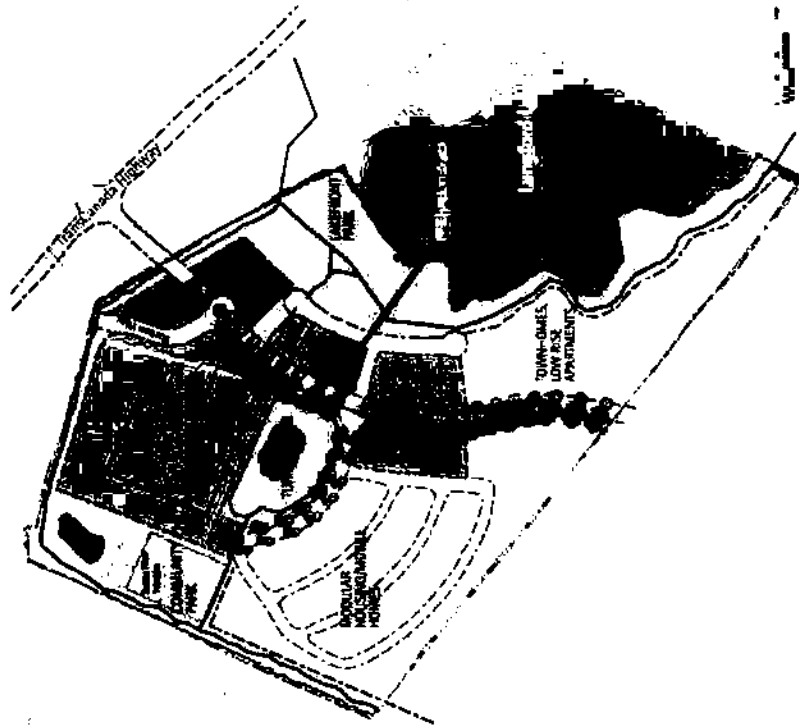
**APPENDIX "A" TO BYLAW No.806
 GOLDSTREAM MEADOWS
 MAP E-1 to BYLAW No.150 - OFFICIAL COMMUNITY PLAN**



Scale: 1:5000

Last Revised AUGUST 11, 2004

Map E-2



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Scale: 1:5000 (1/17" = 100m)

REV: July 13/04
REV: 08/11/04

land use plan CITY OF LANGFORD

Appendix F: General Industrial Development Permit Area Guidelines

Justification

The City of Langford has designated all Industrial land as a Development Permit Area, pursuant to provisions in the Local Government Act. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on developments such that the form and character of new industrial development are of high quality, and best suited to both the surrounding properties and the vision of Langford as expressed in the Langford Plan.

Objective

The objective of this Development Permit area designation is to ensure that new industrial development:

1. is compatible with surrounding land uses.
2. complements the social and environmental goals of this plan.
3. is constructed to high standards, both material and esthetic.

Applicants should provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason should be made. Council may diverge from the guidelines where a compelling rationale which preserves the intent of the guidelines is supplied.

IND1. General Guidelines

All new Industrial developments should:

1. be designed to be compatible with existing development with respect to the siting of buildings, exterior finish, design of buildings, landscaping and design of parking areas;
2. create an environment that is safe, user-friendly, and visually appealing from a pedestrian perspective;
3. provide safe and easily identified access for pedestrians, bicycles and vehicles;
4. ensure that architectural design and building materials are of a high standard in order to ensure a character of development that signifies quality, stability and permanence;
5. provide and/or upgrade sidewalks, pavements, street trees and street lighting;
6. ensure that all unenclosed storage is screened from public roads;
7. provide containers for garbage collection and recycling which will be screened from view, and located in a safe and convenient location on-site; and
8. where building elevations are visible from adjacent roads or properties, ensure that these elevations are finished and treated similarly to the front elevation.

IND2. Integration with the Existing Area

The orientation, scale, form, height, setback, materials and character of new Industrial developments are controlled by development permit areas to ensure compatibility with the surrounding community.

Specifically, these controls include:

1. Height and Massing

- a) The design of new Industrial buildings should respect the scale and height of adjacent development.
- b) In areas of transition, Council will consider the proposed future uses of adjacent properties in assessing the compatibility of proposed developments in neighbourhoods.

2. Lighting

Building and site lighting should:

- a) be sufficient to ensure pedestrian and vehicle safety;
- b) be designed to minimize the illumination of any adjacent residential properties; and
- c) be designed to minimize the affect of lighting on the night sky and adjacent properties, if residential (Outdoor lighting is the main source of light pollution. To minimize this impact, outdoor lighting should be regulated to control the quantity, the quality and the direction of night lighting).

3. Orientation

New Industrial buildings should avoid impeding sunlight penetration and airspace to adjacent properties when those adjacent properties are planned to remain as residential.

4. Storm Water Management

- a) All Industrial developments shall incorporate Stormceptors™, or equivalent approved equipment, to remove oil wastes and sediments from storm water.
- b) Storm water in all Industrial developments shall be managed in accordance with the City of Langford's Storm Water Management Guidelines attached to the Subdivision and Development Servicing Bylaw.

5. Form and Character

- a) Unsightly roof elements, including mechanical equipment and vents should be enclosed, where practically suitable, by roof parapets or other forms of screening.
- b) Site elements such as storage, shipping and loading areas, utility kiosks, transformers and meters, bay doors and garbage receptacles should be screened from adjacent roads.
- c) Unenclosed storage should be sited behind buildings or in the rear of the property, and away from adjacent land uses.

- d) The use of smooth surfaced, light coloured building materials, finished, painted and/or textured "tilt-up" concrete panels, acrylic stucco, glazing, brick, baked enamel finished metal siding or baked enamel metal panels is encouraged.
- e) The use of untreated or unfinished concrete, metal, or aluminium as a final building finish is discouraged.
- f) Building elevations fronting and visible to the street are encouraged to include offices, showrooms and decorative design elements.
- g) Building elevations fronting and visible to the street that include overhead bay doors are discouraged.
- h) All new developments are encouraged to incorporate river rock as a façade treatment and as an element in freestanding signage.
- i) All blank walls (e.g. those walls without fenestration) are to be treated, either with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building, or with a painted mural or other artwork approved by Council, and that a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.

6. Landscaping

- a) Extensive parking areas should be screened by buildings, attractive screens of planting, or low walls.
- b) Landscaping should be provided with the objective of:
 - i. providing screening to protect the privacy of occupants of adjacent properties;
 - ii. providing an effective screen at the time of planting;
 - iii. a minimum 7.5m setback, which may be landscaped, shall be provided to separate Industrial buildings or accessory buildings from adjacent land planned to remain in a single family residential use;
 - iv. providing a landscape feature which is of a type and sufficient maturity to be hardy and resistant to abuse, including vandalism; and
 - v. providing visual diversity to parking areas.
 - vi. incorporating the principles of crime prevention through environmental design (CPTED).
- c) All landscaping should be serviced by an automatic watering system.
- d) The use of plant species which may be considered drought resistant is encouraged in all landscaping.
- e) A continuous landscaping strip of not less than 2.5 m (8 ft) wide should be provided along developed portions of each side of the lot which abuts a highway. This strip may be interrupted by boulevard crossings and pedestrian accesses.
- f) If fencing is provided, it should be on the inside of the landscaping strip.

- g) All adjacent municipal boulevards should be landscaped, irrigated and maintained by adjacent developments.
- h) The provision of space within the landscaping of the site for picnic/lunch areas for staff and customers, and/or play areas (in instances where on-site day-care is provided) is encouraged.

7. Reciprocal Access

In order to provide for efficient and safe vehicular access to properties and between properties, the design of parking areas and vehicle maneuvering aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

IND3. Parking

- 1. New Industrial developments are expected to have accessible parking for visitors and employees which provide convenient access to building entries.
- 2. Parking should be screened from public view and curbed to protect pedestrian paths, building and landscape areas.
- 3. All parking spaces should be delineated with painted lines and finished in a concrete or asphalt surface.
- 4. Secure storage or parking should be provided for bicycles.
- 5. The City Planner may approve Development Permits with variances for parking standards, where the request for such variances are supported by a satisfactory study prepared for the City of Langford by a qualified professional.

IND4. Unenclosed Storage

1. Setback

- 1.1 Unenclosed storage is not permitted in any required front or exterior side yard setback.

2. Parking

- 2.1 Commercial and business park developments are encouraged to identify, at the time of a Development Permit for the principal use building, areas on site that will be used for seasonal unenclosed storage. These areas should be available as space for additional parking when not in use for unenclosed storage.

3. Pedestrian and Vehicular Circulation

- 3.1 Unenclosed storage should not impede either vehicular or pedestrian traffic;
- 3.2 Unenclosed storage should be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and

3.3 Unenclosed storage should not interfere with sight lines for either pedestrian or vehicular traffic.

4. **Screening**

4.1 All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;

4.2 The use of chain link fencing is prohibited; and

4.3 The use of temporary wire fencing is prohibited.

5. **Storage of Combustible or Potentially Hazardous Material**

5.1 Storage areas for toxic, combustible or potentially hazardous material such as liquid petroleum products, fertilizers, herbicides and pesticides must not be sited outside buildings.

6. **Landscaping**

6.1 Unenclosed storage is not permitted within any required landscape screening area; and

6.2 Unenclosed storage is not permitted in any landscape area, unless integrated with the landscaping in a manner that is unobtrusive, does not deteriorate the plantings and landscape material within the landscaped area; and does not interfere with sight lines.

Appendix G: General Commercial Development Permit Area Guidelines

Justification

The City of Langford has designated all Commercial land as a Development Permit Area, pursuant to provisions in the Local Government Act. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on developments such that the form and character of new commercial development are of high quality, and best suited to both surrounding properties and the vision of Langford as expressed in the Langford Plan.

Objective

The objective of this Development Permit area designation is to ensure that new commercial development:

1. is compatible with surrounding land uses.
2. complements the social and environmental goals of this plan.
3. is constructed to high standards, both material and esthetic.

Applicants should provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason should be made. Council may diverge from the guidelines where a compelling rationale that preserves the intent of the guidelines is supplied.

COM1. General Guidelines

All new Commercial developments should:

1. be designed to be compatible with existing development with respect to the siting of buildings, exterior finish, design of buildings, landscaping and design of parking areas;
2. create an environment that is safe, user-friendly, and visually appealing from a pedestrian perspective;
3. provide safe and easily identified access for pedestrians and vehicles;
4. create pedestrian links and public open space;
5. ensure that architectural design and building materials are of a high standard in order to ensure a character of development that signifies quality, stability and permanence;
6. provide full services on streets fronting and flanking the site, including the provision and/or upgrading of sidewalks, pavements, street trees and street lighting;
7. provide containers for garbage collection and recycling which will be screened from view, and located in a safe and convenient location on-site; and
8. where building elevations are visible from adjacent roads or properties, ensure that these elevations are finished and treated similarly to the front elevation.

COM2. Integration with the Existing Area

The orientation, scale, form, height, setback, materials and character of new Commercial developments are controlled by development permit areas to ensure compatibility with the surrounding community.

Specifically, these controls include:

1.0 Height and Massing

- a) The design of new Commercial buildings should respect the scale and height of adjacent development.
- b) New Commercial buildings should not be more than one-storey higher than the permitted maximum height of surrounding existing buildings unless separated by a minimum 8 m (26 ft) buffer.
- c) New Commercial buildings should ensure that the horizontal dimension of transparent ground floor windows, excluding glass door components, does not exceed 60% of the building frontage at street level, avoid continuous façade frontages, and respect the rhythm of the existing streetscape.
- d) Ground floor windows should be a minimum of 0.6 m above the sidewalk elevation.
- e) New Commercial buildings should be "massed" to give the impression of small blocks and create visual interest by providing variations in height and massing.

2. Lighting

Building and site lighting should:

- a) be sufficient to ensure pedestrian and vehicle safety;
- b) be designed to minimize the illumination of any adjacent residential properties; and
- c) be designed to minimize the affect of lighting on the night sky. Outdoor lighting is the main source of light pollution. To minimize this impact, outdoor lighting should be regulated to control the quantity, quality and direction of night lighting.

3. Orientation

New Commercial buildings should:

- a) maintain and enhance existing views;
- b) ensure that developments are pedestrian oriented; and
- c) avoid impeding sunlight penetration and airspace to adjacent properties, when those adjacent properties are residential.

4. Storm Water Management

- a) All Commercial developments shall incorporate Stormceptors™, or equivalent approved equipment, to remove oil wastes and sediments from storm water.
- b) Storm water in all Commercial developments shall be managed in accordance with the City of Langford's Official Storm Water Management Guidelines attached to the Subdivision and Development Servicing Bylaw.

5. Form and Character

- a) The design of new Commercial buildings should utilize variations in the character of rooflines, sloping roof lines, gables and dormers. However, other interesting roof treatments will be considered.
- b) Unsightly roof elements, including mechanical equipment and vents should be enclosed by roof parapets or other forms of solid screening.
- c) Where possible, site elements such as storage, shipping and loading areas, transformers and meters, bay doors and garbage receptacles should be screened from adjacent roads.
- d) The exterior finish of buildings, excluding roof treatments are encouraged to be brick, finished concrete, architecturally faced block, stucco, or wood.
- e) Large expanses of any one material are not acceptable unless effective architectural details are used to break up the visual monotony.
- f) The use of untreated or unfinished concrete, metal, or aluminium as a final building finish is not encouraged.
- g) Multi-level parkades should be integrated within a structure. The exterior façade and site development of these structures should resemble non-parkade buildings typical.
- h) All new developments are encouraged to incorporate river rock as a façade treatment and as an element in freestanding signage.
- i) The City Planner may approve variances to allow signage constructed using tubular neon, provided that the signage is complimentary to the form and character of the commercial building, is in keeping with surrounding commercial development, and does not negatively impact neighbouring residential areas.
- j) All blank walls (e.g. those walls without fenestration) are to be treated, either with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building, or with a painted mural or other artwork approved by Council, and that a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.

6. Landscaping

- a) Landscaping should be provided with the objective of:
 - i. provide screening for privacy and security;
 - ii. providing an effective screen at the time of planting; and
 - iii. a minimum 7.5 m setback, some of which shall include landscaping shall separate Commercial buildings or accessory buildings from adjacent land zoned for single family residential use.
- b) All landscaping shall be serviced by an automatic watering system.
- c) The use of plant species which may be considered drought resistant is encouraged in all landscaping.
- d) All adjacent municipal boulevards should be landscaped, irrigated and maintained by adjacent developments.
- e) Interim landscaping shall be provided, to the satisfaction of the City Planner, on every part of a commercial development site that is not immediately developed according to the ultimate landscape plan included with a Development Permit.

7. Setback

New Commercial buildings should:

- a) be set back from fronting streets to a depth no less than 80% of the setback of buildings on adjoining properties; and
- b) on corner lots, buildings should be set back from both fronting streets to depths equivalent to setback of the buildings on adjoining properties.
- c) Variances for reductions in setbacks may be granted in all commercial zones within Development Permits by the City Planner when the variance being sought supports the objectives of design guidelines.

8. Areas of Transition

In areas of transition, Council will consider the proposed future uses of adjacent properties in assessing the compatibility of proposed developments in neighbourhoods.

9. Safety

Designs for commercial developments are expected to incorporate the basic principles of crime prevention through environmental design (CPTED).

10. Reciprocal Access

In order to provide for efficient and safe vehicular access to commercial properties and between commercial properties, the design of parking areas and vehicle maneuvering aisles shall allow

for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

COM3. Vehicular Circulation

New Commercial development should:

1. ensure safe and convenient access for cars to parking areas;
2. ensure that access for vehicles is separated from pedestrian walkways, provides safe separation distances from nearby road junctions and does not encourage left turns onto or from roads of a collector status or higher where alternatives are available; and
3. ensure that on-site roadways provide safe and convenient access for emergency and service vehicles.

COM4. Parking

1. New Commercial developments are expected to have accessible parking for visitors and employees which provide convenient access to building entries.
2. Parking should be screened from public view and curbed to protect pedestrian paths, building and landscape areas.
3. Underground parking is to be well lighted and provide security measures.
4. Where it is considered to be important to develop and maintain a strong relationship between development, the street and pedestrian activities at the street level, parking between building and street is encouraged to be limited to short-term parking for pick-up and drop-off only, or accessible parking.
5. The City Planner may approve Development Permits with variances for reductions in off-street parking requirements, where the request for such variances is supported by a Parking Demand Study for the proposed use, prepared for the City of Langford by a qualified professional. The Parking Demand Study may consider proximity to bicycle routes and public transit as mitigating factors in determining the demand for off-street parking.
6. The City Planner may approve Development Permits with variances for parking standards, where the request for such variances are supported by a satisfactory study prepared for the City of Langford by a qualified professional.

COM5. Unenclosed Storage

1. **Setback**
 - 1.1 Unenclosed storage is not permitted in any required front or exterior side yard setback.

2. **Parking**

- 2.1 Commercial and business park developments are encouraged to identify, at the time of a Development Permit for the principal use building, areas on site that will be used for seasonal unenclosed storage. These areas should be available as space for additional parking when not in use for unenclosed storage.

3. **Pedestrian and Vehicular Circulation**

- 3.1 Unenclosed storage should not impede either vehicular or pedestrian traffic;
- 3.2 Unenclosed storage should be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and
- 3.3 Unenclosed storage should not interfere with sight lines for either pedestrian or vehicular traffic.

4. **Screening**

- 4.1 All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;
- 4.2 The use of chain link fencing is prohibited; and
- 4.3 The use of temporary wire fencing is prohibited.

5. **Storage of Combustible or Potentially Hazardous Material**

- 5.1 Storage areas for toxic, combustible or potentially hazardous material such as liquid petroleum products, fertilizers, herbicides and pesticides must not be sited outside buildings.

6. **Landscaping**

- 6.1 Unenclosed storage is not permitted within any required landscape screening area; and
- 6.2 Unenclosed storage is not permitted in any landscape area, unless integrated with the landscaping in a manner that is unobtrusive, does not deteriorate the plantings and landscape material within the landscaped area; and does not interfere with sight lines.

Appendix H: General Multi-Family Residential Development Permit Area Guidelines

Justification

The City of Langford has designated all Multi-Family Residential land as a Development Permit Area, pursuant to provisions in the *Local Government Act*. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on developments such that the form and character of new multi-family residential development are of high quality, and best suited to both surrounding properties and the vision of Langford as expressed in the Langford Plan.

Objective

The objective of this Development Permit area designation is to ensure that new Multi-Family Residential development:

1. provides a healthy, safe and livable environment for residents;
2. minimizes the effect on the local environment;
3. provides for vehicular as well as pedestrian needs in a safe manner;
4. is compatible with surrounding land uses.
5. complements the social and environmental goals of this plan.
6. is constructed to high standards, both material and esthetic.

Applicants should provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason should be made. Council may diverge from the guidelines where a compelling rationale which preserves the intent of the guidelines is supplied.

MFR1. Services

All sites should:

1. provide full services on streets fronting and flanking the site, including the provision and/or upgrading of sidewalks and pavements, street trees and street lighting;
2. provide all services by way of underground wiring (electrical, telephone and cable television);
and
3. provide containers for garbage collection and recycling which will be screened from view, and located in a safe and convenient location on-site.

MFR2. Integration with the Existing Area

The orientation, scale, form, height, setback, materials, and character of new multi-family residential developments are controlled by development permit areas to ensure compatibility with existing neighbourhoods and the surrounding community.

Specifically, these controls include:

1. General

The orientation, scale, form, height, setbacks and materials proposed for a MFR development should reflect characteristics consistent with the surrounding context. Sites in older neighbourhoods should be developed in a manner that improves the neighbourhood.

2. Height and Massing

New MFR buildings should:

- a) not be more than one-storey higher than the permitted maximum height of surrounding existing buildings unless they are separated by a minimum 8 m (26 ft) buffer, in which case higher buildings may be acceptable;
- b) create visual interest by providing variations in height, rooflines and massing;
- c) avoid long continuous façade frontage and respect the rhythm of the existing streetscape;
- d) create articulation of building faces with features such as balconies, porticoes, bay windows, and changes in setback at upper storeys; and
- e) respect the scale and height of adjacent houses.

3. Lighting

Building and site lighting should:

- a) be designed to minimize the illumination of adjacent properties; and
- b) be designed to minimize the affect of lighting on the night sky. Outdoor lighting is the main source of light pollution. To minimize this impact, outdoor lighting should be regulated to control the quantity, quality and direction of night lighting.

4. Environmental Impact

- a) New MFR buildings should try to minimize the impact on their environment by:
 - i. siting buildings in such a way that, wherever possible, residential units are sited around, or in harmony with existing natural features such as mature trees and rock outcroppings;
- b) The City will consider variances where the siting of buildings can be shown to lessen environmental impact.

5. Stormwater Management

- a) All MFR developments shall incorporate Stormceptors™, or equivalent approved equipment, to remove oil wastes and sediments from storm water.

- b) Storm water in all MFR developments shall be managed in accordance with the City of Langford's Official Storm Water Management Guidelines attached to the Subdivision and Development Servicing Bylaw.

6. Orientation

New MFR buildings should:

- a) maintain and enhance existing views or vistas from the site;
- b) allow sunlight penetration; and
- c) ensure that units closest to the adjacent street have their façade facing the street.

7. Signage

- a) All signage should be architecturally compatible with the style, composition, materials, colours and details of the buildings within the MFR development as well as the residential buildings of the surrounding neighbourhood.
- b) Wood is the preferred material for signage for MFR developments. Only high-quality, exterior grade wood with suitable finishes should be used.
- c) Developments should ensure that some area is set aside for organized and co-ordinated signage for real estate sales and rentals. This signage should be in keeping with the other provision of this section.
- d) Internally illuminated signs are not permitted. However signs may be illuminated by means of an external light source (i.e.: a small flood light illuminating a wooden sign).
- e) Signs should be mounted so that the method of installation is hidden. Electrical service should also be hidden.
- f) Roof mounted signs are prohibited.
- g) The location and size of signs should be architecturally integrated with the overall design of the buildings and any theme that may be established within the MFR development.
- h) In addition to general signage for MFR developments, individual MFR units will be required to display a house number in accordance with Bylaw No. 91, Langford's House Numbering Bylaw, 1995.

8. Form, Character, and Building Materials

New MFR buildings should be of a residential character by utilizing:

- a) sloping roof lines, variations in the character of rooflines, shielded mechanical equipment, gables and dormers to achieve the effect of a residential building; and
- b) building materials and colours which are compatible with, and enhance surrounding development.

- c) All new developments are encouraged to incorporate river rock as a façade treatment and as an element in freestanding signage.
- d) All blank walls (e.g. those walls without fenestration) are to be treated, either with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building, or with a painted mural or other artwork approved by Council, and that a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.
- e) Pedestrian entries facing the street;
- f) Parking at the rear of the buildings as much as possible;
- g) Staggered entries to create privacy between neighbouring units; and
- h) Shelter from the elements at the front entries.

9. Clustering of Townhouse Developments

- a) All new townhouse developments are encouraged to cluster development on the site such that there are varying numbers of units per townhouse block. The development should be mainly comprised of townhouse blocks with three or more attached units per block. The City Planner may approve developments that incorporate a limited number of townhouse blocks with two attached units or single detached units, provided that the site is adequately serviced.

10. Landscaping

- a) Landscaping should be provided with the objective of:
 - i. providing screening to protect the privacy of occupants of adjacent properties as well as the residents of the multi-family project;
 - ii. providing an effective screen at the time of planting; and
- b) A minimum 7.5m setback which may be landscaped shall be provided to separate MFR buildings or accessory buildings from adjacent land zoned for single family residential use.
- c) The use of plant species which may be considered drought resistant is encouraged in all landscaping.
- d) All landscaping should be serviced by an automatic watering system.

11. Setback

New MFR buildings should:

- a) be set back from fronting streets to a depth no less than 80% of the setback of buildings on adjoining properties; and
- b) on corner lots, buildings should be set back from both fronting streets to depths equivalent to setback of the buildings on adjoining properties.

- c) The City Planner may approve variances for reductions to front, side, and rear lot lines, as part of a Development Permit, and under the following circumstances:
 - i. Where a reduction in a setback or setbacks would improve the relationship between a building and an access route or public road, or allow approved off-street parking, to municipal standards, to be sited between a dwelling and an access route, provided that in no circumstance the setback between a garage door and an access route or street is not less than 5.5 m (18 ft), and the setback between a house and an access route or street is not less than 4.5 m (15 ft);
 - ii. Where a reduction in a setback or setbacks improves the orientation of the dwelling to an access route, or reduces the impact of development on surrounding lands, or avoid sensitive ecosystems or would result in the preservation of trees on site; and
 - iii. Where the City Planner is satisfied that a reduction in a setback does not have significant impacts on adjacent properties (impacts may be mitigated through screening and grade differentiation).
- d) Strata driveways in detached townhouse developments are to be finished to a standard similar to municipal roads in Residential Small Lot zones, and shall include curbs, sidewalks, boulevards, and street trees.

12. Areas of Transition

In areas of transition, Council will consider the proposed future uses of adjacent properties in assessing the compatibility of proposed developments in neighbourhoods.

13. Open Space

MFR developments are expected to provide a certain amount of open space, on-site, which is intended to serve the needs of residents, while at the same time providing an opportunity to enjoy sunlight and views and creating a common focus for each development. This may be accomplished in a number of ways.

- a) Ground-oriented, townhouse-style MFR developments should ensure a minimum area of private outdoor space per unit which is not less than 3 m in width and not less than 10 m² in area. Notwithstanding this guideline, not less than 5 percent of a lot, in a ground-oriented development, shall be developed as useable open space.
- b) In family oriented of MFR developments, outdoor common amenity areas should average more than 3m² for each bedroom with a minimum of 100m² required for all MFR developments.
- c) All common space should connect to public walkways, be separated from both vehicular traffic and parking.
- d) All common space should be situated in an area which allows for sunlight penetration.
- e) All common space should be consolidated in one compact, non-linear and functional area, preferably in a central location and away from the periphery of the site.

- f) Notwithstanding guidelines MF4(a) through MF4(f), outdoor common areas should account for not less than 5 percent of the a lot.
- g) Where play equipment is provided in common areas, the equipment must conform to current safety standards.
- h) The primary private open space should, where feasible, be located on the sunny side of the building, either to the south or to the west.

These guidelines discourage the location of private open space at grade on the front (street) side of a building, due to the potential conflict with the public nature of the street space.

MFR3. Safety and Privacy

1. New MFR developments must provide for the safety and privacy for each residential unit.
2. Entries to each residential unit should be:
 - a) visible to residents;
 - b) made private by staggering or recessing them;
 - c) units adjacent to public roads should have their doors visible and accessible from that road;
 - d) exterior private use areas should be screened to provide for privacy;
 - e) setback from arterial roads should be 12m, to allow for screening and berming;
 - f) designs for multi-family residential developments are expected to incorporate the basic principles of crime prevention through environmental design (CPTED); and
 - g) pedestrian circulation.
3. New MFR developments should:
 - a) be designed to ensure safe and convenient routes for residents;
 - b) provide a trail link between housing clusters within a project;
 - c) provide trail links to the larger neighbourhood in a manner that does not compromise the safety and privacy of the development;
 - d) provide on-site pedestrian circulation which forms a network connecting dwellings to parking and common areas; and
 - e) provide pedestrian pathways that are constructed to a width and tread standard which meet the needs of the user.

MFR4. Indoor Amenity Space

New MFR development should provide a variety of recreational activity rooms, laundry facilities, storage, day care or workshop areas as appropriate to the development.

MFR5. Vehicular Circulation

New MFR development should:

1. ensure safe and convenient access for cars to central parking areas and dwelling units;
2. ensure that access for vehicles is separated from pedestrian walkways, provides safe separation distances from nearby road junctions and does not encourage left turns onto or from roads of a collector status or higher where alternatives are available;
3. ensure that access for vehicles is sited to cause the least disruption to other site uses; and
4. ensure that on-site roadways provide safe and convenient access for emergency vehicles, moving vans and service vehicles.

MFR6. Parking

1. New MFR developments are expected to have accessible parking for residents and visitors which provide convenient access to building entries.
2. Resident and visitor parking should be screened in a manner which limits their view from public roads.
3. Resident and visitor parking should be curbed to protect pedestrian paths, building and landscape areas.
4. Underground parking should be adequately illuminated and provide security measures.
5. If parking for recreational vehicles is to be provided, this should be in low traffic areas, preferably at the back of the development, and screened with natural materials such as cedar trees or hedges.
6. MFR developments are expected to provide safe and secure storage facilities for bicycles.
7. The City Planner may approve Development Permits with variances for reductions in off-street parking requirements, where the request for such variances is supported by a Parking Demand Study for the proposed use, prepared the City of Langford by a qualified professional. The Parking Demand Study may consider proximity to bicycle routes and public transit as mitigating factors in determining the demand for off-street parking.
8. The City Planner may approve variances for reductions in the number of required accessible parking spaces in multi-family residential developments where the building form is that of detached townhouses, and each individual dwelling unit has its own driveway and parking area.

Appendix I: Development Permit Area Guidelines for Two-Family (Duplex) Multi-Family Residential Development

Justification

The City of Langford has designated all Two-Family (Duplex) Multi-Family Residential land as a Development Permit Area, pursuant to provisions in the *Local Government Act*. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on development such that the form and character of two-family (duplex) residential development are of high quality, and best suited to both surrounding properties and the vision of Langford as expressed in the Langford Plan.

Objective

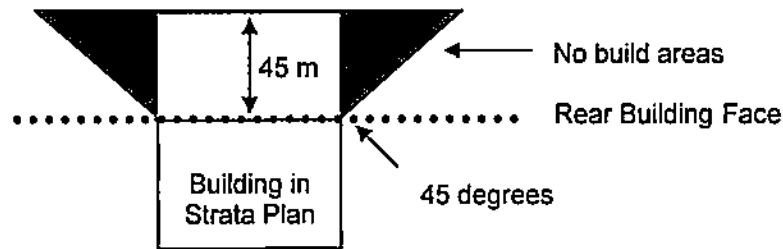
The objective of this Development Permit Area designation is to ensure that new two-family (duplex) development:

1. provides a healthy, safe and liveable environment for residents;
2. minimizes the effect on the local environment;
3. provides for vehicular as well as pedestrian needs in a safe manner;
4. is compatible with surrounding land uses;
5. complements the social and environmental goals of this plan;
6. is constructed to high standards, both material and aesthetic;

Applicants should provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason should be made. Council may diverge from the guidelines where a compelling rationale which preserves the intent of the guidelines is supplied.

MFD1. ORIENTATION

- (a) Developments must ensure that both residential units face the street with a minimum 4.5 m (14.8 ft) habitable area of wall frontage.
- (b) Where two buildings form part of the same building strata plan, and by definition constitute a two-family dwelling, each unit within that building strata plan must be oriented toward the fronting street, and no part of a residential building within a building strata plan may be sited directly behind another residential building in the same building strata plan, as defined as within any part of a rectangular area that extends perpendicularly from the rear building face of a residential building to the rear lot line.
- (c) Where two buildings form part of the same building strata plan, no part of a residential building within that strata plan may be sited within either triangular area bounded by a line extending 45 m (148 ft) perpendicularly from the rear building face of another building within the same building strata plan and a line extending 63.6 m (209 ft) along a 45 degree angle from the rear corner of another building within the same building strata plan, as illustrated in the figure below:



- (d) Notwithstanding subsection MFD1(b), a residential building that forms part of a building strata plan, or any part of that residential building, may be sited directly behind another residential building in the same building strata plan if the distance between the two residential buildings is greater than 45 m (148 ft).
- (e) Where two buildings form part of the same building strata plan, the distance between the two residential buildings must not be less than 6 m (20 ft).
- (f) Where a residential building forms part of a building strata plan on a property greater than 0.8 ha (2 acres) in area, the size of that building's footprint must not exceed 242 m² (2 600 ft²) of gross floor area.
- (g) Where a residential building forms part of a building strata plan on a property of 0.8 ha (2 acres) or less in area, the size of that building must not exceed 186 m² (2 000 ft²) of gross floor area.
- (h) Where two residential buildings form part of the same building strata plan, the gross floor area of one building may not be less than 85% of the gross floor area of the other building within that building strata plan.
- (i) Where two buildings form part of the same building strata plan, no building that forms part of that strata plan may have a width or depth less than 9.7 m (32 ft).
- (j) Where two buildings form part of the same building strata plan, both buildings shall not be identical, but shall exhibit a form and character that is complimentary. Each building should utilize the same or similar materials for siding, roofing and trim elements. The finish and colour of these materials should be complementary, and should not be identical. Buildings should utilize variations in rooflines, fenestration, and façades to establish the individual character of each building, while at all times maintaining a common architectural theme.
- (k) Secondary suites are prohibited in any residential dwelling that form part of a building strata plan.
- (l) Where the design guidelines, contained herein, that pertain to situations where two buildings for part of the same building strata plan are deemed to be in conflict with any other design guideline for two-family (duplex) multi-family residential development, those guidelines for detached duplexes shall supercede the design guidelines for attached duplexes.
- (m) Notwithstanding any other design guideline in this section, where two residential buildings form part of the same building strata plan on a lot of less than 0.8 ha (2 acres), the physical

separation between the two buildings must be sufficient to allow a 3 m (10 ft) wide driveway to be located between the two buildings.

- (n) Where two residential buildings form part of the same building strata plan on a lot of less than 0.8 ha (2 acres), the setback from any interior side lot line may be reduced to 1.2 m (4 ft).
- (o) Notwithstanding any other design guideline in this section, where two residential buildings form part of the same building strata plan on a lot of less than 0.8 ha (2 acres), the front width of any one building may not be less than 85% of the front width of the other building within that strata plan.
- (p) For buildings that form part of the same building strata plan on a lot of less than 0.8 ha (2 acres), any provision of this section that pertains to building width or depth shall not apply.
- (q) Provided that any attached two-family dwelling meets all of the guidelines pertaining to the form and character of two-family residential dwellings contained with this Appendix, the City Planner may, in the interest of good design, approve variances for reductions in lot width, building envelope width or side yard setback.
- (r) All new developments are encouraged to incorporate river rock as a façade treatment and as an element in freestanding signage.

MFD2. SETBACK

- (a) Developments must provide a minimum 3.7 m (12 ft) side yard setback. The exceptions are in the case of corner lots, and when all parking is provided within the building.
- (b) The City Planner may approve variances for reductions to front, side, and rear lot lines, as part of a Development Permit, and under the following circumstances:
 - i. Where a reduction in a setback or setbacks would improve the relationship between a building and an access route or public road, or allow approved off-street parking, to municipal standards, to be sited between a dwelling and an access route, provided that in no circumstance the setback between a garage door and an access route or street is not less than 5.5 m (18 ft), and the setback between a house and an access route or street is not less than 4.5 m (15 ft);
 - ii. Where a reduction in a setback or setbacks improves the orientation of the dwelling to an access route, or reduces the impact of development on surrounding lands, or avoid sensitive ecosystems or would result in the preservation of trees on site; and
 - iii. Where the City Planner is satisfied that a reduction in a setback does not have significant impacts on adjacent properties (impacts may be mitigated through screening and grade differentiation).

MFD3. PARKING

At least three parking stalls must be provided for each dwelling unit and no more than two parking stalls per unit may be located in the front yard.

MFD4. MASSING

Mirror image duplexes are discouraged unless each unit has a significant amount of fenestration and architectural detail.

MFD5. HEIGHT

Buildings are limited to two storeys per dwelling unit.

MFD6. DRIVEWAY

Each dwelling unit should have its own separate driveway.

Appendix J: General Design Guidelines for Downtown Langford

JUSTIFICATION

The City of Langford has designated all of Downtown Langford, as shown shaded in Map 3B, as a Development Permit Area pursuant to the provisions of Section 879(1)(d) of the *Local Government Act*. The justification for this designation is to provide a design framework for the consistent development and redevelopment of the downtown, by establishing specific Design Guidelines. Design Guidelines provide guidance and direction for the conceptual design of structures, site amenities, landscaping and streetscaping. Guidelines provide a design context for site planning, building design and landscape plans. They provide a design context for all new development and set a design standard and image appropriate for the area. These Design Guidelines are intended to provide flexibility to allow for individual diversity but at the same time provide a comprehensive framework to promote a unique and identifiable character for Downtown Langford.

OBJECTIVE

The objective of this Development Permit Area designation is to ensure that all new development in Downtown Langford is consistent with the City's long-term vision for Downtown Langford. Downtown Langford is a special place that plays a critical role in a resident's and visitor's impression of the City. High quality planning and design is essential for Downtown Langford. The relationship of the remainder of Langford to the downtown and adjacent areas is also an important planning and design consideration.

These Design Guidelines encourage consideration of wholeness (that is, an individual development's impact on others around it) and aesthetic attributes while being architecturally and technically sound. They encourage solutions that are complete and specific to the Downtown area, as well as specific sites.

In particular, these Design Guidelines:

- Identify a preferred vision and development concept for the Downtown;
- Stimulate the development of a visually appealing and identifiable place;
- Ensure harmony or compatible design elements within a particular development and between different development areas; and
- Provide clear direction for site design, building character and orientation, signage, site landscaping, and open space.

Applicants should provide a checklist or statement indicating how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reasons should be made. Council may diverge from the guidelines where a convincing or persuasive rationale that preserves the intent of the guidelines is supplied.

D1. GENERAL GUIDELINES

Objectives

- To promote Downtown Langford as a dynamic commercial, retail, office, residential, entertainment and civic function centre;
- To promote high quality building and landscape design;
- To integrate downtown development with surrounding areas;
- To promote a pedestrian environment and pedestrian-scale development in Downtown Langford;
- To encourage redevelopment of Downtown Langford into a more traditional downtown which includes street-oriented commercial and retail activities, traffic calming, cycling, civic uses and on-street parking;
- To recognize the interrelationship of adjacent land uses;
- To encourage residential development in and near the downtown;
- To promote a safe and attractive downtown.

1.1. Site Development

Intent

To promote Downtown Langford as a dynamic and more traditional downtown community and civic centre, which is a focus for specialty retail, office, entertainment, restaurant and public services, and medium density residential uses.

Guidelines

- a) Each development site should be examined to determine its potential impact upon the Downtown.
- b) Where a site is determined to have an effect upon the Downtown's role and function, the design solution for that site should identify clearly how the site integrates with the overall character of the Downtown.
- c) Each development should examine and identify its relationship to pedestrian use, street frontage and public spaces.
- d) All development shall have a street and pedestrian orientation.
- e) Development should have a pedestrian scale.

1.2. Responding to Site Conditions and Context

Intent

To promote high quality site planning which is sensitive to off-site and on-site features such as existing land use and views.

Guidelines

- a) Prior to site design, a site analysis should be undertaken to identify significant on-site and off-site opportunities and constraints that might inform design.
- b) Site Planning and architectural design should also be responsive to built or natural systems surrounding the site in a manner that enhances the overall image of the Downtown.
- c) Views through to treed hillsides should be carefully incorporated into any new development.

1.3. Access, Circulation, and Parking

Intent

To ensure an effective and efficient pedestrian and vehicular circulation system, including bicycles, which enhances the image of Langford and which reduces conflicts between the pedestrian/vehicular realm.

Guidelines

- a) Goldstream Avenue should be redesigned into a 'Main Street' with wide sidewalks, treed street edge, pedestrian crossings, on-street parking and vehicle movement lane.
- b) Pedestrian access to retail and commercial buildings or uses should be continuous along the façade of the building, at the same grade as the sidewalk.
- c) Modulation of building façades at ground level should be used to enable various alternative street activities, including browsing, outdoor cafes, street entertainment.
- d) Buildings should be sited in a manner that provides safe and attractive pedestrian networks that supplement the streetscape network.
- e) Public access to retail and commercial uses on the ground floor should occur directly from the sidewalk.
- f) Corner buildings should be sited in a manner that will be conducive to natural pedestrian movements at ground level.
- g) Shared driveway access between adjacent buildings is encouraged.
- h) Roads in the Downtown should be enhanced with the addition of landscape feature plantings to help define the pedestrian edge.

- i) Local streets should encourage on-street parking to reduce on-site parking requirements and reflect a more intimate character.
- j) The City Planner may approve Development Permits with variances for reductions in off-street parking requirements, where the request for such variances is supported by a Parking Demand Study for the proposed use, prepared for the City of Langford by a qualified professional. The Parking Demand Study may consider proximity to bicycle routes and public transit as mitigating factors in determining the demand for off-street parking.
- k) Public parking and employee or service vehicle access and pedestrian access from streets/sidewalks should be distinct and separate from one another.
- l) Outdoor cafes should be encouraged adjacent to the public realm.
- m) Overhead elements should be provided over portions of the sidewalk for weather protection, to enhance pedestrian scale and modify microclimates. These should be individualized by building.
- n) While maintaining continuity in building façades, a network of mid-block pedestrian alleys should be provided to link adjacent activity areas and backyard parking to front street commercial development.
- o) Sidewalk development should be of a consistent character to reinforce a common design image throughout the Downtown.
- p) In order to provide for efficient and safe vehicular access to commercial properties and between commercial properties, the design of parking areas and vehicle maneuvering aisles shall allow for access to adjoining properties, and this access (to adjoining properties) shall be secured by way of a reciprocal access easement registered on title.

1.4. **Unenclosed Storage**

1.4.1 **Setback**

1.4.1.1 Unenclosed storage is not permitted in any required front or exterior side yard setback.

1.4.2 **Parking**

1.4.2.1 Commercial and business park developments are encouraged to identify, at the time of a Development Permit for the principal use building, areas on site that will be used for seasonal unenclosed storage. These areas should be available as space for additional parking when not in use for unenclosed storage.

1.4.3 **Pedestrian and Vehicular Circulation**

1.4.3.1 Unenclosed storage should not impede either vehicular or pedestrian traffic;

1.4.3.2 Unenclosed storage should be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and

1.4.3.3 Unenclosed storage should not interfere with sight lines for either pedestrian or vehicular traffic.

1.4.4 **Screening**

1.4.4.1 All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;

1.4.4.2 The use of chain link fencing is prohibited; and

1.4.4.3 The use of temporary wire fencing is prohibited.

1.4.5 **Storage of Combustible or Potentially Hazardous Material**

1.4.5.1 Storage areas for toxic, combustible or potentially hazardous material such as liquid petroleum products, fertilizers, herbicides and pesticides must not be sited outside buildings;

1.4.6 **Landscaping**

1.4.6.1 Unenclosed storage is not permitted within any required landscape screening area; and

1.4.6.2 Unenclosed storage is not permitted in any landscape area, unless integrated with the landscaping in a manner that is unobtrusive, does not deteriorate the plantings and landscape material within the landscaped area; and does not interfere with sight lines.

D2. **IMAGE MAKING**

Objectives

- To create signature entrance features at main roads that act as entries to the downtown;
- To encourage a pleasant pedestrian-friendly shopping and work area;
- To promote long term high quality development which is visually appealing; and
- To promote the downtown as a regional entertainment destination.

2.1. **Sense of Entrance and Entrance Feature**

Intent

To create a sense of transition as one enters or leaves the Downtown. This may be achieved through symbolic entrance features or dramatic changes in the streetscape image. At a smaller and site-specific scale, each building or property should have a strongly identified entrance as well.

Guidelines

- a) A special entrance feature consisting of ornamental fencing and manicured landscaping should be created at Station Avenue, Atkins Avenue and Goldstream Avenue.
- b) Civic use located at the corner of Atkins Avenue and Goldstream Avenue should be treated as statement buildings that contribute to the Downtown's entrance image.

- c) Enhanced lighting, both in illumination and quality/quantity of light standards/fixtures, which meets safety standards, should be provided along all downtown streets.
- d) Enhanced landscaping should be provided along all downtown streets.
- e) Site design of corner lots should explore opportunities to provide landmarks and entrance features.

2.2. Civic Uses

Intent

To create an attractive public realm by establishing engaging civic uses in key locations.

Guidelines

- a) Civic uses will be located at key intersections and visually prominent positions.
- b) Civic buildings will be designed to set a high standard for all other Downtown development.
- c) Public art should be encouraged as a means of providing interest, civic identity and community pride.
- d) Public art should be encouraged as a desirable public amenity.
- e) Public art shall be carefully designed to fit the site context and Langford's Downtown Vision.
- f) Public plazas, with suitable public art, will be encouraged at all Civic building locations.

2.3. Pedestrian Environment

Intent

To establish a high quality, pleasant pedestrian area which creates a positive urban image.

Guidelines

- a) Wide sidewalks [minimum of 3 metres (10 feet)] should extend throughout the Downtown.
- b) Pedestrian pathways should connect the Downtown to other parts of the City.
- c) New development in the Downtown shall have a pedestrian orientation rather than a vehicle orientation.
- d) New development should reflect a pedestrian scale by ensuring that buildings do not overwhelm adjacent pedestrian areas.

D3. DEVELOPMENT PATTERN

Objectives

- To establish a high quality downtown area through the use of high quality new development;
- To create visually appealing public spaces;
- To use approved landscaping features to highlight selected interesting commercial development, such as new development adjacent to public space;
- To create a pleasant integrated development pattern which stimulates pedestrian use, reduces land use and circulation conflicts and promotes cohesive development.

3.1. Defining Edges and Connections

Intent

To establish clear distinctive edges to Downtown development areas and between public and private spaces. The streetscape should be well defined in an urban manner using formal standards for both hard and soft landscape materials.

Guidelines

- a) A unified visual language for a characteristic streetscape should be established, including light standards, sign standards, feature site furnishings, pedestrian paving widths/patterns/materials.
- b) Streetscape elements on roads in the Downtown Core Area and Transition Area should be formal and include unit pavers and trees in hard surfaces with planters or tree grates.
- c) Streetscape elements on roads in the Residential, Live/Work, Community Use and Institutional Areas should be softer in character - boulevards should be planted with sod and trees.
- d) Width of sidewalks and rights-of-way should allow for the provision of street trees.
- e) Landscaping should define the edges of development areas while providing continuity between buildings within a development area.
- f) Entry points to the Downtown, particularly the Downtown Core Area, should be clearly defined as visual edges.
- g) Private and public spaces should be identifiable to local residents and visitors alike.

3.2. Visual Quality

Intent

To promote high quality visual images. The streetscape should be designed to ensure a formal and well-maintained appearance that is aesthetically pleasing and provides a unifying green appearance.

Guidelines

- a) All streets should be planted with formal plantings of street trees and, where appropriate, be complemented with low shrub plantings in planters and planting beds.
- b) Streets should be designed to a high quality standard, including formed curbs, in the Live/Work Area, Residential Area, Community Use Area and Institutional Area, and unit pavers in the Commercial Core Area and Mixed-use Transition Area.
- c) Overhead power lines and obtrusive utility boxes shall **not** be permitted for new development in the Downtown Area.
- d) Streetscape design should take into consideration the selection of materials that will ensure a lasting neat appearance, which is easily maintained.
- e) Fencing and planting materials should provide a visually softening effect, while still maintaining sight lines for safety.
- f) Residential uses should be buffered from industrial or public activities.

3.3. Screening Views

Intent

To protect important views and buffer views to parking areas and some commercial uses.

Guidelines

- a) Vertical streetscape elements such as planting or decorative walls and fences should be used to screen adjacent less desirable views.
- b) Landscape elements should be used to screen residential areas from the rear of commercial uses or parking associated with commercial uses.
- c) Commercial waste containers should be screened from public view.
- d) Use of berms, shrub beds, low walls, and low, decorative fences should be considered to screen undesirable views, soften views of expansive architectural features, and to provide visual interest to expansive site features such as parking areas.

3.4. Street Lighting

Intent

To promote safety. On-site lighting should be sufficient to provide clear orientation and personal safety. Additional consideration should be given to enhancing special features or aesthetic qualities.

Guidelines

- a) Lighting shall be provided for all walkways, driveways, parking areas, and loading areas to ensure personal safety and site security.

- b) Metal halide or equivalent white lighting may not be used.
- c) On site lighting shall minimize reflective impact on the night sky by being ground-oriented.
- d) Signage and special architectural or landscape amenities should be enhanced with additional feature lighting.
- e) Lighting fixtures should be concealed where possible or incorporated into the architecture of the building.
- f) Where lamp standards and fixtures are exposed, the aesthetic quality of these elements must be considered to ensure an overall positive image to the development.
- g) Continuous lighting should be provided along all walks and trails.
- h) Lighting design should take into account minimum photometric standards for safety.
- i) Lighting levels should be enhanced to highlight special features, intersections, and passenger loading zones.
- j) The scale of lamp standards and luminare height should relate to both the vehicular roadside scale as well as the sidewalk pedestrian scale.
- k) Street lighting (type and location) should accentuate the Downtown, especially during winter months, by illuminating key landmarks and landscape features.
- l) Decorative lighting should be used throughout the winter months, not only for the holiday season.

3.5. Roads and Parking

Intent

To provide access and parking that is attractive and efficient.

Guidelines

- a) Parking should be internalized, or provided at the back and side of new development.
- b) On street parking shall be permitted throughout all land use areas, except where street widths restrict space availability.
- c) The City Planner may approve Development Permits with variances for reductions in off-street parking requirements, where the request for such variances is supported by a Parking Demand Study for the proposed use, prepared for the City of Langford by a qualified professional.
- d) Shared driveways and parking areas should be encouraged for adjoining developments, to minimize street intersections.
- e) Large parking lots need to be divided into attractive parking areas through the use of landscaping, such as screened land use buffers and planted medians.

- f) Parking lots should be designed to minimize the ponding of surface water.
- g) Special street markings and signage should be considered to enhance identification and use of on-street parking areas.
- h) Parking medians should be planted with trees for shade/wind protection. Consideration should be given to the use of conifers to reinforce the natural environment in Langford;
- i) Lines of sight should be preserved at corners of parking lots.
- j) Planting medians in parking lots should be a minimum of 3 metres wide to support shade tree rooting area.
- k) Parking lots shall be buffered with vegetation while permitting views into and through for safety purposes.
- l) All parking areas shall be paved, drained and appropriately screened.
- m) Where possible, access should be combined and parking should be shared for all land-use areas.

3.6. Streetscape Materials

Intent

To promote use of high quality street and landscaping materials

Guidelines

- a) Streetscape materials should be selected based on the following criteria:
 - Durability and performance; and
 - Aesthetic appeal and timeless quality.
- b) Streetscape materials should be selected which establish a vibrant, high quality image along all streets.
- c) Interlocking pavers shall be used as sidewalk material throughout the downtown.
- d) Street trees shall be used at every opportunity on every street.
- e) Different varieties of trees should be used to add interest and distinguish one street from another.
- f) Paving materials should be used to mark pedestrian areas, set aside parking areas, and make walkways more distinctive from traffic lanes.

3.7. Public Open Space

Intent

To promote appropriate open space areas. Open space should not be thought of as "leftover" space. Rather, the intent of planning for open space should be to enhance the public image, and create meaningful recreational opportunities for the community.

Guidelines

- a) Pedestrian related spaces shall be considered for all open space areas.
- b) All streets should incorporate wide and safe pedestrian/cyclist realms.
- c) Common open space areas may be located at grade or on top of a structure that is integrated into the design of the building.
- d) The Town Centre Pedestrian Commercial Area in particular should have a pedestrian focus.
- e) The Chamber of Commerce site should be designated and designed as a key open space, available for a range of community activities.
- f) Atkins Avenue and the E&N Railway should be developed as attractive open space entrance feature to the downtown.
- g) Open space should be extensively landscaped to enhance the visual, physical and environmental qualities of the downtown.
- h) Pedestrian paths and walks should be clearly delineated through the use of interlocking brick pavers.

D4. LANDSCAPE DESIGN OF SPECIFIC PARCELS

Objectives

- To establish a high quality landscape image which reflects the character of the area;
- To minimize maintenance costs;
- To reinforce the sense of place of the Downtown and the role of the Downtown as an important core commercial, residential and specialty area;
- To promote use of high quality plant materials.

4.1 Landscape Features

Intent

To promote high quality landscape design and create interest at a pedestrian scale.

Guidelines

- a) Pedestrian surfaces should be emphasized by using unit pavers.
- b) Retaining walls and screening walls should be softened with the planting of vines or cascading shrubs.
- c) Chain link fences shall not be permitted.
- d) Wrought iron fence material is encouraged for highlight locations, such as plazas and entrance features.
- e) Entrances to the downtown, on public and private property, shall be of high quality landscape features which convey an image complementary to the Downtown Vision.
- f) Landscape detailing should create an interesting and positive pedestrian realm.
- g) Only low, fences that do not restrict sightlines (for safety purposes) are permitted at the interface of the private/public realm.
- h) Highlight site entries by landscape construction of arbours, archways, or pergolas.
- i) Use architectural elements in the landscape to provide shelter and create focal.
- j) Points.
- k) Street furnishings (lights, bollards, waste receptacles, bicycle stands, tree grates) shall follow an identified palette or kit of parts for the Downtown.
- l) Arbours, archways and pergolas should be used to highlight parking site entries.
- m) Where planters are used they should be of sufficient size to accommodate shrubs and masses of plants.
- n) Where possible, planters should be designed to accommodate seating for pedestrians.
- o) Interim landscaping shall be provided, to the satisfaction of the City Planner, on every part of a development site that is not immediately developed according to the ultimate landscape plan included within a Development Permit.

4.2 Plant Materials

Intent

To promote the use of high quality plant materials. Approved high quality plant material should be selected to reinforce a positive, green image.

Guidelines

- a) Landscape design and development should enhance the overall character, and image of the Downtown.
- b) Plant materials selection should provide visual interest and variety throughout the year.
- c) Plant material, such as the use of specimen Douglas Fir at selected off-street sites, should reflect the character of the Langford locale where possible.
- d) Plant material selection should complement site use and scale of development.
- e) Plant material should provide year round appeal (colour, texture, form) through use of flowering shrubs, perennials, winter twig colour.
- f) Landmark planting should be encouraged at key intersections (Goldstream and Peatt, Goldstream and Bryn Maur, Goldstream and Jacobson) using a style that repeats signature elements at key intersections in Downtown (Goldstream and Millstream, Goldstream and Jacklin).
- g) Plant material should include a mix of deciduous and coniferous species using native plants where feasible.
- h) Plant material should take into account reduced water/maintenance requirements.
- i) Landscape design should provide an interesting mix of canopy and ground cover elements.
- j) Landscape material should be of pedestrian scale.
- k) Sight lines should be preserved for safety through landscape areas.

D5. BUILDING FORM AND CHARACTER

Objective

- To integrate all building and landscape development into a holistic commercial, residential and institutional area;
- To site buildings as part of a larger setting, complementing adjacent uses and buildings;
- To promote use of building materials that have a lasting quality;
- To encourage building form, type and character that is visually appealing, has pedestrian scale and is appropriate for the site and its broader context.

5.1. Architectural Character

Intent

To design and construct new buildings which reinforce Downtown Langford as a traditional downtown that is pedestrian oriented.

Guidelines

- a) The character of the Downtown should be defined as a high quality environment that is distinguished by its organized, but varied façades with superior detailing and signage.
- b) Architectural form shall present modern forms and materials with clean lines.
- c) All primary buildings should be oriented to the street and should have a clear and positive pedestrian orientation (i.e., direct entrances to street).
- d) Architectural components shall be used to differentiate one face of the building from another. The design of structures should be architecturally compatible with other structures through the use of similar and complementary forms, materials and scale.

5.2. Architectural Features

Intent

To design and construct new buildings which have an interesting and appropriate building form.

Guidelines

- a) The character of the Downtown should be defined as a high quality environment that is distinguished by its organized but varied façades with superior detailing and signage.
- b) Architectural features and details should articulate structure forms and modulate façades.
- c) Recesses, overhangs, canopies and sunscreens should be used to articulate the building façades. Monolithic building expression that results in box-like structures with little surficial articulation shall be discouraged.
- d) Individual tenancies shall be defined clearly with articulated entrances and consistent sign treatment.
- e) Repetitive elements, such as entrances, windows and signage should be organized to present modulated façades.
- f) The façades of multi-tenant buildings shall be organized to provide a strong and consistent rhythm to the streetscape. Flat, undifferentiated building faces shall be avoided.
- g) Architectural details and massing shall be expressed so that the base of the structure and its relationship to ground plane activity as well as the roof and its relationship to the skyline is expressed clearly.
- h) All new developments are encouraged to incorporate river rock as a façade treatment and as an element in freestanding signage.

5.3. Façade Treatment

Objective

The future downtown will require careful attention to detail. In particular, building façades must be carefully designed to ensure that they relate in a positive fashion to the pedestrian environment.

5.3.1 Minimum Wall Articulation

Intent

To promote walls that express a variety of three-dimensional forms and prevent the construction of expansive blank walls.

Guidelines

- a) No wall that faces a street or an open area on the same lot (such as a parking lot) shall have a blank, uninterrupted length exceeding 9 metres without including design details such as the following: change in plane, change in texture or masonry pattern, windows, a landscape device such as a trellis with vines or an equivalent element that subdivides the wall in human scale proportions.
- b) Side or rear walls that face walkways may include false windows and door openings defined by frames sills and lintels, or similarly proportioned modulations of the wall, only when actual doors and windows are not feasible because of the nature of the use of the building.
- c) The sides of the building that are not on a property line shall have materials that are similar or complementary to the material on the street side of the building.
- d) Materials should not dramatically change at the corners of buildings unless the corner is on a property line.

5.3.2 Building Façade

Intent

To promote a sense of public presence for all buildings. Attention must be paid to all faces of structures that have a 'public face' (i.e. facing a street). Special attention should be given to the façades of buildings on corner sites where visibility is high.

Guidelines

- a) Façades that face streets or pedestrian frontage shall be subdivided and proportioned using features such as windows, entrances, arcades, arbours, awnings, canopies, and trellises along no less than fifty percent of the façade.

- b) All solid walls of a façade shall have a recognizable "base" consisting of:
 - i. Thicker walls, ledges, or sills;
 - ii. Integrally textured materials such as stone, masonry, or concrete;
 - iii. Integrally coloured and patterned materials such as smooth finished stone or tile;
 - iv. Lighter or darker coloured materials; and
 - v. Planters.
- c) All façades shall have a recognizable top consisting of (but not limited to):
 - i. Three-dimensional cornice treatments that project the top of the building outwards.
 - ii. A sloping roof with a minimum 40 cm overhang in concert with a cornice or a sloping roof with an overhang visually supported by brackets.
 - iii. A parapet that is articulated.
- d) Flat faced canopies or roof overhangs are not permitted unless they are supported by columns and conform to the above.
- e) Sloping Roofs:
 - i. Where sloping roofs are used they shall have a minimum slope of 5 in 12;
 - ii. Canopy roofs on multiple storey buildings shall have a minimum slope of 5 in 12.
- f) All blank walls (e.g. those walls without fenestration) are to be treated, either with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building, or with a painted mural or other artwork approved by Council, and that a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.

5.4. Entrances

Intent

To promote public entrances that are clearly identifiable and accessible.

Guidelines

- a) Public entryways and public interface functions should be designed to express a pedestrian scale.
- b) Primary building entrances shall be clearly defined and shall have a sheltering element such as a canopy awning, arcade, or portico to provide protection from the weather.

- c) The primary entrance to a building shall be located along the front wall of the building unless otherwise required for handicapped access. Secondary entrances such as those for second floor apartments must be visible from a street or alley (for safety).
- d) Public entryways and public interface functions should be used as opportunities to enhance the building image.
- e) Individual entrances in multi-tenant buildings shall have clear identities, such as alcoves, varied doorway materials and varied compatible colours.

5.5. Encroachments

Intent

To encourage articulation of architectural features and provide protection from the elements for pedestrians.

Guidelines

- a) Architectural features, such as bay windows, decorative roofs and canopies and entry features may project up to 1.25 metres into street rights-of-way, provided that they are not less than nine 2.75 metres above the sidewalk.
- b) Trellises, canopies and fabric awnings may encroach 1.8 metres onto the sidewalk.
- c) Canopies and awnings when provided must project a minimum of 1.5 metres from the building.
- d) Awnings less than 1.8 metres in length are permitted to have less depth.

5.6. Roofscapes

Intent

To promote interesting and varied roofs that address weather protection issues.

Guidelines

- a) Long expanses of uninterrupted single-height flat roofs should be avoided.
- b) Roof forms should incorporate elements that create visual interest, such as cornices, gables and dormers.
- c) Functional elements, such as mechanical equipment and roof penetrations, shall be screened or integrated with the roof form in a manner consistent with the overall architecture of the building.
- d) Roof top equipment shall be located to minimize exposure to the street. Parapet height of flat roofs should be set to screen the view of vents and roof-top equipment from neighbouring streets and sites.
- e) Roof forms should reinforce the rhythm of street façades.

5.7. Materials and Colours

Intent

To identify a colour palette for finishes that promotes harmony within and between developments.

Guidelines

- a) Building design shall utilize a variety of materials and finishes used in combination to articulate the building components and differentiate expansive elevations.
- b) Vinyl siding is not an acceptable cladding material.
- c) Building materials shall be compatible with their context, including adjacent structures and surrounding landscape. Materials that might unnecessarily date the overall development or materials used in a non-contextual novelty fashion will be discouraged.
- d) Building colour schemes shall be balanced and shall be compatible with the surrounding landscape and adjacent structures.
- e) Façade colours shall be low reflectance, subtle, neutral or earth tone colours within a medium or moderately dark range of value.
- f) The use of high intensity colours, metallic colours, black or florescent colours shall be prohibited.
- g) Exterior building materials shall not include smooth faced concrete block (triple score acceptable), and smooth-faced tilt up concrete panels, or smooth concrete.
- h) Building trim and accent areas may feature brighter colours, including primary colours.
- i) Accent colours should identify public entrances to buildings.
- j) Exposed concrete (except for normal foundation projection above grade) must be heavily ribbed, textured, coloured, or bush hammered.
- k) All building materials are to be sufficiently durable and shall be built to withstand west coast climate.
- l) Reflective or heavily tinted glass is discouraged, except for detailing.
- m) Wherever possible, colours should be integral to the material and not applied.

5.8. Signage

Intent

To encourage a system of signs that complements the image of the Downtown.

Guidelines

- a) Building signage shall be limited in scale and integrated with the design of the building façades. For single tenant buildings, one corporate I.D. sign will be permitted per building or view plane.
- b) Exterior neon signs may be permitted on commercial buildings in selected areas of high visibility.
- c) Façade mounted signs, projection or overhang signs and awning signs are encouraged.
- d) Building signage should reflect the character of the building function to assist in orientation and character.
- e) Building walls shall not be treated as billboards.
- f) Murals may be placed on building walls if they reflect an appropriate character and image compatible with Downtown redevelopment.
- g) Roof mounted signs are prohibited.
- h) For a multi-tenant building, the use of a sign box that is incorporated into the elevational treatment of the building is encouraged.
- i) Building and tenant identification signs should be organized as distinct architectural elements, reinforcing rhythm and character of the building façades.
- j) All façade signage shall be either externally illuminated sandblasted wood, externally illuminated metal, or of a composite type, illuminated only by exposed tubular neon. The use of internally illuminated fluorescent box signage is strongly discouraged.

D6. AREA SPECIFIC GUIDELINES

6.1. Town Centre Pedestrian Commercial Area

6.1.1 Massing and Building Height

Intent

To design and construct new buildings which have a mass and height appropriate for a pedestrian-scale Commercial Downtown

Guidelines

- a) Buildings shall not exceed four stories in height.
- b) Buildings along Goldstream Avenue should not be less than two stories in height.
- c) Notwithstanding subsection 6.1.1(a), buildings along Peatt Road shall not exceed three stories in height.

- d) Where residential space is proposed over ground floor commercial, the fourth floor shall be stepped back to enhance light penetration to the street and provide variety to building form.
- e) Seventy five percent of the length of the street front façade shall be located no more than 1.5 metres from the property line along Goldstream Avenue.
- f) Avoid long, uninterrupted street façades; use plan articulation for doorways, retail windows, outdoor sitting spaces, etc. and use architectonic devices (articulation of structural elements like columns) to segment longer façades.
- g) Wherever possible, use fixed roof overhangs and canopies to cover at least 1.5 metres of pedestrian walking area along street front façades.
- h) Roof overhangs or canopies must not be more than 3 metres above the walking surface.
- i) Subtle articulation of building walls can be achieved by manipulating window placement in walls, casings and other trim details, resulting in various shadow lines.
- j) More pronounced articulation can be achieved by setting windows and entrances in or out from the wall.

6.1.2. Wall Articulation

Intent

To promote walls that express a variety of three-dimensional forms and prevent the construction of expansive blank walls.

Guidelines

- a) Building Bays shall be a maximum of 9 metres in width.
- b) Visually, architectural features such as columns, pilasters, canopies, reveals, or horizontal offsets shall define bays.

6.1.3. Streetscaping

Intent

To create a very high quality pedestrian environment using quality landscape materials.

Guidelines

- a) Brick pavers shall be used for all sidewalks;
- b) Formal landscaping along Goldstream Avenue shall include trees within tree grates, low shrub beds and use of wrought iron fencing in detailing;

- c) Public plazas should utilize materials, including brick pavers, that are complementary to adjacent public and private space;
- d) Street lighting shall be of a consistent type, colour and quality along Goldstream Avenue.

6.2. Mixed Use Transition Area

6.2.1 Massing and Building Height

Intent

To permit a variety of buildings.

Guidelines

- a) Where residential space is located over ground floor commercial, each additional floor shall be stepped back to enhance light penetration and provide variety to building form.

6.2.2 Streetscaping

Intent

To create a high quality pedestrian environment using quality landscape materials.

Guidelines

- a) Unit pavers shall be used for all sidewalks in the Mixed Use Transition Area.
- b) Formal landscaping shall include trees within grassed boulevards and use of wrought iron fencing in detailing.
- c) Public and private plazas should utilize materials that are complementary to adjacent public and private space.
- d) Where buildings have a setback from the front lot line, significant landscaping will be required.
- e) Street lighting shall be of a consistent type, colour and quality within each Mixed Use Transition Area and should blend with adjacent other Areas.

6.2.3 Setbacks

Intent

To stimulate a strong relationship between commercial buildings and the pedestrian realm.

Guidelines

- a) Varied setbacks will be permitted and will be related to site use.

- b) Building setbacks for commercial uses should be minimized to enhance the pedestrian environment.
- c) Residential development should have a landscaped setback.
- d) Parking is not allowed in the front yard.
- e) That the City Planner be permitted to, in conjunction with Mixed Use Transition Development Permits, issue variances for the front, rear, and side yard setbacks when the following conditions exist:
 - (i) Building height does not exceed the height allowed for the land use zone for the neighbouring property; and
 - (ii) Intensive landscape screening is provided to mitigate impact.
- f) Storeys above the second floor should be set back at various distances from front and exterior side lot lines to accommodate features such as balconies, patios, decks, entrance features and sunlight penetration.

6.3 Residential Area

6.3.1 Massing and Building Height

Intent

To permit a variety of ground oriented housing which maintains a small town scale, distinct from nearby Victoria.

Guidelines

- a) Building with four stories shall be stepped back in the fourth floor to permit light penetration to the street.
- b) Multi-unit buildings shall present the main entrances or an equivalent 'gateway' to the principal frontage street.
- c) Private yards shall be clearly defined from public walkways.
- d) Ground floor units shall be raised above surrounding grade by at least .5 meter to ensure proper overlook of public areas such as sidewalks.
- e) Make available direct outside entry to ground floor units as well as common lobby/hallway entrances.

6.3.2 Streetscaping

Intent

To create a high quality pedestrian environment using quality landscape materials.

Guidelines

- a) Brick pavers shall be used for all sidewalks in the Residential Area.
- b) Formal landscaping shall include trees within grassed boulevards.
- c) Private plazas should utilize materials that are complementary to adjacent public space.
- d) For ground-oriented townhouses, significant landscaping will be required.
- e) Street lighting shall be of a consistent type, colour and quality throughout the Residential Area.

6.4 Live/Work Studio Areas

6.4.1 Massing and Building Height

Intent

To permit a variety of live/work studio space.

Guidelines

Buildings shall not exceed three stories.

6.4.2 Streetscaping

Intent

To create a high quality environment using quality landscape materials.

Guidelines

- a) Brick pavers shall be used for all sidewalks in the Live/Work Studio Areas.
- b) Formal landscaping shall include trees within grassed boulevards.
- c) Private plazas should utilize materials that are complementary to adjacent public space.
- d) Street lighting shall be of a consistent type, colour and quality.

Setbacks

- a) Maximum Building Setbacks should be:
 - i. Front: 3m
 - ii. Side: 2m
 - iii. Rear: 10m

Appendix K: Design Guidelines for Hull's Field

JUSTIFICATION

The City of Langford has designated all Commercial and Multi-family Residential Development as a Development Permit Area pursuant to the provisions of the Local Government Act. The justification of this designation is to ensure that Council has the ability to secure the necessary information and establish conditions on developments such that the form and character of new commercial and multi-family residential development are of a high quality, and best suited to both the surrounding properties and the vision of Langford as expressed in the Langford Plan.

OBJECTIVE

The objective of these Development Permit Guidelines are to supplement the General Development Permit guidelines for Commercial and Multi-Family Residential Development (found in Appendix G and H respectively) specifically for the development of the property commonly known as Hull's Field. These design guidelines are intended to ensure that development is:

1. Constructed to high standards, both material and aesthetic;
2. Complements the social and environmental goals of this plan; and
3. Compatible with other surrounding land uses.

The essential nature of this development is to meld the rural character of the amenity lands at the west end of the site with the proposed residential and commercial uses at the east end of the site. The design of the landscaping and pedestrian systems is an important means to achieving this.

A concept plan showing the areas that are Parcels "A", "B", "C", "D", "E" and "F", may be found at the end of this appendix as Map K1.

When there is a conflict between these guidelines and the general guidelines, these will supercede.

HUL1. Trail Along South Property Line

1. Existing trees should be retained wherever possible;
2. Dying, dead, diseased or hazardous trees may be removed, subject to an arborist's evaluation of the tree's health or situation, and upon approval by the City of Langford of an arborist's report;
3. New trees should be planted to maintain a landscape screen and the transition between residences on Jenkins Avenue, the townhouses in Parcel "B" and "C" and the commercial development in Parcel "A";
4. Commercial buildings should not face the trail but should present an attractive "back" to the community;

5. The character of the residential buildings abutting the park trail should reflect the residential character of the community adjacent on Jenkins Avenue by incorporating the following design elements:
 - a. Maximum two-storey height;
 - b. Pitched roofs;
6. Roads should not adjoin or parallel the south boundary of Parcels "A", or "B", except for the intended connection to Rex Road;
7. Private patios within the amenity land trail in the statutory R.O.W. shall not interfere with the public character of the trail. Strong definition between public and private space should be created using landscape elements such as hedging, trees and shrubs.

HUL2. Jacklin Road

1. Commercial building(s) adjacent to Jacklin Road should enhance the streetscape through high quality building and landscape design. This should be achieved by ensuring that the design of the building interacts visually with Jacklin Road with windows and a variety of surface textures, planes, colours and finishes;
2. Landscaping of an appropriate scale to the buildings should occupy the frontage and act as a visual balance;
3. The pedestrian entrance into the site should be initiated and highlighted with a physical presence on Jacklin Road;
4. Parking areas immediately adjacent to Jacklin Road should be screened by landscaping;
5. Small sitting areas should be created, at periodic intervals, adjacent to commercial uses;
6. A strong "edge" should define the Jacklin Road frontage. This could be in the form of a low stone wall, hedging or a decorative wood fence;
7. The massing and scale of buildings on Jacklin Road should be appropriate to the scale of activity on the street and should define and contain this edge.

HUL3. Residential/Commercial Node

1. The node at the intersection of Parcels "A", "B", and "C" is the focus for the residential community, a play area for children, and a gathering and meeting place for adults. This node should also be the interface between the natural park trail and the more urban esplanade walkway;
2. The area adjacent to the commercial use should be hard surfaced with brick pavers and developed with landscaping, seating and gathering areas;
3. The park trail portion should be developed with walking trails, a pond and tree planting as well as hard surfaced areas.

HUL4. Pedestrian System

1. A trail, to municipal standards, which enhances the existing path should be developed along the south property line. This trail should extend westward through the site to Glen Lake road and the existing trail at the west end of Hull's Field, and also link to Hull's Road and the Residential/Commercial Node;
2. An "Esplanade" should be created to link Jacklin Road to the Residential Commercial node through Parcel "A". This esplanade should be integrated with the landscape system, with a variety of paving textures, occasional benches and lighting;
3. The "Esplanade" should be continuous and linked from Jacklin Road to the Residential/Commercial node and be combined with a "day lighted" storm drainage system wherever practical;
4. Commercial or residential activity, or combined commercial and residential activity should reinforce the pedestrian activity on the walkway system, and in particular on the Esplanade.

HUL5. Parking Lots

1. Landscaping should be introduced into parking areas to visually soften them. Within the parking areas, this landscaping should be located both in narrow strips and in periodic large landscape areas. The intent of the landscape areas is to provide sufficient width (between 15 and 30 cm) to provide space for plantings in sufficient depth to provide "layered screening" (e.g. trees and shrubs);
2. Parking areas should be screened from major arteries and from the residential community on Jenkins Road;
3. At points of intersection between the pedestrian system, roads, and/or parking lots, contrasting paving types and patterns should be used;
4. A reduction in parking standards may be given at the time of Development Permit application if a parking demand study is provided which indicates that the lower standard is sufficient to accommodate on-site parking demand.

HUL6. Integration of Commercial and Residential

1. The residential uses should integrate with commercial uses so that they both enrich and reinforce the "Esplanade" in Parcel "A";
2. Residential uses in Parcel "A" can be positioned either beside or above commercial uses and should be situated directly on the pedestrian system without setback;

HUL7. Storm Drainage

1. A portion of the storm drainage system should be "day lighted" – carried in open swales – and should act as both a visual amenity and a natural filtration system;

2. The "day lighted" system should reinforce and complement the pedestrian system and help highlight the integrated residential/commercial functions. A year-round pond should be developed at a point of emphasis such as the residential-commercial node and the cul-de-sac."

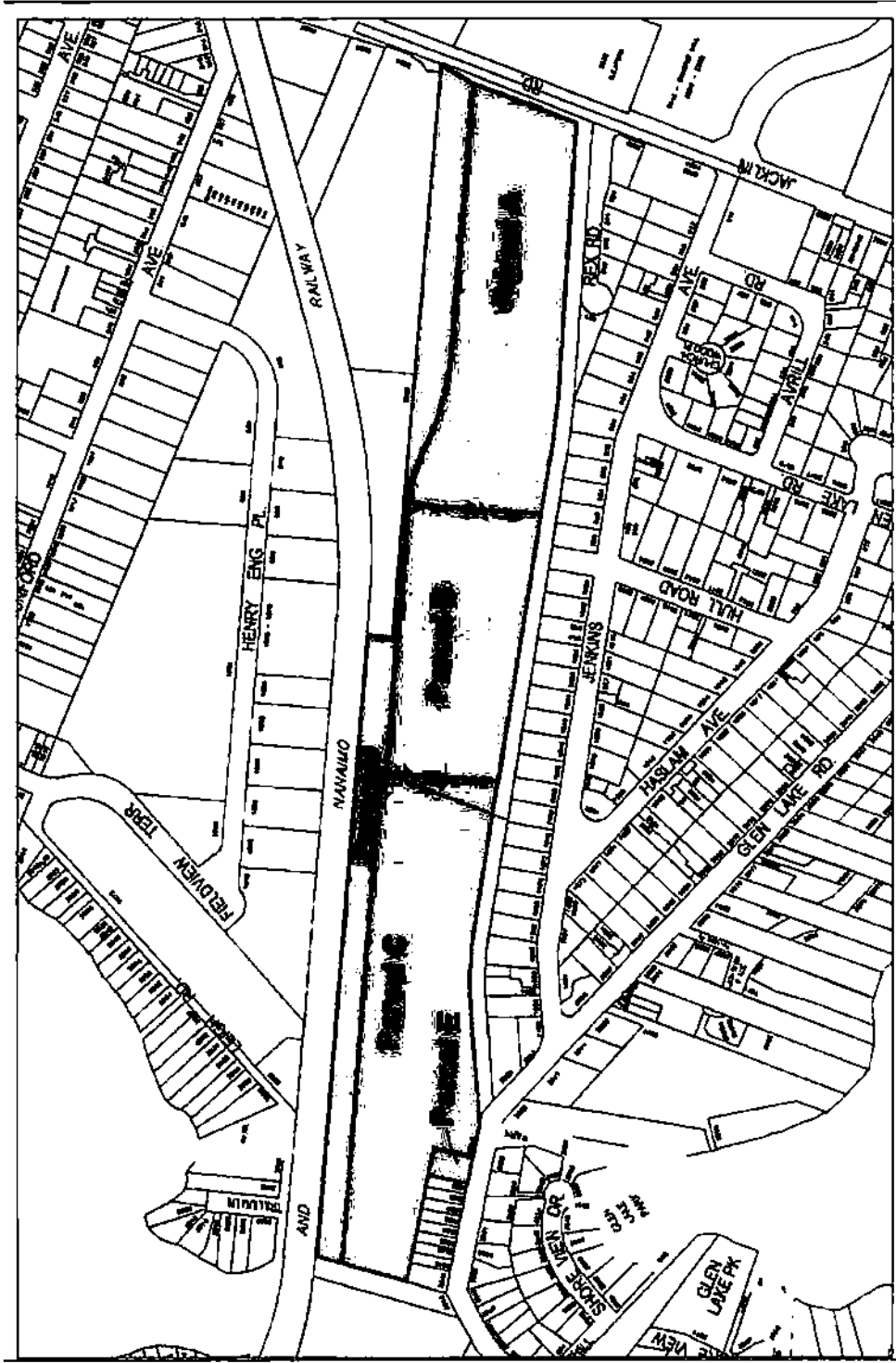
HUL8. Setbacks

1. Landscape screening may be considered in reducing building and parking setbacks;
2. In all parcels which include multiple residential uses, some off-street parking may be located in any required setback from a front or exterior side lot line provided a landscape screen, effective at the time of planting, is installed, if applicable.

HUL9. Interim Landscaping

Interim landscaping shall be provided shall be provided, to the satisfaction of the City Planner, on that part of a comprehensive development site that is intended to be developed for commercial use, and is not immediately developed according to the ultimate landscape plan included within a Development Permit.

APPENDIX K1 -- HULL'S FIELD



Appendix L: Design Guidelines for Intensive Residential Development

Justification

The City of Langford has designated all Residential Small Lot land, and all land within subdivision plans created using the Strata Lot Averaging provisions of the Bare Land Strata Regulation under the *Strata Property Act*, for all lots under 550m², as an Intensive Residential Development Permit Area, pursuant to provisions in the *Local Government Act*. The justification for this designation is to ensure that Council has the ability to secure the necessary information, and establish conditions on developments such that the form and character of new residential development on smaller than average lots are of high quality, and best suited to both surrounding properties and the vision of Langford as expressed in the Langford Plan.

Objective

The objective of this Development Permit area designation is to ensure that new intensive residential development:

1. Provides a healthy, safe and livable environment for residents;
2. Minimizes its impact on the local environment;
3. Provides for vehicular as well as pedestrian needs in a safe manner;
4. Is compatible with surrounding land uses;
5. Complements the social and environmental goals of this plan; and
6. Is constructed to high standards, both materially and esthetically.

Applicants should complete the provided checklist to indicate how their proposal complies with these guidelines. Where some element of the design does not comply with a guideline, a justification stating the divergence and the reason should be made. The City may diverge from the guidelines where a compelling rationale, which preserves the intent of the guidelines, is supplied.

APPLICATION

Development permit applications are required prior to subdivision approval and are to be for subdivisions rather than for individual properties.

R1. INTEGRATION WITH THE EXISTING AREA

The orientation, scale, form, height, setback, materials, and character of new intensive residential developments are controlled in development permit areas to ensure compatibility with existing neighbourhoods and the surrounding community.

Specifically, these controls include:

1. General

The orientation, scale, form, height, and materials proposed for an intensive residential development and/or individual dwellings within an intensive residential development should reflect characteristics

that are consistent with the context of the surrounding neighbourhood. Sites in older neighbourhoods should be developed in a manner that improves the neighbourhood.

The City Planner is authorized to approve Development Permits with variances, where the variance(s) being sought relate to the required setbacks from front, rear, and/or side yard lot lines, **and** where the intent of the variance is to create an improved building envelope, minimize environmental impact, better relationship between buildings within an intensive residential development, or where a setback is adjacent to park land or existing uses where the impact of the variance would be minimal or minimized through screening or a significant change in elevation. Variances may also be approved for pedestrian sidewalk location and width, and for lighting requirements.

2. Height and Massing

New buildings should:

- a) Create visual interest by providing variations in height, rooflines and massing; and
- b) Avoid building plans that are repetitive.

Houses on corner lots shall address each street frontage with an attractive building face, including elements that may project into side yard setbacks. The City Planner may approve variances for projections into setbacks where appropriate.

3. Environmental Impact

- a) New small lot buildings should minimize the impact on their environment, where practical, by siting buildings in such a way that residential units are sited around, or in harmony with existing natural features such as mature trees and rock outcroppings; and
- b) The City Planner may approve variances where the siting of buildings can be shown to lessen environmental impact.

4. Orientation

New intensive residential buildings should:

- a) Maintain and enhance existing views or vistas from the site;
- b) Allow sunlight penetration; and
- c) Ensure that units have their façade facing the street.

If a building is on a corner lot, i.e. having two street frontages, the City Planner may designate which frontage is to be considered the front lot line and may approve variances appropriately.

5. Form, Character, and Building Materials

New small lot buildings should be of a residential character, utilizing:

- a) Variations in the character of rooflines (e.g.: gables and dormers) to achieve design themes (e.g., traditional Victorian or Edwardian single-family residential buildings);
- b) Building materials that are compatible with, and enhance, surrounding development;

- c) Window trim or casings and details on the fascia of the building, such as belt-courses, to enhance visual interest.

Adjacent houses should not be identical in form and character, but should be complementary to each other.

A representative sample of building elevations to be used in the development shall be submitted as part of the application.

6. Storage

- a) Each lot must contain 10m² (100sqft) of space designated as storage space, inclusive of closets. This space may be provided within the house or in an accessory building to the rear of the house.
- b) Storage space contained within the house may be provided as a crawlspace, or alternatively, one half of a double garage may be designated as storage space.
- c) Storage space provided outside of the house may be provided in a shed to the rear of the house. In this case, setbacks from rear and side lot lines may be eliminated to allow the shed to be appropriately sited in the rear yard.

7. Landscaping

- a) Landscaping should be provided with the objective of:
 - i. Providing some screening, where practical, to protect the privacy of occupants of properties adjacent to the intensive residential development;
 - ii. Providing low-height vegetation between adjacent driveways to mitigate the visual impact of paved surfaces; and
 - iii. Providing some effective screening at the time of planting.
- b) Rear yards are encouraged, where practicable, to be level;
- c) The use of plant species which may be considered drought resistant is encouraged in all landscaping;
- d) Street trees should be planted in the road frontage or right of way (to Bylaw 500 standards) after construction of house and driveway (spacing and variety of trees to the approval of the City Planner); and
- e) Small lot residential developments should, where practicable, provide a common green space for use by residents of the development. Funds to create this green space and any equipment associated with it shall be taken from the density bonusing, unless the equipment is gifted to the municipality.

IR2. PARKING

1. New intensive residential developments are required to provide private off-street parking in accordance with the City of Langford's Zoning Bylaw, provided that each lot/unit has three parking spaces.
2. In order to meet the requirement of 3 parking spaces per house, each house in the development should have a 6m (19.5ft) wide driveway, which may accommodate two vehicles. The third parking space may be provided in a garage or in close proximity to the property. The City Planner may vary the parking requirement by allowing one of the three required spaces to be located in the road right of way if it is in close proximity to the lot.
3. New intensive residential developments are expected to provide accessible parking for residents and visitors, which may be provided on-street or in designated common areas. There shall be 1 visitor parking space per four houses, with a minimum of five spaces per development, unless, in the opinion of the Municipal Engineer, adequate parking exists in the area surrounding the development.
4. A plan showing parking spaces provided for each unit and for the development as a whole shall be submitted as part of the application. If, in the opinion of the City Planner, the parking standards described in IR2.1), 2) and 3) are satisfied due to a combination of on- and off-street parking, the specific standards may not need to apply.

EXEMPTIONS

The following are exempted from a development permit application:

1. Residential R1 or R2 subdivision where three or fewer lots are smaller than 550 m² (achieved through lot averaging).
2. Changes to buildings in a small lot development which occur after completion provided the changes are consistent with the design theme.
3. Any aspect of colour on the buildings.
4. Landscaping in private yards (excluding street trees).
5. Residential subdivision or intensive residential development of any land controlled by a Section 219 covenant, approved by the City Planner, and registered in favour of the City of Langford, which controls the form and character of intensive residential development of the subject property.

Appendix M: Bear Mountain Estates Area Plan

1.0 Bear Mountain Estates Area Plan

A comprehensive development plan for the Bear Mountain Estates Area was submitted to the City of Langford in 2001. The Bear Mountain Estates Area comprises approximately 220 ha of land. The Bear Mountain Estates Plan proposes to develop the area shown on attached Map 7 as a new neighbourhood within the City of Langford. The neighbourhood is proposed to include a golf course resort, residential community, commercial, and institutional uses.

In designing this new neighbourhood, the plan policies and development permit guidelines will encourage a development pattern that seeks to preserve and enhance existing landforms, wetlands, and vegetation wherever possible. In addition, comprehensive guidelines to protect the neighbourhood from interface fires will be followed.

A mix of housing styles are proposed and will include attached housing, apartment, condominium, single-family residential, two-family residential, secondary suite, and granny flat units for a total of 2 200 dwelling units (not including suites or granny flats). Higher densities will be associated with the proposed village centre. In addition, a comprehensive system of trails and parks will be dedicated and constructed by the owner of the lands. These parks will include linkage trails between neighbourhoods, a perimeter trail and trails connecting to the City of Langford trail network. There will be a combination of natural open space and developed, passive parklands and tot lots as well as a village green within the village centre.

2.0 Planning Principles and Policies

Development of the Bear Mountain Estates Neighbourhood would be based on the following principles.

- 2.1 Development will include a distinct identity, based on an English Arts and Crafts architectural theme.
- 2.2 The neighbourhood will include a neighbourhood commercial centre, which has a distinct sense of place and opportunities for commercial activities and employment.
- 2.3 The neighbourhood will include a park and trail network that integrates with Langford's Trail Master Plan and Parks and Recreation Master Plan.
- 2.4 A comprehensive network of paths, sidewalks, and trails will link neighbourhoods, parks, open spaces, and commercial areas.
- 2.5 Linkages to existing and future neighbourhoods adjacent to the Bear Mountain Neighbourhood will be provided by way of roads and trails.
- 2.6 Development will be in accordance with strict environmental standards designed to integrate the development with the natural environment.
- 2.7 Environmental features will be respected and protected where possible during development and will be maintained. Development will be allowed to occur within a flexible zoning

scheme; however, it will be regulated in accordance with development permit guidelines for the following purposes:

- 2.7.1 Protection of the natural environment;
 - 2.7.2 Regulation of form and character of intensive residential, multi-family, and commercial development;
 - 2.7.3 Protection of development from hazardous conditions; i.e., wildfire and steep slopes.
- 2.8 Prior to proceeding with development, an overall environmental management plan shall be provided to the City of Langford. Subsequent to acceptance of an overall environmental management plan, each phase of development will require a development permit for form and character, environmental conservation, and protection from hazardous conditions.
- 2.9 Up to 400 single-family residential units (or up to 700 multi-family units) and an 18-hole golf course and clubhouse will be allowed to be developed with access and egress from Millstream Road. Once this development threshold has been achieved, no further development will be allowed until such time as a comprehensive traffic study with road alignments and interchanges with the Trans Canada Highway are provided to the satisfaction of the City of Langford.
- 2.10 The Bear Mountain neighbourhood shall be a complete community with opportunities for working, recreating, and living. The intent is to minimize the requirement for automobile trips outside of the neighbourhood and maximize opportunities for walking and cycling within the neighbourhood for many of the neighbourhood's daily requirements.
- 2.11 The neighbourhood will be subject to the City of Langford's Development Permit Guidelines for Interface Fire Hazards and Steep Slopes.

3.0 Development Permit Areas and Guidelines

In accordance with Section 919.1 of the *Local Government Act*, all of the lands identified within the Bear Mountain Estates Neighbourhood Plan are designated development permit areas for the following purposes.

- 3.1 Protection of the natural environment, its ecosystems and biological diversity.
- 3.2 Protection of development from hazardous conditions.
- 3.3 Establishment of objectives for the form and character of intensive residential developments.
- 3.4 Establishment of objectives for the form and character of commercial and multi-family residential developments.

Council's general Development Permit Guidelines for environmental protection, commercial, and multi-family form and character and interface fire hazard protection apply to this area. In the event of a conflict between guidelines, the Bear Mountain Guidelines shall prevail.

The Bear Mountain Neighbourhood Plan provides for multi-family, institutional, single-family, intensive residential, and commercial land uses. Map 7 (Land Use Concept Plan for Bear Mountain)

identifies the general layout of open spaces, residential, commercial, and golf club uses. Within the residential designated areas, multi-family and intensive residential uses, in addition to single-family uses, may occur. Further, Map 7 will be established as a development permit guideline, which provides overall direction for the layout of land uses. While this layout is not intended to be inflexible, any deviation from the plan must be supported by substantial compliance with relevant development permit guidelines and an amended concept plan demonstrating that the overall layout concept meets the relevant guidelines and bylaws of the City of Langford.

4.0 Environmental Development Permit Guidelines

4.1 Justification

The Bear Mountain area includes certain hazardous conditions (i.e.: steep slopes, interface fire hazard) as well as sensitive ecosystems identified by the BC Conservation Data Centre. Further, there are many wetland areas and areas which may have potentially significant environmental values which should be protected where possible.

4.2 Objectives

The objectives of this designation are intended to ensure redevelopment will be allowed to occur in a manner that:

- 4.2.1 Ensures that development in steep slope areas is designed and engineered to provide a high level of protection from ground instability and/or slope failure;
- 4.2.2 Ensure that development within the interface fire hazard area is managed in a way that minimizes the risk of damage to property or people from interface fire hazards; and managed in a way that mitigates interface fire hazards while still addressing environmental issues.
- 4.2.3 Significant environmentally sensitive areas are preserved and enhanced where possible;
- 4.2.4 General habitat and biodiversity values and natural environment regimes are preserved and enhanced;
- 4.2.5 Riparian values are preserved and enhanced or replaced with more valuable riparian area.

4.3 Steep Slopes

- 4.3.1. When land is altered, due regard shall be given to maintaining the normal drainage system and regulating storm water run-off. Exposed soil on steep slopes subject to erosion shall be re-vegetated or otherwise protected from run-off erosion.
- 4.3.2. Development will not be permitted on hillsides of 30% or greater with poor soil stability or susceptibility unless engineered to resolve the hazard.

4.4 Riparian Guidelines

- 4.4.1 No land clearing or development may occur within the lands until such time as the riparian areas have been identified, leave strips protected, and a development permit issuing any

alteration of land is granted. The City may grant variances to setbacks to watercourses if the majority of riparian values are retained in accordance with a report from a registered professional engineer or a professional biologist.

- 4.4.2 Prior to the development of any golf course, an environmental management plan for the golf course, which ensures that the development and ongoing operation and maintenance of the golf course minimizes incursion and impact into riparian areas and other sensitive ecosystems must be provided to the City of Langford and if required by the City, a Section 219 covenant registered on the lands ensuring that ongoing environmental strategies are adhered to by the owners of the golf course and adjacent properties.
- 4.4.3 All natural wetlands are encouraged to remain in their current state unless specific plans are presented and accepted by the City of Langford for the relocation and enhancement of wetland areas. The goal is to have the overall value of riparian areas at a higher level than what they were prior to any site disturbance.
- 4.4.4 Although there are no fish bearing water bodies located on the Bear Mountain property, the watershed drains into fish bearing water bodies downstream. Accordingly, enhancements on-site may be required by the City in order to mitigate any impact caused by the development of the lands and to enhance the values of those water bodies.

4.5 **Wildlife Habitat and Biodiversity Guidelines**

- 4.5.1 Rocky outcrops should be preserved and integrated into site design where possible.
- 4.5.2 Native planting should be used within private landscaping areas and municipal boulevards where appropriate.
- 4.5.3 Veteran trees and snags should be preserved where their presence does not constitute a hazard.
- 4.5.4 Natural features should be protected from infrastructure and the City may grant variances to road widths if such variances assist in preserving environmentally significant features.
- 4.5.5 Sidewalks should meander in order to avoid significant natural features and to add to the neighbourhood aesthetics. Prior to any land clearing or construction, detailed environmental inventories of significant features in areas proposed for development shall be conducted. Site development shall minimize impact on existing sensitive ecosystems. This can be done by either clustering of residential units, varying infrastructure standards, or preserving significant features by way of non-disturbance Section 219 covenants or parkland dedication.
- 4.5.6 Habitat areas shall be provided with connectivity to allow for the natural migration of wildlife.
- 4.5.7 Prior to any land clearing or construction, a sediment and erosion control plan must be provided to the satisfaction of the Municipal Engineer and security to ensure compliance may be required.

5.0 Form and Character Development Permit Guidelines

5.1 Justification

The City of Langford designates all commercial, multi-family residential, and intensive residential development areas within the Bear Mountain Estates Plan Area as a development permit area for the purposes of controlling form and character of development. Intensive residential is defined as any lot that is less than 550 m² in area. The purpose of this designation is to ensure that Council has the ability to secure the necessary information and establish conditions on development such that the form and character of new commercial, multi-family, and intensive residential developments are of a high quality and are well-suited to the surrounding properties and environment.

5.2 Objectives

The objectives of the form and character development permit guidelines are to ensure that development is constructed to high standards in both material and aesthetic qualities, developed within an English Arts and Crafts architectural theme, and that development is compatible with surrounding land uses. Further, as the goal for the Bear Mountain Plan Area is to have a high quality mix of single- and multi-family and commercial uses, the form and character of the intensive residential, multi-family, and commercial areas must be compatible in design theme and site planning to ensure good integration of the mix of land uses. Multi-storey buildings, over five storeys in height, may depart from the English Arts and Crafts design theme, provided that some elements of English Arts and Crafts style architecture are retained, particularly at the first and second storeys.

To facilitate the quality of development and integration of land uses the following guidelines will be used to supplement the City of Langford's general development permit guidelines for commercial and multi-family development.

6.0 Location and Distribution of Uses

- 6.1 The location and distribution of uses should be generally as illustrated on Map 7.
- 6.2 Residential density will be high around the resort village centre and country club area.
- 6.3 A multi-level residential complex may be developed in the eastern portion of the site.
- 6.4 Within the rest of the residential areas, a mix of attached and detached housing will be provided in a way that minimizes site clearing, road cuts, tree removal, and leaving, where possible, significant topographic and environmental features.
- 6.5 The proposed hotel may be developed in or adjacent to the village centre.

7.0 Village Centre

- 7.1 The Village Centre shall be designed to include and encourage public gatherings.
- 7.2 Specific features should include an amphitheatre, plazas, and landscaped courtyards.
- 7.3 The Village Centre should include landscaped water features and have pedestrian only precincts.

- 7.4 The Village Centre will include a central pedestrian plaza that has a central water feature.
- 7.5 A mix of stand-alone commercial and mixed-use buildings will surround the plaza.
- 7.6 Buildings will be encouraged to have main entrances fronting onto the plaza and, where possible, onto adjacent roads.
- 7.7 Restaurants and cafes will be encouraged to have outdoor sidewalk dining areas.
- 7.8 Interim landscaping shall be provided, to the satisfaction of the City Planner, on that part of a comprehensive development site that is intended to be developed for commercial use, and is not immediately developed according to the ultimate landscape plan included within a Development Permit.

8.0 **Traditional English Arts and Crafts Architectural Guidelines**

The overriding architectural theme of Bear Mountain Estates will be fashioned after an English "Arts and Crafts" style. This will apply to all multi-family residential, intensive residential, and commercial development. Victorian and Edwardian features may be used in adapting the Country theme into the more urban areas of the development. Multi-storey buildings, over five storeys in height, may depart from the "Arts and Crafts" architectural theme, provided that design elements that evoke the "Arts and Crafts" architecture theme are incorporated into the building design. Specific design elements will be as detailed in sections 8.1 to 8.4. Specific design guidelines for the form and character of multi-storey buildings over five storeys in height are as detailed in section 12.0.

8.1 **Finishes**

In addition to glazing and doorways, a minimum of three exterior finishing treatments are required and may include:

- 8.1.1 Stucco;
- 8.1.2 Rock;
- 8.1.3 Wood siding;
- 8.1.4 Concrete composite siding;
- 8.1.5 Wood shingle; or
- 8.1.6 Brick.

No one treatment may comprise more than 65% of any elevation (exclusive of roofing).

8.2 **Roof**

- 8.2.1 Rooflines shall have a minimum 6:12 pitch for major roof elements.
- 8.2.2 Minor roof elements may have a lesser pitch, but must not exceed 30% of the total roof mass.

8.2.3 Hidden gutter features are encouraged.

8.2.4 Roof materials may include:

8.2.4.1 High quality fibreglass;

8.2.4.2 Metal profile;

8.2.4.3 Wood shingle (subject to wildfire interface guidelines);

8.2.4.4 Slate;

8.2.4.5 Concrete composite.

8.3 **Form**

8.3.1 Articulations of building elevations and rooflines are encouraged and are the main architectural element sought by the design guidelines.

8.3.2 Dwelling space inside of articulated rooflines is encouraged.

8.3.3 Use of stone, cultured stone, or brick exterior chimneys is required. Exposed vent pipes for fireplaces or fuel-fired appliances shall only extend vertically from a chimney that is clad in stone, cultured stone, or brick. Chimney enclosures constructed of concrete composite siding, or stucco are not permitted. Lateral vent pipes, or 'through the wall' vents for fireplaces and fuel-fired appliances are not permitted.

8.3.4 Use of fieldstones for accents is encouraged.

8.3.5 Use of over hanging eaves, and large front porches with pillars is encouraged.

8.3.6 Extensive use of dormers (gable, eyebrow, hipped, or shed) is encouraged.

8.4 The images shown as figures M1 to M7 are visual guides showing the style of architecture to be used in all multi-family residential, intensive residential, and commercial development except for the golf clubhouse.



Figure M1



Figure M2



Figure M3



Figure M4

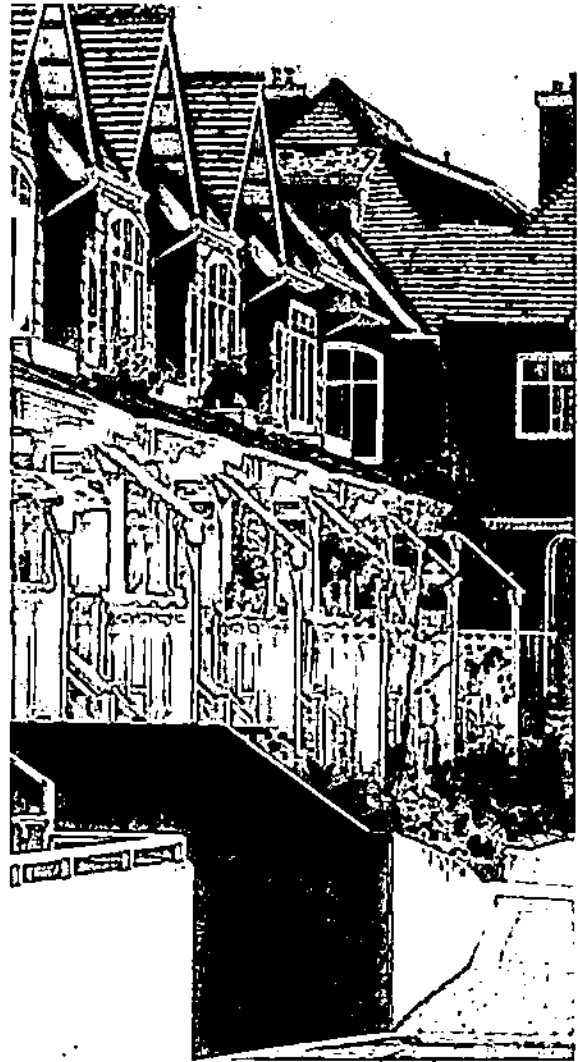


Figure M5



Figure M6



Figure M7

9.0 **Streetscape**

- 9.1 The overall character of the development will reflect the natural environment and the golf course resort.
- 9.2 Individual buildings should complement existing building and the streetscape as a whole.
- 9.3 Buildings will not be identical and will reflect a style identified in the traditional English Arts and Crafts Guidelines. Buildings shall be encouraged to be located close to road frontages and incorporate verandas. Notwithstanding the close proximity to roads, building envelopes will be encouraged to be staggered or building façades will be encouraged to articulate in order to avoid a row housing appearance from the street.
- 9.4 High walls that overshadow neighbouring yards should be avoided.
- 9.5 Strata driveways in detached townhouse developments are to be finished to a standard similar to municipal roads in Residential Small Lot zones, and shall include curbs, sidewalks, boulevards, and street trees.

10.0 **Building Orientation and Siting**

- 10.1 Orientation and siting for any building should meet the following objectives:
 - 10.1.1 Collect summer breezes.
 - 10.1.2 Provide protection from winter wind and rain.
 - 10.1.3 Incorporate natural topographic features and minimize intrusive or radical site works.
 - 10.1.4 Preserve the natural vegetation and significant trees where these are not required to be removed due to wildfire hazard mitigation.
 - 10.1.5 Take advantage of distant and local views.
 - 10.1.6 Provide for maximum privacy.
 - 10.1.7 Allow for some usable yard space.

10.2 **Entrances**

- 10.2.1 Entrances or routes to buildings should be clear and accessible from the street frontage.
- 10.2.2 The front entry of a building should be defined with adequate overhangs for weather protection and wherever possible incorporate verandas.
- 10.2.3 The design of entrances or routes to buildings should not hinder sight lines or surveillance of visitors and intruders.
- 10.2.4 Where possible garage doors should not be the dominant feature of a building's front façade, but should be located to one side, slightly behind, or at right angles to the main street frontage.

10.3 **Parking**

- 10.3.1 Where possible car parking for residential and non-residential lots is to be visually screened from sidewalks and neighbours and prevented from dominating the streetscape.
- 10.3.2 In a village centre or cluster housing developments, parking lots should be broken into smaller areas with landscape dividers or other developments. Ideally, an unbroken parking area should be no larger than about six to eight spaces in a village centre, four spaces in residential areas, and 20 spaces in the clubhouse area.
- 10.3.3 The use of a consistent height of evergreen tree canopy throughout the larger lots is encouraged to help provide weather protection and visual screening from overlooking buildings.
- 10.3.4 Access driveways should be designed to use a minimum of asphalt paving. Permeable surfaces will be considered.
- 10.3.5 Desirable materials include brick, concrete pavers, concrete, exposed aggregate, streetprint, asphalt, or combinations of the above to complement the building material requirements and the natural environment.
- 10.3.6 Estate lots may use gravel or crushed, cemented rock in driveway locations not visible from adjacent roads.
- 10.3.7 Freestanding parkades will not be allowed.
- 10.3.8 Provisions for secure bicycle parking must be made in all commercial and multi-family developments.

10.4 **Fencing**

- 10.4.1 In order to foster a safer more community-oriented environment, fences, and walls in excess of 4 ft in height that extend forward from the front building face are not permitted.
- 10.4.2 If a fence or wall is necessary for privacy or noise control, a partially voided articulated fence or wall in combination with planting and natural materials in keeping with the environment and the building will be considered.
- 10.4.3 Low rock walls and evergreen hedges are encouraged.
- 10.4.4 Chain link fencing is prohibited unless required for safety along trails at the approval of the City Planner.

10.5 **Outdoor Areas**

- 10.5.1 Buildings shall be sited and oriented to provide usable space for entertaining, utilities, storage, play areas, and views.
- 10.5.2 Unsightly storage and utility areas or kiosks shall be screened.

- 10.5.3 Garbage containers in particular, must be enclosed and fully screened.
- 10.5.4 Accessibility from indoor areas, ability to catch sunlight and summer breezes, audio and visual privacy, and safety shall be considered when providing outdoors, private, or semi-private space.
- 10.6 **Energy Efficiency**
- 10.6.1 Site and orientation of buildings should be done in a manner that will increase the energy efficiency and subsequently reduce the cost required for heating, cooling, and lighting.
- 10.6.2 Openings on the west sides of the buildings should be provided to catch the westerly summer breezes and on the opposite side of the buildings to assist in cross ventilation.
- 10.6.3 Higher openings or group roof ventilation designed to expel summer heat is encouraged.

11.0 **Interface Fire Hazard Guidelines**

The development shall be designed in accordance with the City of Langford's Development Permit Guidelines for Interface Fire Hazards. In lieu of an professional engineer's report on Interface Fire Hazards, the City of Langford may accept a registered professional forester's report.

12.0 **Multi-Storey Buildings Over Five Storeys in Height**

The following guidelines shall apply to all multi-storey buildings over five storeys in height.

12.1 **General Form and Character**

Multi-storey buildings over five storeys in height are not required to be developed strictly within the "arts and crafts" architectural design theme of Bear Mountain Estates. In addition to the design guidelines contained within sections 8.1, 8.2, and 8.3, the following guidelines are intended to direct the design of multi-storey buildings over five storeys.

- 12.1.1 The overall architectural design of each building shall consist of heavy articulation, particularly at the main floor, battered foundation walls, dormers, rock or stone clad chimneys, large porches, window boxes and balconies, transomed windows, roofs with wide overhangs, multiple roof planes, trellised porches, columns and piers supporting roof elements and porches, and decorative railings and roof edges.
- 12.1.2 Buildings over five storeys should have a strong relationship to the street and, in particular, a human or pedestrian scale. This should be accomplished by:
 - 12.1.2.1 Including, at the ground level of each building, dwelling units or commercial units with direct pedestrian access to the street;
 - 12.1.2.2 Including covered arcades, towers, trellises, roof overhangs, dormers and chimneys into building design at a human scale;
 - 12.1.2.3 Using extensive stepping and terracing of the elevations and altering the massing of structures so that the heights of various portions of the building are staggered between two and eight storeys;

- 12.1.2.4 Avoiding any massing or single material that rises vertically more than four storeys, unless the façade is punctuated by articulation, terraces, balconies with towers, pergolas or dormers;
- 12.1.2.5 Including in the building façade dormers, shed roofs and balconies that break up vertical and horizontal planes;
- 12.1.2.6 Including vaulted ceilings on top storeys and glazing in gable ends to add verticality to buildings;
- 12.1.2.7 Ensuring that the majority of any eighth storey or any building is built within the roof gable;
- 12.1.2.8 Limiting the access to underground parking to not more than one entrance per road frontage, and by siting driveways and/or garage doors in discreet ways so that they do not front directly onto the street;
- 12.1.3 Buildings over five storeys in height are expected to exhibit an "arts and crafts" architectural theme within the first two storeys, and should exhibit a strong relationship to this architectural theme throughout the first four storeys. At storeys above the fourth storey, buildings should incorporate design elements which relate the architectural character of the "arts and crafts" architecture employed at the lower storeys. Figures M1 to M7 of this Appendix are intended to provide a visual guide to the "arts and crafts" architectural theme;
- 12.1.4 The design of multi-storey buildings over five storeys shall ensure that there is appropriate transition between building levels. The seventh floor may be designed as a horizontal penthouse floor, provided that these dwelling units are multi-storey;
- 12.1.5 Mechanical equipment shall be fully screened within the roofline of any building;
- 12.1.6 Buildings over five storeys in height shall not be used as platforms for radio masts, cell phone towers or other telecommunication device, unless these structures are discreetly located within the roofline of the building;
- 12.1.7 Buildings over five storeys in height may incorporate a single flag pole for one of each of the following: the Canadian Flag, the Province of British Columbia Flag; the City of Langford Flag; and a Bear Mountain Flag.

12.2 Roof and Finishes

- 12.2.1 Roof design shall be as outlined in section 8.2;
- 12.2.2 A variety of colours, materials, textures and forms shall be incorporated into the design of multi-storey buildings over five storeys for the purpose of breaking up the massing of the building, while accentuating building lines and patterns;
- 12.2.3 In addition to the design guidelines contained within sections 8.1, 8.2, and 8.3, exterior finishing treatments may include any combination of smooth, exposed aggregate, scored or patterned concrete as one finishing treatment; and

12.2.4 No one treatment may comprise more than 65% of any elevation (exclusive of roofing).

12.3 **Exemptions**

12.3.1 The following are exempted from a development permit application. Residential subdivision or intensive residential development of any land controlled by a Section 219 covenant, approved by the City Planner, and registered in favour of the City of Langford, which controls the form and character of intensive residential development on the subject property.

13.1 **Multi-Storey Design Guidelines**

In addition to the design guidelines contained with Appendix G and H (if applicable), the following guidelines are intended to direct the design of buildings over five storeys in the Bear Mountain Estates Plan Area.

13.2 The overall architectural design of each building shall feature varied articulation, particularly at the lower storeys, decorative railings and roof edges.

13.3 Buildings over 5 storeys in height should be clustered within the Bear Mountain Estates Plan Area to afford a minimum area occupied by streets and provide generous open space.

13.4 To make buildings over 5 storeys in height "pedestrian friendly", the first two storeys of towers should be strongly articulated through the use of overhangs, signage and other means of stimulating visual interest. Where possible, upper storeys should actually be set back and recede or appear to recede; this can be accomplished by tiering the lower storeys of the tower (i.e., the second and third storeys).

13.5 Balconies should be a minimum of 1 m² in area.

13.6 Units may include their own "earth balcony" capable of supporting planters with shrubs and small trees.

13.7 Each front entrance to ground-oriented units should be visually distinctive from its neighbours, or capable of becoming distinctive from its neighbours.

13.8 Each doorway and entrance should provide a combination of appropriate entrance transitions through the use of alcoves, seating, direction changes, level changes, and variation in materials.

13.9 In areas where multiple towers are constructed or anticipated, a central common area with intensive landscaping and open areas available for barbeques and picnics should be provided. These areas should be linked to the surrounding towers by pedestrian paths accessible to the disabled. The central common area should also be connected by pedestrian paths to other smaller common areas, service commercial amenities, adjacent parks and the greater neighbourhood trail system.

13.10 Terraced building articulations are encouraged; flat roofs and terraced areas should be accessible and "roof-scaped" with landscaping and patio style recreational amenities.









- 13.11 Buildings over five storeys in height should have a strong relationship to the street, and in particular should exhibit a human or pedestrian scale at the ground level. This should be accomplished by:
- 13.11.1 Including, at the ground level of each building, dwelling units or commercial units with direct pedestrian access to the street.
 - 13.11.2 Using extensive stepping and terracing of the elevations and altering the massing of structures so that the heights of various portions of the building are staggered between higher and lower storeys.
 - 13.11.3 Including in the building façade dormers, shed roofs and balconies that break up vertical and horizontal planes where practical; and
 - 13.11.4 Limiting the access to underground parking to not more than one entrance per road frontage, and by siting driveways and garage doors in discreet ways so that they do not front directly onto the street where practical.
- 13.12 Where practical, architectural elements such as intersecting gables and vaulted ceilings should be constructed on top storeys along with glazing in gable ends to add verticality to buildings.
- 13.13 The top storey of any building should be built within the roof gable where practical.
- 13.14 The design of buildings over five storeys in height should ensure that there is appropriate transition between building levels. The top floor may be designed as a penthouse floor provided that the penthouse is integrated into gables or peaked intersecting gables where possible.
- 13.15 Mechanical equipment should be fully screened within the roofline of any building.
- 13.16 Buildings over five storeys in height should not be used as platforms for radio masts, cell phone towers or other telecommunication devices, unless these structures are discreetly located with the roofline of the building or visually obstructed with architectural screening.
- 13.17 Buildings over 5 storeys in height should have a minimum 30 m separation from each other.
- 13.18 Nearby buildings over 5 storeys in height should be positioned in a manner that helps to define street edge definition while at the same time allowing for gaps in views and sunlight penetration to open spaces.
- 13.19 Development of buildings over 5 storeys in height should be positioned in a manner that helps to define street edge definition while at the same time allowing for gaps in views and sunlight penetration to open spaces.
- 13.20 All multi-storey development incorporation buildings with more than 5 storeys should incorporate Crime Prevention Through Environmental Design (CEPTED) design principles.

- 13.21 Semi-private exterior amenity spaces should be made accessible to a variety of user groups including the disabled.
- 13.22 Where possible, buildings over 5 storeys in height should have some setback from the boundaries of the lot to allow for ground level landscaped areas and semi-private outdoor space.
- 13.23 Rooftops that are visible from the windows and balconies of taller buildings should appear attractive and be designed to appear well maintained and attractive over time.
- 13.24 Rooftop mechanical equipment installed on roofs should be fully screened from the windows and balconies of taller buildings.
- 13.25 Where a minimum of 200 residential units are proposed within a single development, buildings within the development may create an architectural identity other than that required by the design guidelines of Appendix , H, and M, provided that the following provisions are met to the satisfaction of the City Planner:
- a) the buildings and landscaping are designed comprehensively with a unified theme;
 - b) the buildings consist of high quality building materials; and
 - c) public art is incorporated into the landscape plan.

Map 7



Legend

-  Detached/Semi-attached Housing
-  Low Rise/Townhouse (max 3 levels)
-  Medium Rise (max 10 levels)
-  High Rise
-  Bear Mt. Village
-  Open Green Space
-  Golf Course
-  Vineyard

concept plan

langford_bc
Subconsultant

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Drawing date: 05.29.06
 rev:

Appendix N: Development Permit Area Guidelines for Interface Fire Hazards

1. JUSTIFICATION

The City of Langford has designated all lands shown as extreme or high hazard on Map N1 as a Development Permit Area pursuant to provisions in the *Local Government Act*. The justification for this designation is to ensure that Council has the ability to regulate development within high and extreme wildfire hazard risk areas in a way that minimizes the risk associated with these hazards.

2. OBJECTIVE

The objective of this Development Permit Area designation is to ensure that development within the interface fire hazard area is:

- a) Managed in a way that minimizes the risk of damage to property or people from interface fire hazards; and
- b) Managed in a way that mitigates interface fire hazards while still addressing environmental issues.

3. DEVELOPMENT PERMIT AREA GUIDELINES FOR INTERFACE FIRE HAZARDS

- a) Within the designated high and extreme risk interface fire hazard areas, Council may, by resolution, issue development permits.
- b) Every application for a development permit shall be accompanied by plans indicating the following:
 - (i) Location of all existing and proposed buildings and structures;
 - (ii) Siting of parking areas and driveways;
 - (iii) Extent and nature of existing and proposed landscaping including details of trees and ground cover; and
 - (iv) The exterior materials of existing and proposed buildings (siding and roofs).
- c) Within the areas identified as high and extreme interface fire hazard areas, the following Development Permit Guidelines apply:

IFH1

For developments that only have one access route, exterior sprinkler systems on dwellings for protection against exposure fires are encouraged.

IFH2

A plan for the expedient removal of all land clearing debris (wood and vegetation) resulting from development must be submitted and complied with prior to the registration of any new subdivision plan. The approving officer may consider accepting security for the removal of

the material within three (3) months of plan approval provided that the materials do not remain on site during high fire risk seasons.

IFH3

For new developments in high or extreme interface fire hazard areas the City Planner may require a report, prepared by a Registered Professional Biologist and a Registered Engineer qualified to assess interface fire hazard and make recommendations to mitigate the hazard to development. The City may accept the recommendations of a Registered Professional Forester in lieu of a Professional Engineer. This report shall provide recommendations for minimizing interface fire hazard in a manner that seeks to preserve, where possible, sensitive ecosystems that may occur in close proximity to development.

IFH4

The report may include recommendations for relaxations to restrictions on exterior building materials and roof sprinklers if resulting development changes the actual level of risk from extreme or high to moderate or low. All buildings within 30 m of a high or extreme wildfire risk area as identified by the Registered Professional Engineer must include fire resistant construction materials for exterior siding and roofing.

IFH5

Within high or extreme wildfire interface areas, a Section 219 covenant may be required, which prohibits any outdoor burning.

IFH6

In order to accommodate development within the high or extreme fire hazard areas, development is encouraged to be clustered and variances and density averaging may be considered in order to accommodate the clustering of residential densities.

IFH7

Because of the potential for interface wildfires to interfere with hydro service to developments, and thus interfere with residential sprinkler systems, all hydro servicing in new developments within high and extreme interface fire hazard areas is encouraged to be underground and is required for developments of four (4) or more lots of urban density (i.e., lots less than 1,000 m²).

IFH8

For new developments in high or extreme interface fire hazard areas, Council and the approving officer may consider requiring the development of a trail system around the developments, which would accommodate fire vehicle access for fighting wildfire in interface areas.

IFH9

In order to ensure the ongoing restriction on wood fuel adjacent to residences the City Planner may require a landscape bond. In lieu of the bond, the City Planner may accept a section 219 covenant requiring property owners to ensure the fuel restriction zone around houses and buildings is maintained and that if they are not maintained, they may be required to pay a rent charge of \$1 000 per year.

IFH10

In designing new subdivisions and neighbourhoods within the high to extreme fire hazard development permit areas, proponents shall consider the incorporation of fire breaks adjacent to residential areas. These may be in the form of cleared parkland, roads, or trails.

IFH11

For areas designated within the interface fire hazard development permit area, a development permit is not required under the following conditions:

1. For buildings or subdivisions that are applied for prior to June 1, 2002 and are not completed prior to December 31, 2002.
2. For minor additions to existing houses, i.e., less than 500 ft² (46.45 m²) or for renovations within existing building.

IFH12

For new developments in high or extreme fire hazard areas the City Planner may require that landscape plans be prepared in consultation with both a Registered Professional Biologist and a Registered Professional Forester, and provide recommendations for ensuring minimal fuel loading within landscaped areas, ongoing protection from interface fire hazard, and the type and density of fire resistive plantings that may be incorporated within landscaped areas to help mitigate the interface fire hazard.

IFH13

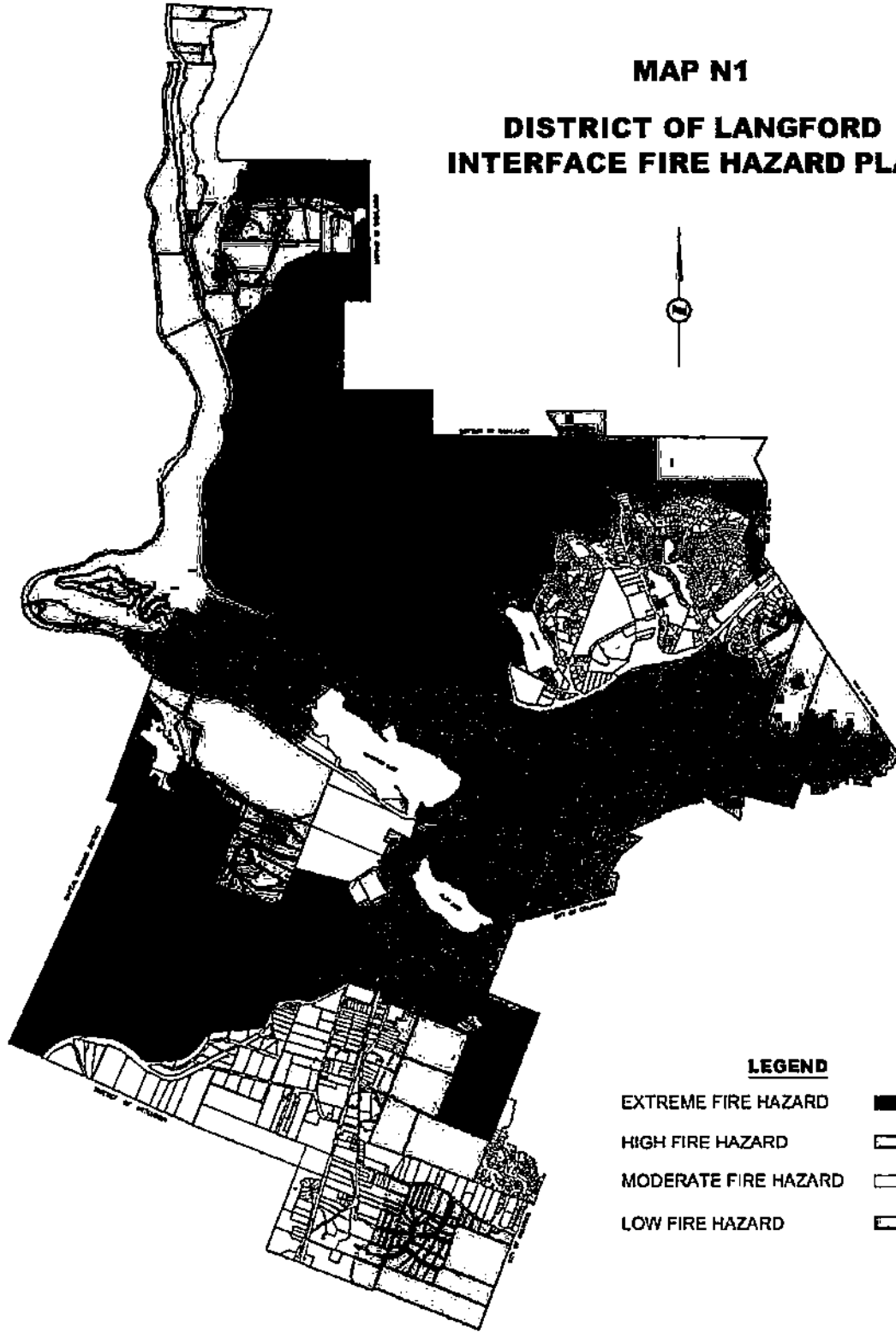
1. New developments should provide adequate fuel removal and reduction zones prior to any subdivision or building permit approval;
2. New developments shall provide fuel removal and fuel reduction zones as necessary for adjacent existing developments where there is not sufficient room available on the adjacent property to establish full fuel removal and reduction areas.

IFH14





1. **Fuel Removal Zone One:** a distance of at least 10 m and, depending on the surrounding environment, possibly up to 20 m from the proposed building and projections. This distance depends upon and increases with the degree of aspect of slope and on the surrounding fire risk.

- a) All trees which pose a fire risk, underbrush, dead limbs and debris piles to be removed;
 - b) Landscape with low combustible vegetation only—plant deciduous trees to provide shade to cool the area around the dwelling and to enhance wildlife habitat;
 - c) Underground automatic irrigation system to be installed and maintained;
 - d) Bark mulch, wooden fencing or coniferous plant species are prohibited; and
 - e) No outdoor storage of building material, landscape debris or combustible materials.
2. **Fuel Reduced Zone Two:** a distance of 20 m to 30 m from the proposed building and projections.
- d) Low tree density to be provided and maintained by removing smaller trees and ensuring that the crowns of trees are separated by a minimum distance of 3 to 6 m;
 - e) Mature trees to be maintained, but limbs to a height of 10 m (30 ft) are to be removed;
 - f) High combustible bush and undergrowth to be removed;
 - g) Only deciduous trees to be planted and maintained and to have a minimum of 3 to 6 m between crowns; and
 - h) Deciduous trees to be planted and maintained to provide shade and enhance wildlife habitat.

MAP N1
DISTRICT OF LANGFORD
INTERFACE FIRE HAZARD PLAN



LEGEND

- EXTREME FIRE HAZARD 
- HIGH FIRE HAZARD 
- MODERATE FIRE HAZARD 
- LOW FIRE HAZARD 

Appendix O: Jacklin/Sooke Road Comprehensive Development Area Design Guidelines

JUSTIFICATION

The City of Langford has designated all areas in which Commercial and Multi-family Residential Development is permitted as a Development Permit Areas pursuant to the provisions of the *Local Government Act*. The justification of this designation is to ensure that Council has the ability to secure the necessary information and establish conditions on developments such that the form and character of new commercial and multi-family residential development are of a high quality, and best suited to both the surrounding properties and the vision of Langford as expressed in the Langford Plan.

OBJECTIVE

The objective of these Development Permit Guidelines is to supplement the General Development Permit guidelines for Commercial and Multi-Family Residential Development (found in Appendix G and H respectively) specifically for the development of the property that is within the Comprehensive Development 7 – Sooke/Jacklin (CD7) Zone. These design guidelines are intended to ensure that development is:

1. Constructed to high standards, both material and aesthetic;
2. Complementary to the social and environmental goals of this plan; and
3. Compatible with other surrounding land uses.

The nature of this development is to create a transition between commercial activities along Sooke Road and single family residential uses to the south by providing opportunities for a comprehensive mix of commercial and multi-family residential activities.

A concept plan showing the areas that are Lot 1 and Lot 2, may be found at the end of this appendix as Map O1.

When there is a conflict between these guidelines and the general guidelines, these will supersede.

O1. SITE ORGANIZATION

Intent

To ensure that the siting and massing of new commercial buildings on this site is sympathetic to the local context.

Guidelines

1. Parking and service areas should be screened from neighbouring properties and the public realm;
2. Parking areas should be broken into smaller groupings and well screened with landscaping;

3. The siting and massing of new commercial buildings on this site should be massed in a manner that is sympathetic to the local context. Buildings over four stories are not considered appropriate;
4. Garbage, deliveries, loading, and other building utility areas should be screened from public view with enclosures or landscaping; and
5. If buildings are being considered for intrusion into landscape zones, they must be considered carefully to ensure screening from adjacent properties.

O2. ENVIRONMENT

Intent

To ensure that development occurs in a coordinated manner that considers existing site conditions.

Guidelines

1. Existing site conditions, such as grades should be fully investigated prior to considering landscape treatments;
2. Existing vegetation and drainage patterns should be accommodated, where possible and practical; and
3. Re-grading with new walls and terracing should shall be done in concert with existing grades so that wall heights are minimized.

O3. SIGHT LINES

Intent

To ensure that the natural slope of the land is taken into consideration when designing buildings and their massing, and in particular that the impact of elevational differences between commercial and residential uses is considered.

- 1.0 All rooftop mechanical equipment shall be fully screened from view from adjacent buildings and public areas.

O4. SITE LIGHTING

Intent

To provide clear orientation, enhance personal safety, special site features or aesthetic qualities, and ensure that site lighting is contained on-site, and does not illuminate adjoining properties or the night sky.

Guidelines

1. Lighting shall be provided for all walkways, driveways, parking areas, and loading areas to ensure personal safety and site security. Lighting levels should be enhanced to highlight special features, drive aisles, intersections, and passenger loading zones;
2. Metal halide or equivalent white lighting may not be used;
3. On site lighting shall be fully downcast, and designed to minimize reflective impact on the night sky and to eliminate light pollution and glare to adjoining properties;
4. Lighting fixtures should be concealed where possible or incorporated into the architecture of the building. In particular, lighting fixtures should be of a type which shields the bulb from view, when viewed from adjoining properties or adjacent streets;
5. Signage and special architectural or landscape amenities should be enhanced with additional feature lighting;
6. Where lamp standards and fixtures are exposed, the aesthetic quality of these elements must be considered to ensure an overall positive image to the development;
7. Continuous lighting should be provided along all pedestrian paths and walkways;
8. Lighting design should take into account minimum photometric standards for safety;
9. The scale of lamp standards and luminare height should relate to both the vehicular roadside scale as well as the sidewalk pedestrian scale; and
10. Decorative lighting is encouraged throughout the winter months, not only for the holiday season.

O5. HARD AND SOFT LANDSCAPING

Intent

To ensure that all hard and soft landscaping is of a high quality and/or constructed to a high material and aesthetic standard.

Guidelines

1. General

- a) Private land should be clearly delineated from public road right-of-way using by using different types of landscape materials.

2. Shrub and Groundcover Planting

- a) Planting schemes should be based on natural looking or native plants with reduced water and maintenance requirements wherever possible. No artificial turf shall be permitted.

3. Street Trees

- a) Where boulevard street trees are to be planted on private land, the species of trees shall be selected from City of Langford's approved urban street tree list; and
- b) A double row of street trees shall be planted along any internal roadway or driveway that serves and separates commercial uses from residential uses to buffer and screen the commercial site down slope and to the north of the proposed Jacklin Road housing development site.

4. Sidewalks

- a) Clear, delineation between vehicles and pedestrian zones is important and necessary for successful site design. The pedestrian circulation areas (sidewalks) should be well defined.

5. Retaining Walls

- a) Retaining walls that are visible within the public realm should be carefully designed with aesthetics and structural soundness considered. The style, materials, and colour should be compatible with the project design approach.
- b) Changes in grade from the public to private realm should be carefully landscaped to have low sections or terraces of walls no greater than 2 metres in height per wall. Retaining walls are to be constructed with textural finish and are to be planted with trailing shrubs or vines to soften their overall appearance.
- c) Retaining walls are to be constructed along Road "A" and Jacklin Road, as indicated on the plan. The Jacklin Road wall profile shall conform to the above guidelines in appearance and be approximately one metre in height. The wall construction shall undulate and vary with shrub foundation planting to break up its overall length.

06. SIGNAGE

Intent

To enhance safety by providing clear orientation, whether from a pedestrian or vehicular oriented perspective, and to recognize the importance of this site as an "entrance" to the City of Langford, and the Sooke Road commercial area.

Guidelines

At the intersection of Sooke Road and Jacklin Road an entry point will be marked with a "Welcome to Langford" sign on the south southwest quadrant. This sign feature will be further emphasized with bench seating, textured pavement, pedestrian scale lighting and flowering trees and shrubs in planting beds.

Appendix P: Westhills Green Community Master Plan

EXECUTIVE SUMMARY

The Westhills Green Community located in the City of Langford, British Columbia will set a new standard for sustainable neighbourhood development in Canada. The Master Plan calls for the 210 hectare (517 acre) Westhills comprehensive development to meet Leadership in energy and Environmental Design (LEED) certification standards for neighbourhood development. Major guiding factors of the design and implementation of Westhills are considerations of location, alternative transportation modes, environmental preservation, community agriculture, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. These characteristics are demonstrated in the creation of approximately 5,950 new residential units, supporting commercial, civic and educational uses, with approximately 84 ha (207 acres) of the subject property being designated as park and open space (equivalent to 40% of the total land area).

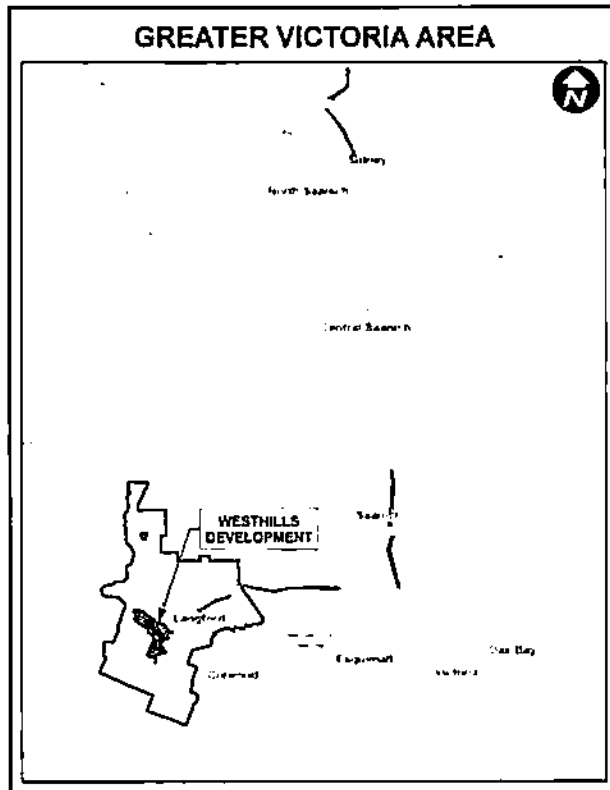
A draft plan for the Westhills Green Community was devised through a charette process involving a variety of stakeholders and consultants including representatives from the University of British Columbia Design Centre for Sustainability, the BC Ministry of Community Services, Canada Green Building Council and the City of Langford. Through this planning process the City of Langford has been able to form a cohesive partnership with the owners of Westhills Land Corporation, with a commitment to excellence in environmental design.

A significant amendment to the plan was made in 2007 resulting from negotiations with the School District on the location and layout of a school site, the addition of approximately 17 ha (44 acres) of land to the plan and an additional 950 dwelling units. Also, additional sustainability aspects were added to the concept plan including: trail network, bike network, use of energy efficient street lighting, solar powered traffic lights, community agriculture plan, a requirement for all single-family and townhouse buildings to comply with "Built Green™", the possibility of geothermal and hydroelectric energy generation, and the use of porous concrete pavers throughout the village centre roads and sidewalks.

In addition to the new plan elements, the vision includes buildings that will accommodate both live and work spaces, a village centre, neighbourhood retail service centres, an internal commuter bus service, and a commuter rail station. High quality public spaces, mixed uses, cultural and educational facilities and residences will provide for a community available to all ages, and income levels. By valuing ecological processes, and following LEED standards, the Westhills Green Community will be a vibrant addition to the City of Langford and set a national precedent for socially and environmentally sustainable design.

1.0 INTRODUCTION

The City of Langford is a rapidly growing community of 22,000 people in the Westshore area of the Capital Regional District in British Columbia. The Regional District's Growth Strategy calls for approximately half of the growth in the Region to occur in the Westshore, predominantly in Langford and Colwood.



Map 1.1
Map showing the relative location of the Westhills
Plan Area relative to Greater Victoria

The entire 210 hectare (517 acre) Westhills site is located within the Capital Regional District's Regional Urban Containment and Servicing Policy Area as designated in the Regional Growth Strategy. Adopted in 2003, this strategy contains eight strategic initiatives that form the bases for a 25-year plan for growth management in the Greater Victoria Region.

The Westhills Master Plan addresses all eight strategic initiatives as described below:

- **Keep Urban Settlement Compact.** Approximately 75% of the residential units within Westhills will be contained within clustered medium and high density multi-family developments.
- **Protect the Integrity of Rural Communities.** All of the Westhills development is located within the designated Regional Urban Containment and Servicing Policy Area and not in designated rural residential or renewable lands.

- **Protect Regional Green and Blue Space.** Approximately 40% of the Westhills site is designated as green space, while areas of protected forest will provide wildlife corridors and largely maintain natural views from across Langford Lake.
- **Manage Natural Resources and the Environment Sustainability.** The Westhills Master Plan calls for 100% of the commercial and high density multi-family development to meet LEED criteria for environmental sustainability. All single-family and townhouse development will comply with or exceed "Built Green™" construction standards.
- **Build Complete Communities.** Westhills will be a truly comprehensive community containing a mixed use core area providing retail and service amenities within walking distance to residences.

- **Improve Housing Affordability.** In addition to supplying the region's housing market with much needed new homes, 150 rental units will be provided within the Westhills Master Plan as an affordable housing measure. Further, all single-family lots 550 m² in size or larger shall be built with secondary suites and small lots may include suites.
- **Increase Transportation Choice.** The backbone of the Westhills' transportation network will be a commuter rail link to the region's employment centre in Victoria. Westhills will also have an integrated shuttle bus, transit, pedestrian and bicycle trail system between residential areas and the nearby mixed use core area.
- **Strengthen the Regional Economy.** Westhills will provide workplaces for hundreds of local residents upon completion of the multi-use service commercial core area which may include a learning commons.

The land comprising the plan area has been under development application in the past; however, the City of Langford has been encouraging the development of a single comprehensive plan for the area as opposed to the piecemeal approaches previously proposed. Moreover, the City wishes to ensure that the lands are developed to the highest environmental standard and in the most sustainable manner. To achieve this, efforts were focused on working with the Canada Green Building Council to design the plan as the first LEED for Neighbourhood Development (LEED ND) neighbourhood demonstration project in Canada. LEED, which stands for Leadership in Energy and Environmental Design, evaluates developments on a broad range of criteria that deal with environmental and social issues.

To achieve this goal the City of Langford has worked with the land owner in partnership with the Canada Green Buildings Council and the Ministry of Community Services and have collectively assembled a team of multi-disciplinary professionals to prepare the plan desired by the City. The team included:

City of Langford staff	Planners
Ministry of Community Services staff	LEED consultants
Quantity Surveyor	Architects
Property owner's representatives	Landscape architects
Professional biologist	Land developer contractor
Civil engineer	Smart growth consultants
Economist/rapid transit specialist	Transportation engineer
Professional forester	

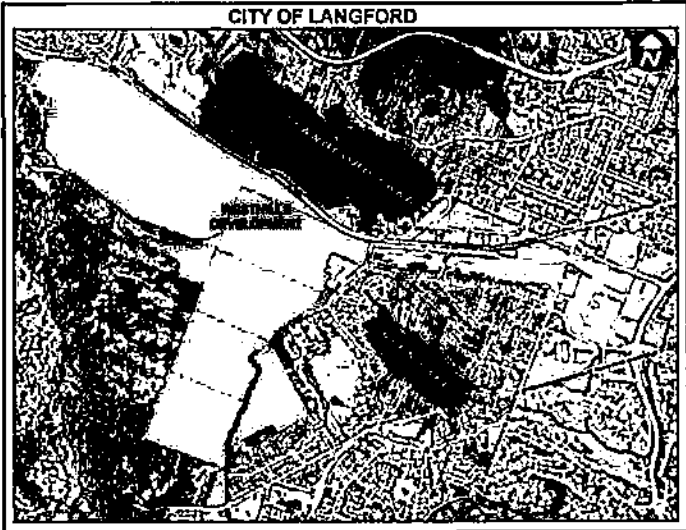
The plan is inspired and guided by LEED principles, and is written with the LEED criteria embedded in its policies. In addition to following the LEED model, the plan also includes a form based code for all multi-family and commercial development. In other words, zoning controls for multi-family and commercial development are limited to land use and density. While the siting, massing, height and other form and character controls are integrated into the development permit process.

1.1 Study Area

The City of Langford's Official Community Plan identified the plan area for Westhills as a future neighbourhood. The land comprises approximately 210 hectares (517 acres) of undeveloped Greenfield land. It includes a variety of terrain including wetlands, streams, ponds, hills, forest and

rocky outcrop areas. It is situated adjacent to existing neighbourhoods of Glen Lake, Raven's View, and the City core area. To the north across the E and N rail corridor is Langford Lake and a developing business park and residential area.

The ownership of the Rail corridor has recently been transferred to a group called the Island Corridor Foundation. The mandate of the foundation is to preserve the rail corridor and to



encourage the operation of freight and commuter rail service. At present there is no commuter rail or light rapid transit system in the Capital Region. Transit Service is provided by busses on a highway system that is significantly limited by topography and water bodies. This system is also becoming increasingly congested and will need expensive upgrades/expansions to accommodate the region's growth. Ultimately, expanded road systems only tend to facilitate additional costly low density suburban and rural sprawl and in a few years, previous congestion levels return.

Map 1.2
Westhills Green Community Location Map (Not to Scale)

1.2 Process

The planning process included the following stages:

A. Pre-Public Activities

A multi-disciplinary review was conducted on the Draft LEED ND criteria prepared by the United States Green Building Council (USGBC) which is the CaGBC's sister organization. These draft criteria were reviewed by the planning team and adapted to better suit the Canadian context. This process preserved the intent of the USGBC's draft guidelines while making changes that reflect the primarily Greenfield nature of local development. These draft guidelines outline pre-requisites and credits that are used as a measuring tool to evaluate the environmental performance of a development and award LEED certification. All pre-requisites must be met for a project to be certified. Projects can pick and choose which credits they will achieve and are awarded points according to their adherence to credit requirements.

The criteria are divided into five main categories: Location Efficiency, Environmental Preservation, Compact, Complete and Connected Neighbourhoods, Innovative Design and Resource Efficiency. Depending on the number of points awarded, a project will be rated as "Certified", "Silver", "Gold", or "Platinum".

A Charette design process was conducted with the team members and facilitated by the Design Centre for Sustainability (DCS) at UBC ("Smart Growth on the Ground" group). The design exercise relied on a statement of project goals and the LEED ND criteria to guide design decisions. The design workshop produced a conceptual plan based on the discussion, drawing, and general agreement on the many planning issues related to "green" site development. The DCS is an academic leader in applying sustainability concepts to the development of land, cities, and communities. A charette is a means for applying leading-edge approaches to developing community and can ensure cross fertilization between researchers and professionals.

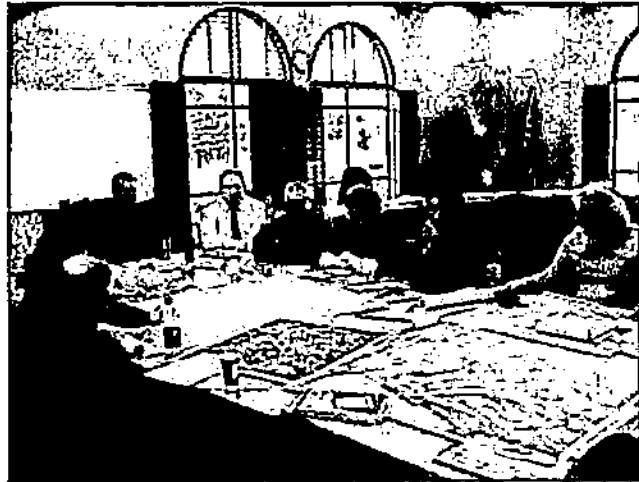


Fig 1.1
LEED Design Charrette held at City Hall

Following the preparation of a conceptual plan, a rigorous analysis of topographic and environmental limitations and service requirements was conducted in order to turn the conceptual plan into a more detailed land use site plan that could be physically achieved. This analysis and the product went through several iterations as input from the project's team members was received and addressed. An important part of the analysis of the proposal included an assessment of how the proposal rated against the draft LEED ND criteria. The results indicate that if all parts of the proposed development are achieved, a gold or platinum rating would be given under the proposed LEED criteria. The detailed site plan received the following assessment from the Design Centre for Sustainability at the University of British Columbia:

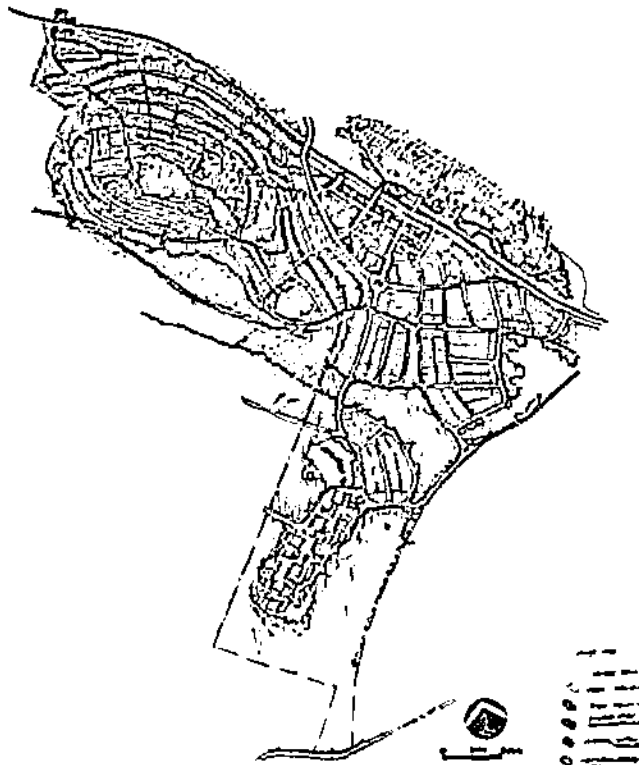


Fig 1.2
Westhills Conceptual Plan created in the initial design Charrette

"We are pleased to note that the project team has clearly abided by the design instructions embedded in the concept plan. The Layout of the neighbourhoods is consistent with the concept

plan, and achieves the balance between environmental, social and economic parameters that the charrette team worked so hard to achieve. We commend the team on its continuing adherence to green design principles.... The Design Centre for Sustainability at UBC has been very pleased to help define a process and product for the Westhills project that transcends the normal approach to neighbourhood design and development."

Elisa Campbell, Director

B. Public Engagement

Once the proposal had been prepared to the point that there was confidence in the technical aspects of the plan, the fast plan was brought to the public in the following ways:

- i. A large public open house was held to introduce the proposal to the public along with the team members and project partners. This open house included a significant educational component on LEED and the Canada Green Building Council. Over 3000 invitations went out to homes in the surrounding residential areas and newspaper advertisements were placed inviting attendance to the open house. Approximately 200 residents attended and of them, 93 completed questionnaires the night of the meeting. The results of the questionnaire showed strong support for the plan and the green/LEED approach.
- ii. Following the open house, a website was launched for the purpose of further engaging and informing the public as well as a means to receive further input on the plan. The public was informed of the website through community newspaper advertisements.
- iii. In addition to these venues, the City of Langford also invited the public to its planning and zoning committee meeting (which is composed of citizen and Council representatives). This venue allowed for another opportunity to exchange information and views between the public, staff, the developer and committee members.

2.0 VISION AND PLANNING/DESIGN PRINCIPLES AND GOALS

2.1 The Vision

The vision for this community is that all aspects of the neighbourhood will focus on sustainability. Buildings will accommodate both live and work spaces. The community will be anchored by one large village centre (a mixed use commercial core) and two smaller neighbourhood retail service centres. The village centre will be focused on a commuter rail station and the amenity of Langford Lake. The urban environment in the village centre will be comprised of high quality public spaces, mixed uses, cultural and educational facilities and populated with many residences. A variety of housing—including single-family homes, town homes, condominiums, and affordable rental properties—will make the community available to people of all ages and income levels. Amenities will be located within easy walking distance of the neighbourhood core, while bike and bus routes and a commuter rail line to downtown Victoria will minimize traffic congestion.

An emphasis on compact, high density development will conserve land: 40% of the area will be preserved as parkland and open space and 100% of wetlands and streams will be protected.

All buildings will be environmentally sound. Features including energy-efficient and low pollution lighting, low-flow toilets, grey water irrigation systems, and pesticide-free landscaping will ensure that the entire neighbourhood is focused on facilitating an ecologically responsible lifestyle. The construction itself will also reflect the overall goal; locally produced and recycled materials will be used and construction waste will be carefully managed.

Green areas will be used for ecological processes and where appropriate, recreation, passive parks and community agriculture.

2.2 Planning Goals

The following goals were identified to guide the development of this site. These goals were kept in mind as a higher level framework for design decisions throughout the charette and subsequent design work:

- a) Create a healthy community that results in the optimum social, economic, and physical well-being of its people and the natural environment;
- b) Apply design principles for urbanization that respect the unique visual quality and rural history of the area;
- c) Use land efficiently;
- d) Protect and restore natural areas and ecological processes important to people, flora and fauna;
- e) Preserve clean and natural flow in area streams;
- f) Provide for a fair share of the region's new jobs;
- g) Improve air quality;
- h) Provide ample and affordable housing, schools, public infrastructure, facilities and transportation choices in the neighbourhood; and
- i) Preserve and create cultural opportunities throughout the community.

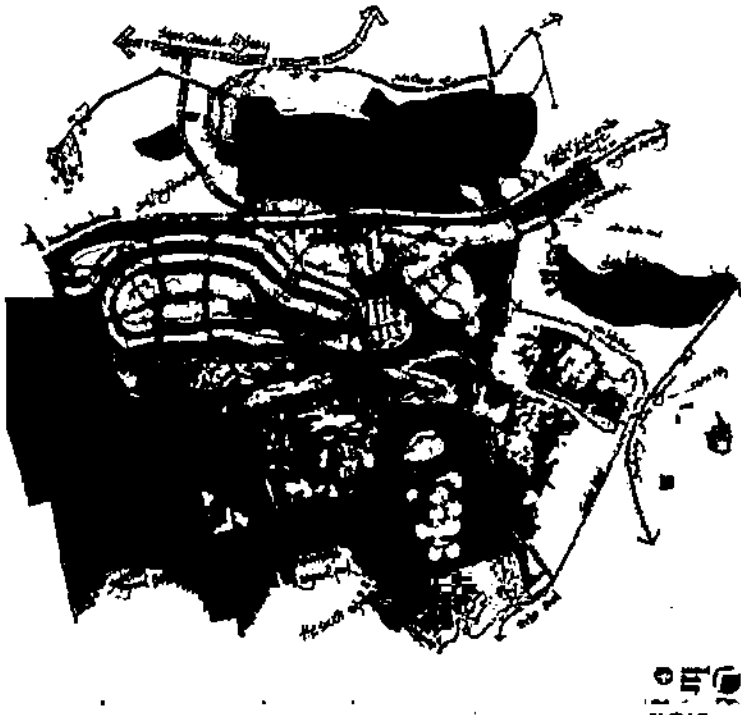


Fig 2.1
Wildlife and green corridor connectivity

2.3 Neighbourhood Characteristics

The following characteristics of sustainable neighbourhoods were also identified as being important to embed into the project design:

- | | |
|-----------------|--------------------|
| a) Integrated; | g) Productive; |
| b) Efficient | h) Multi-purpose; |
| c) Responsive | i) Low impact; |
| d) Adaptable; | j) Healthy; |
| e) Permeable; | k) Affordable; and |
| f) Transparent; | l) Inspirational. |



Fig 2.2
Paradise Park conceptual rendering

Layered on to these goals were the LEED ND criteria which were reviewed and adapted where appropriate to reflect the Canadian context and in particular for other Greenfield developments in Canada.

2.4 Key Design Concepts

A number of key concepts were derived from the project objectives and from site issues identified by the project team as they worked through the different stages of design. These concepts provided a general framework for the site planning:

- a. Preserve high value ecosystems;
- b. Maintain a network of open space;
- c. Concentrate development in buildable areas;
- d. Achieve desired number of residential units;
- e. Design street network to minimize slopes;
- f. Create a pedestrian-friendly environment;
- g. Place transit and commercial services at the core; and
- h. Connect to Langford community.

3.0 PLAN ELEMENTS

3.1 Land Use

The Westhills plan area is proposed to include a variety of land uses including:

- Single-family;
- Multi-family residential;
- Commercial;
- Institutional,
- Mixed use; and
- Park.

The specific allotment of land area shared between these uses is shown on Table 3.1 and Map 3.1.

Table 3.1 Land Use Summary

Land Use	Percentage %*
Residential	36.5%
Mixed use	7%
Open space	40%
Roads	15%
School site	1%
Business park	0.5%
Total	100%

*Approximate percentages

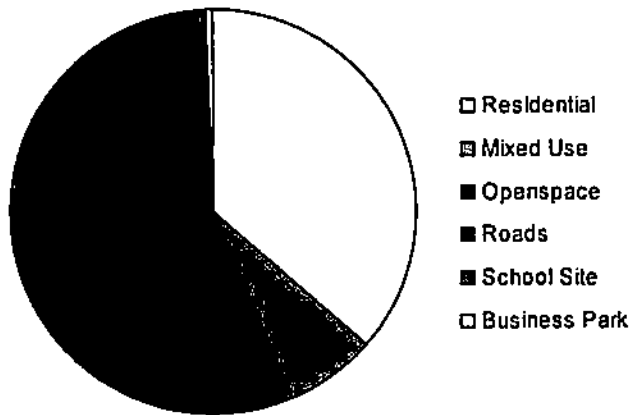


Fig 3.1
Land Use Graph showing percentage areas of designated land uses



LEGEND

SF7TH SINGLE FAMILY DWELLING/TOWNHOUSE	MU2 MIXED USE COMMERCIAL CORE	LANGFORD LAKE FORESHORE
MD1 MHA TL FAMILY DWELLING MEDIUM DENSITY	OPEN SPACE	BUSINESS PARK
MD2 MHA TL FAMILY DWELLING HIGH DENSITY	STREET NETWORK (R.O.W.)	
MU1 MIXED USE SUB COMMERCIAL CORE	SCHOOL SITE DEDICATION OPEN SPACE	

Westhills **LAND USE CONCEPT PLAN**
WESTHILLS GREEN COMMUNITY



Map 3.1
 Land Use Graph showing percentage areas of designated land uses

The layout of the Westhills Green Community is approximate so that the location of land uses can be shifted to protect sensitive ecosystems provided that approximately 50% of the total area subject to the Westhills Master Plan is preserved as park or designated open space.

The concept plan identifies a high density village centre (a mixed use commercial core) adjacent to Langford Lake. An overall floor space ratio of 4 to 1 (development to land area) is contemplated for the village centre. To give effect to the plan, the zoning bylaw will permit a 4:1 density; however, Council may consider amending the zoning bylaw to transfer some of the density within the village centre from lower density site specific developments.

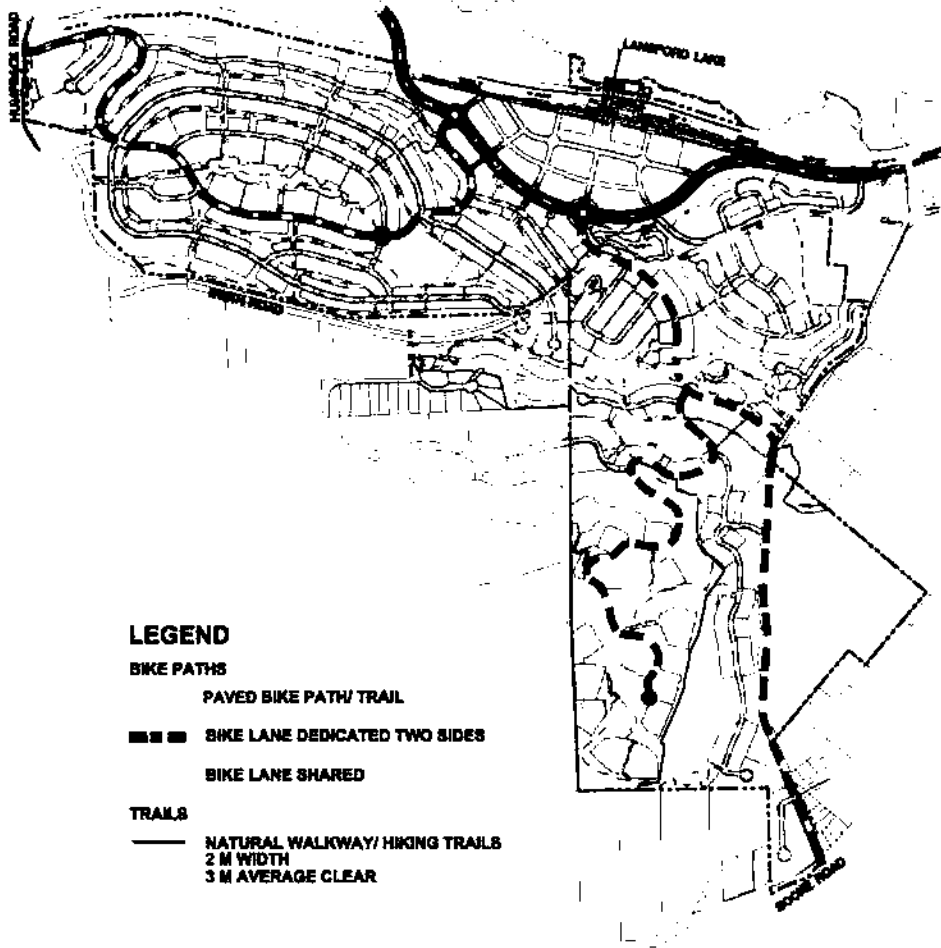
3.2 Transportation

One of the objectives of the master plan is to employ alternative transportation strategies to significantly reduce reliance on automobiles.

The layout and density of the Westhills plan is based on the provision of a commuter rail service and the use of alternative transportation modes. While the community will still function without the service in terms of road facilities, the plan's design is intended to work best with commuter rail.

Commuter rail is intended to provide much of the transportation needs between this area to the rest of the City of Langford and other areas of the region. To support rail service, it is also proposed to provide an internal energy efficient shuttle bus service for residents wanting to go to the rail service or to the commercial areas in the plan area or adjacent to the plan area.

The plan also calls for a full network of internal trails, sidewalks and bicycle lanes as shown on the plans labeled Map 3.2. Appropriate connections would be made to adjacent trails and sidewalks and in particular to the Trans Canada Trail and Galloping Goose Trail.



LEGEND

BIKE PATHS

PAVED BIKE PATH/ TRAIL

BIKE LANE DEDICATED TWO SIDES

BIKE LANE SHARED

TRAILS

NATURAL WALKWAY/ HIKING TRAILS
2 M WIDTH
3 M AVERAGE CLEAR

Westhills BIKE PATHS & TRAILS PLAN
WESTHILLS GREEN COMMUNITY



Map 3.2
Westhills Bike Paths and Trails Plan

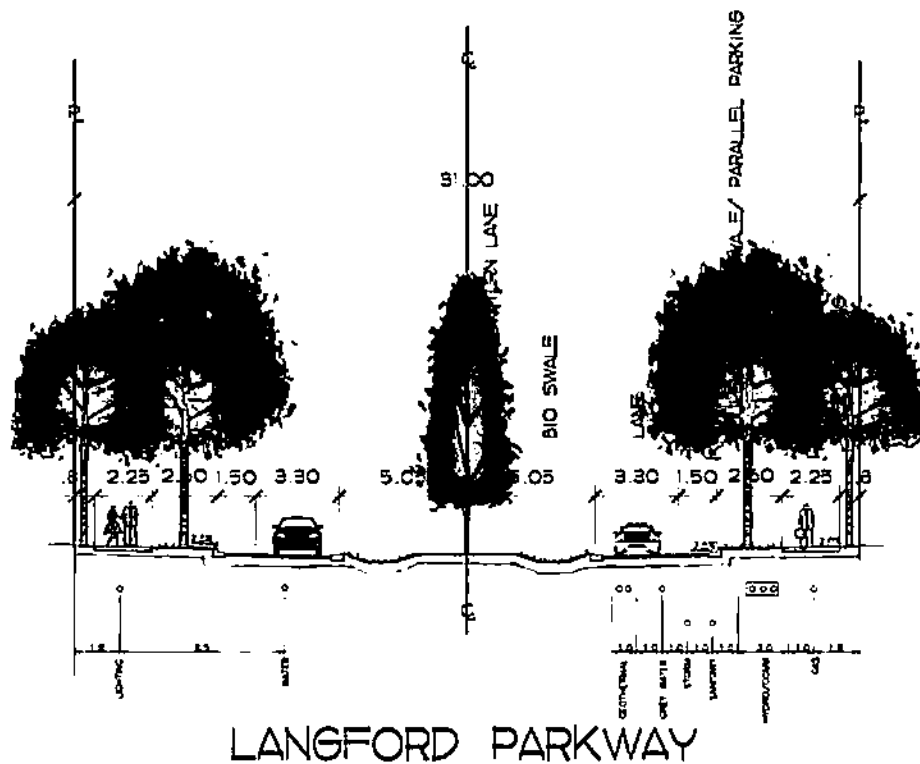
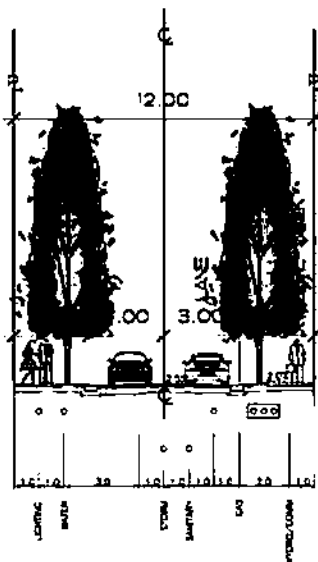


Fig 3.2
Westhills Green Community Langford Parkway streetscape section concept

By incorporating a fully multi-modal transportation plan including, commuter rail, shuttle busses, transit, bicycle and pedestrian networks, it is feasible for residents to greatly reduce their use of the automobile.

Considerable attention has been given to appropriate road standards. These are attached as Figures 3.3 and 3.4. The goal of these standards is to minimize land dedicated to roads and incorporate green elements for storm water (bio swales) treatment and for aesthetics. In addition to these standards, the development will incorporate woonerf designed street sections to calm traffic and to make the street part of the public green space.



COMMERCIAL CORE LANE

Fig 3.3

Westhills Green Community commercial core streetscape section concept

3.3 Parks and Open Space

Forty percent of the plan area is proposed as park land. This exceeds the provincial legislation entitlement of 5% at the time of subdivision by 35%.

Most of the park land is contemplated to be kept in a natural state and is proposed to be improved through the removal of invasive species. Active recreation is accommodated in one planned sports field and also can be included in the proposed school site. Recreational trails throughout will provide the residents opportunities to walk and bicycle.

One of the significant features of the plan is the provision of a public fishing pier and beach on Langford Lake.



Fig 3.4
Conceptual rendering of residential development in the Westhills Green Community

3.4 Design Guidelines

The entire site is designated as a Development Permit Area for Form and Character (multi-family and commercial), for environmental protection and for protection of development from hazardous conditions (steep slopes and interface fire hazard). This is to ensure that the built environment is of a high quality and promotes security and also respects and protects natural areas proposed to be protected and retained and to ensure that the development is provided protection against hazardous conditions.

Justification

Achieving a quality urban environment through design is important to ensure that dense urban environments are attractive, inviting and ultimately successful places.

It is the premise of the LEED program to ensure that environmentally sensitive areas are protected, therefore, the environmental Development Permit process is important to effect that goal.

Given the existence of steep slope areas on the site and the dense tree cover, the Development Permit process for protecting development from these potential hazards, is important.

3.4.1 Guidelines

Development Permit guidelines contained in Langford's Zoning Bylaw are largely sufficient to guide form and character, protect the environment and protect development from hazards. Accordingly, the general guidelines for multi-family, commercial development apply to this area as do the guidelines for environmental protection and for protection of development from hazardous conditions.

General Form and Character Guidelines

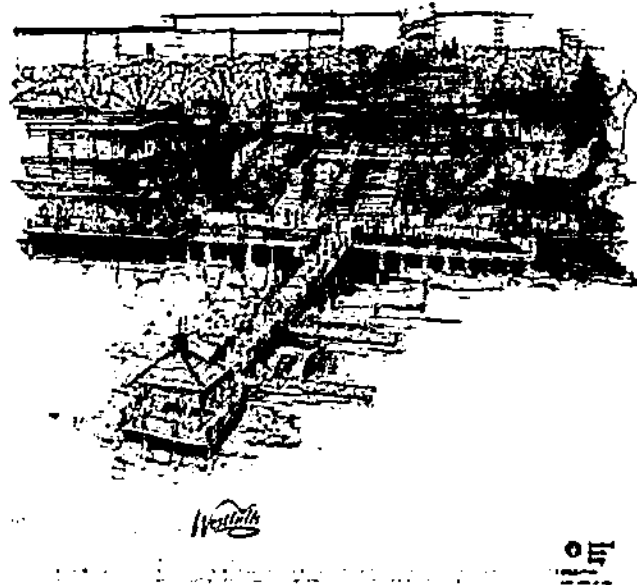
The objective of these Development Permit Guidelines is to supplement the General Development Permits Guidelines for commercial and multi-family residential development (found in Appendix G and H respectively) specifically for the development of the property commonly known as the Westhills Green Community. These design guidelines are intended to ensure that development:

1. Is constructed to high standards, both material and aesthetic;
2. Complements the social and environmental goals integrated with the LEED standards contained within this plan; and
3. Is compatible with other surrounding land uses.

Development in the own Centre area of the Westhills Green Community as shown in the Westhills Land Use Plan must also follow the General Design Guidelines for Downtown Langford (found in Appendix J). Where Downtown Langford Guidelines refer to arterial roads in the downtown area, these guidelines are understood to also be applicable to arterial roads in the Westhills Town Centre Area.

When there is a conflict between these guidelines and the general guidelines, the following will supersede.

Fig 3.5
Waterfront amenity conceptual drawing



Location and Distribution of Development

- 3.4.1.1 The location and distribution of uses should be generally as illustrated in the map contained within Map 3.1 (Westhills Land Use Concept Plan).
- 3.4.1.2 The City Planner may approve variances to the layout on the map contained within Map 3.1 (Westhills Land Use Concept Plan), if in his/her opinion that:
- a) There is superior environmental protection with the variance and that this is corroborated with a report from a professional biologist; and/or;
 - b) There is superior screening of the development from visitors from adjacent existing development with no significant net environmental loss and that this is corroborated by a professional biologist and/or;
 - c) There is a reduction in site impact due to topographic conditions with the variance and that this is corroborated by a professional biologist or other appropriate professional.
- 3.4.1.3 Each phase of development shall be supported by an environmental impact mitigation and remedial plan to minimize the impact of servicing and development.
- 3.4.1.4 The two clusters of medium and high density development (West and South area of plan) are premised on the inclusion of neighbourhood commercial uses.

Multi-Storey Residential and Multi-Use Buildings Over Five Storeys in Height

In addition to the design guidelines contained within Appendix G and H and J (if applicable), the following guidelines are intended to direct the design of residential and multi-use buildings over five storeys in the Westhills Development Permit Area.

- 3.4.1.5 The overall architectural design of each building shall feature varied articulation, particularly at the lower storeys, decorative railings and roof edges.
- 3.4.1.6 Buildings over five storeys in height should be clustered within the Westhills community to afford a minimum area occupied by streets and provide generous open space.
- 3.4.1.7 To make buildings over five storeys in height "pedestrian friendly", the first two storeys of towers should be strongly articulated through the use of overhangs, signage and other means of stimulating visual interest. Where possible, upper storeys should actually be set back and recede or appear to recede; this can be accomplished by tiering the lower storeys of the tower (i.e., the second and third storeys).
- 3.4.1.8 Balconies should be a minimum of 1 m² in area.
- 3.4.1.9 Units may include their own "earth balcony" capable of supporting planters with shrubs and small trees.
- 3.4.1.10 Each front entrance to ground-oriented units should be visually distinctive from its neighbours, or capable of becoming distinctive from its neighbours.

- 3.4.1.11 Each doorway and entrance should provide a combination of appropriate entrance transitions through the use of alcoves, seating, direction changes, level changes, and variation in materials.
- 3.4.1.12 In areas where multiple towers are constructed or anticipated, a central common area with intensive landscaping and open areas available for barbecues and picnics should be provided. These areas should be linked to the surrounding towers by pedestrian paths accessible to the disabled. The central common area should also be connected by pedestrian paths to other smaller common areas, service commercial amenities, adjacent parks and the greater neighbourhood trail system.
- 3.4.1.13 Terraced building articulations are encouraged; flat roofs and terraced areas should be accessible and “roofscaped” with landscaping and patio-style recreational amenities.
- 3.4.1.14 Buildings over five storeys in height should have a strong relationship to the street, and in particular should exhibit a human or pedestrian scale at the ground level. This should be accomplished by:
- 3.4.1.14.1 Including, at the ground level of each building, dwelling units or commercial units with direct pedestrian access to the street;
 - 3.4.1.14.2 Using extensive stepping and terracing of the elevations and altering the massing of structures so that the heights of various portions of building are staggered between higher and lower storeys;
 - 3.4.1.14.3 Including in the building façade dormers, shed roofs and balconies that break up vertical and horizontal planes where practical; and
 - 3.4.1.14.4 Limiting the access to underground parking to not more than one entrance per road frontage, and by siting driveways and garage doors in discreet ways so that they do not front directly onto the street where practical.
- 3.4.1.15 Where practical, architectural elements such as interesting gables and vaulted ceilings should be constructed on top storeys along with glazing in gable ends to add verticality to buildings.
- 3.4.1.16 The top storey of any building should be built within the roof gable where practical.
- 3.4.1.17 The design of buildings over five storeys in height should ensure that there is appropriate transition between building levels. The top floor may be designed as a penthouse floor provided that the penthouse is integrated into gables or peaked intersecting gables where possible.
- 3.4.1.18 Mechanical equipment should be fully screened within the roofline of any building.
- 3.4.1.19 Buildings over five storeys in height should not be used as platforms for radio masts, cell phone towers or other telecommunication devices, unless these structures are discreetly located within the roofline of the building or visually obstructed with architectural screening.

- 3.4.1.20 Buildings over five storeys in height should have a minimum 30 m separation from each other.
- 3.4.1.21 Nearby buildings over five storeys in height should be positioned in a manner that helps to define street edge definition while at the same time allowing for gaps in views and sunlight penetration to open spaces.
- 3.4.1.22 Development of buildings over five storeys in height should frame and preserve views, using a balance of higher and lower building heights rather than groups or large areas of buildings that are uniform in height.
- 3.4.1.23 All multi-storey development incorporating buildings with more than five storeys should incorporate Crime Prevention Through Environmental Design (CEPTED) design principles.
- 3.4.1.24 Semi-private exterior amenity spaces should be made accessible to a variety of user groups including the disabled.
- 3.4.1.25 Where possible, buildings over five storeys in height should have some setback from the boundaries of the lot to allow for ground level landscaped areas and semi-private outdoor space.
- 3.4.1.26 Rooftops that are visible from the windows and balconies of taller buildings should appear attractive and be designed to appear well-maintained and attractive over time.
- 3.4.1.27 Rooftop mechanical equipment installed on roofs should be fully screened from the windows and balconies of smaller buildings.

All medium and high density developments must include landscape plans for all outdoor areas with the intent of creating interesting, pedestrian-friendly, accessible, landscaped, areas with areas for sitting, bicycle racks, and where appropriate, interaction and play.

Woonerf Streets

3.4.1.28 Where practical, woonerf streetscape design principles should be used to design new neighbourhood streets in the Westhills Green Community Area. "Woonerf" is a Dutch word that translates into English as "street for living". Woonerfs are designed in such a way that the needs of automobile drivers are rectified with the needs of the users of the street as a whole; such users include pedestrians, bicyclists and playing children. Woonerfs therefore sustain lower traffic velocities through the utilization of integrated traffic calming devises and intensive landscaping.

Figs. 3.6 and 3.7
Examples of Woonerf streetscapes



www.pedbiketimages.org / Dan Burden



www.transport2000.org.uk / Julia Thomas

Fig. 3.8
Westhills Village Centre
conceptual streetscape
rendering



4.0 LEED BASED DEVELOPMENT OBJECTIVES

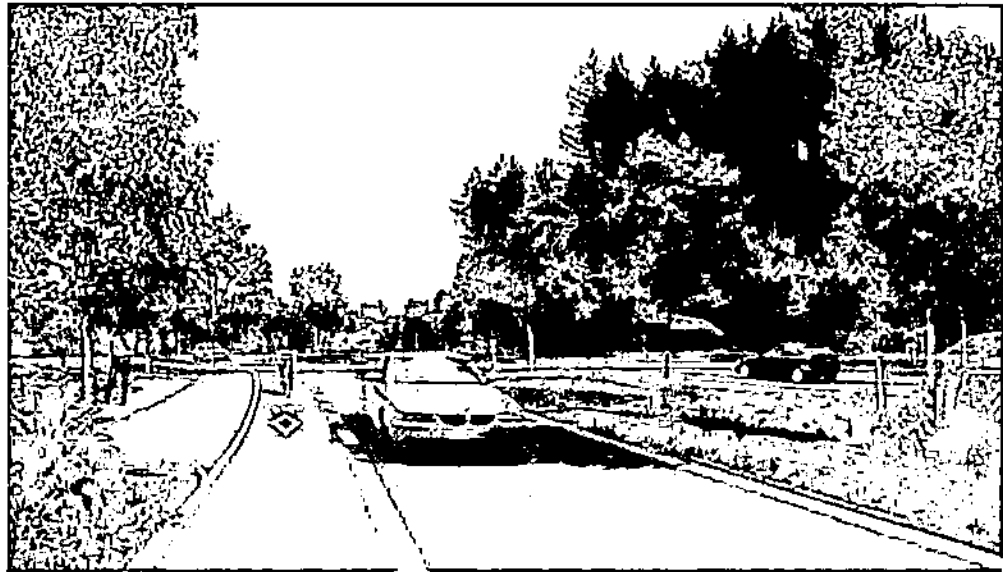
The Westhills Community is intended to serve as a demonstration project for a new standard of “green” neighbourhood development. The project will include a wide variety of commercial, residential and mixed use building types. The community will be developed generally in accordance with guidelines that are being created by the Canadian Green Building Council (CaGBC). These guidelines, which are in the draft phase, will form a green building certification system called LEED for Neighbourhood Developments (LEED ND). The current LEED system (LEED for New Construction) assesses individual buildings, while the LEED ND system will look beyond individual buildings and assess entire neighbourhood developments.

Each of the following plan objectives illustrate ways in which the Westhills Community may achieve leadership in energy and environmental design (LEED) and reach toward LEED ND certification. These policies are meant to be flexible to allow for development with the best possible likelihood of LEED certification.

4.1 Location Efficiency Objectives

4.1.2 Transportation Efficiency: As the Westhills Community develops, extensive networks of pedestrian paths and cycling routes will be provided, including infrastructure for secure bike storage at appropriate locations (e.g. the rail station, commercial areas, etc.). These facilities are expected to provide connections within the Westhills Community, as well as connect to the wider network of pedestrian paths and cycling routes in Langford. The creation of a commuter rail service, one that will provide transportation to Victoria, as well as a shuttle bus service that will provide transport within the development are also goals of the Westhills Community. Together these initiatives are expected to increase transportation efficiency and choice.

Fig. 4.1
Conceptual rendering of a parkway streetscape in the Westhills Green Community



4.1.2 Water and Stormwater Infrastructure Efficiency: The Westhills Community is expected to create a stormwater management system based on maintaining pre-development run-off and ensuring water quality is maintained through bio-treatment on site.

4.1.3 Reduce Automobile Dependence: The Westhills Community will seek to reduce dependence on automobile use through a variety of initiatives, which should include the creation or establishment of: compact urban forms, commuter rail, shuttle bus service, a network of trails and bike paths, woonerf streets and other traffic calming measures, and including employment and recreational opportunities within the Westhills Community.

4.1.4 Contribution to Job-Housing Balance: The Westhills Community is expected to include both residences and employment. Proximity of the Westhills commercial core to Westshore Town Centre, downtown Langford and the Goldstream Meadows Business Park, and the creation of the commuter rail will aid in further integrating commercial and residential activities.

4.1.5 Schools: The Westhills Community should incorporate educational opportunities by providing land on the eastern portion of the property for a school site, and by allowing for

educational facilities in the community core that may include early childhood education, higher learning and adult learning (e.g.: Learning Commons).

4.1.6 Access to Public Space: The Westhills Community is expected to create a number of public spaces. The community core area and Langford Lake foreshore area should be designed to include inviting public gathering areas (e.g. a public amphitheater and a pier). Approximately 40% of land base is intended to be public space, and parts of this area will include amenities such as public sports fields and neighbourhood parks.

4.2 Environmental Preservation Objectives

4.2.1 Imperiled Species and Ecological Communities: The Westhills Community should protect imperiled species and ecological communities. Implementation of development should incorporate the guidance of biologists during site specific designs.

4.2.2 Parkland Preservation: The Westhills Community should preserve significant portions of the property as new parklands.

4.2.3 Stormwater Management: The Westhills Community will prepare a storm water management plan for the entire site to the satisfaction of the City of Langford. This plan should include the capture of waste water for reuse in landscaping. Waste water should be biologically treated to improve water quality. Bio-swailes should be incorporated into road designs wherever possible, and porous concrete paving units will be incorporated in the village centre where feasible.

4.2.4 Wetland and Water Body Conservation and Management: The City of Langford will require a stormwater management plan to aid in the conservation of wetlands and water bodies. Wetlands within the Westhills development and habitat in wetland areas should be enhanced under the direction of qualified professionals.

Fig. 4.2

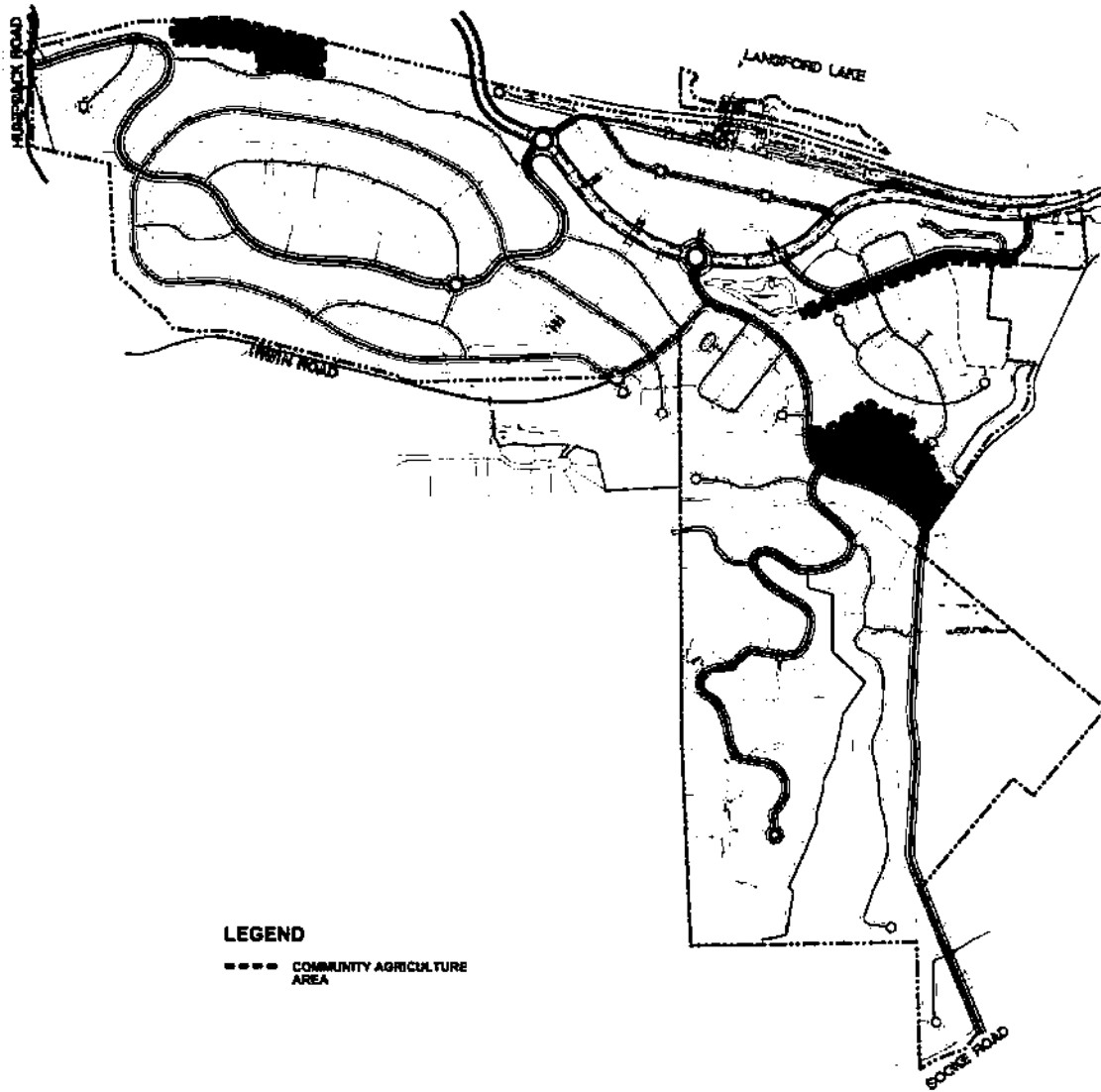
Drawing showing the North Shore of Langford Lake after completion of the Westhills Green Community



4.2.5 Erosion and Sediment Control: The Westhills Community should design a sediment and erosion control plan to the satisfaction of the City of Langford.

4.2.6 Farmland Protection. The Westhills Community intends to dedicate lands for community agriculture to the City of Langford. These lands will be planted with appropriate fruit trees and will be available for community allotment gardens in the areas shown on Map 4.1.

- 4.2.7 Support Off Site Land Conservation:** The Westhills Community encompasses the lands commonly known as Parkdale hill. This area of significant sensitive ecosystem is entirely outside of the proposed development area for Westhills, and this site (Parkdale Hills) should be preserved with limited public access.
- 4.2.8 Steep Slope Preservation/Erosion Protection:** No building in the Westhills Community should occur on slopes greater than 30% and should also conform to City of Langford requirements for erosion control and slope stability.
- 4.2.9 Site Disturbance:** The development of the Westhills Community should seek to minimize site disturbance during construction. This objective is furthered by the high density of the development, particularly in the commercial core and the large portion of land left as open/green space.
- 4.2.10 Outdoor Hazardous Waste Pollution Reduction:** The Westhills Community should develop best practices for Integrated Pest Management, to the satisfaction of the City of Langford, that will be used on public lands. A covenant should be registered on title to control the use of pesticides and fertilizers on private lands.



LEGEND
 - - - - COMMUNITY AGRICULTURE AREA

Westhills COMMUNITY AGRICULTURAL PLAN
 WESTHILLS GREEN COMMUNITY



Map 4.1
 Westhills Community Agricultural Plan

Fig. 4.3
Conceptual rendering
showing a multi-use
residential streetscape in
the Westhills Green
Community



4.3 Compact, Complete and Connected Neighbourhood Objectives

4.3.1 Open Community: The Westhills Community should be an open community, and not be gated.

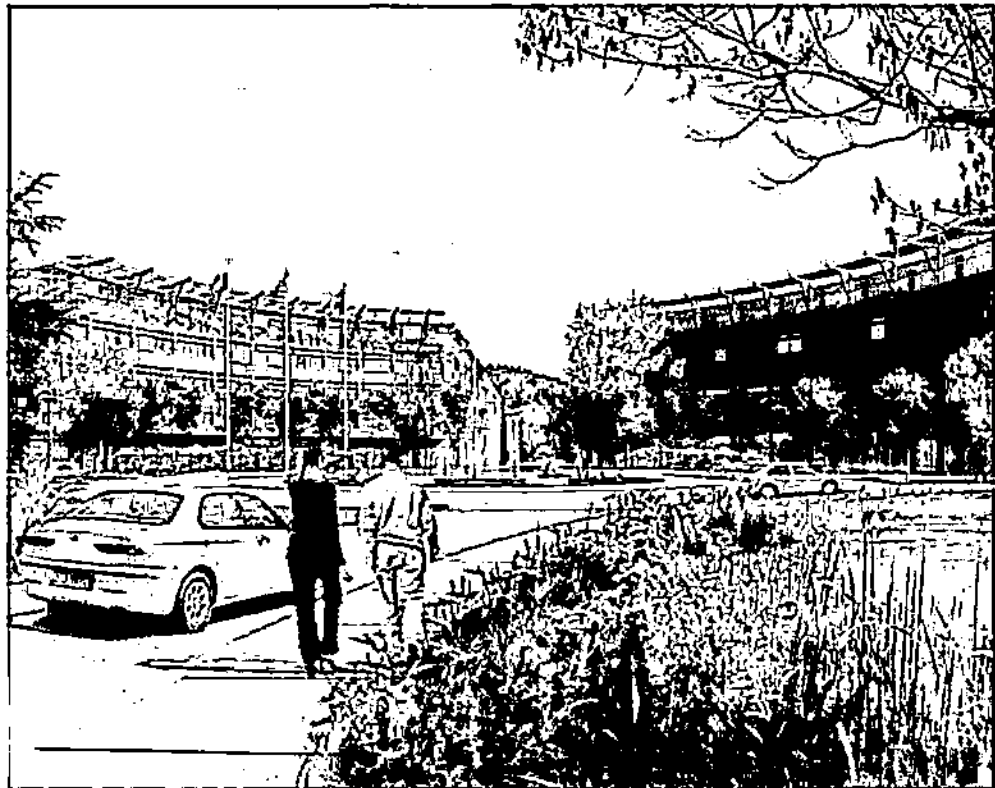
4.3.2 Compact Development: The Westhills Community is intended to be a compact development with specific areas of high density and an interconnecting transportation system that will include commuter rail and shuttle busses, creating pedestrian-friendly streets with sidewalks and walking paths. Most of the development should be in close walking distance (within 400 m) to public transit. A community shuttle should provide service for all of the plan areas.

4.3.3 Density of Uses: The Westhills Community should provide a large variety of land uses (commercial, residential, institutional and service utilities) and these should be within ease walking distance of most residents.

4.3.4 Housing Diversity: The Westhills Community should include a wide variety of housing types, including small, medium, and large single-family lots, homes with mandatory secondary suites, live/work residences in the downtown core, condominium apartments and townhouses of various sizes, duplexes and assisted living facilities.

- 4.3.5 Affordable Rental Housing:** The Westhills Community will include a minimum of 150 rental apartment units and secondary suites should be mandatory in all single-family dwellings on all lots over 550 square meters in area, and permissive in single-family dwellings on smaller lots.
- 4.3.6 Affordable Market Housing:** The Westhills Community should ensure that 5% of housing is affordable by providing one-third of its single-family dwellings on affordable small lots in addition to other forms of affordable multi-family units.
- 4.3.7 Reduced Parking Footprint:** The Westhills Community will create woonerf streets with on-street parking as well as underground parking. All roads should be designed for some level of traffic calming and parking, as may be practical.

Fig. 4.4
 Conceptual rendering showing public amenity areas in the Westhills Green Community Village Centre



- 4.3.8 Pedestrian Network and Walkable Streets:** All buildings in the Westhills Community should be designed so that their shape, access, and adjacent streets meet Smart Growth principles and are pedestrian-oriented. The Westhills Community should provide direct and safe connections for local destinations and neighbourhood centers with continuous sidewalks along all streets within the project, as well as woonerf streets and footpaths. All streets should be punctuated with frequent pedestrian intersections.
- 4.3.9 Pedestrian Safety and Comfort:** The Westhills Community should provide on-street and off-street parking. The allowable speed on these streets should conform to municipal requirements and pedestrian safety. Street trees should be provided along streets in the commercial core between the travel way and sidewalks at an interval of no less than 12 m.

The ground floor of residential buildings should have a finished floor elevation of no less than 600 mm above the sidewalk grade.

4.3.10 Superior Pedestrian Experience: The Westhills Community should be designed and built with a minimum of 50% of the total number of office buildings including ground floor retail. All business and/or public service buildings should have public access off street sidewalks or plazas. Trees that are planted in the public realm should provide effective shade, when mature, to over half the sidewalks within the Commercial Core. All sidewalks in the commercial areas should be constructed with pavers and in the village centre the road surface should use concrete pavers, where feasible.

4.3.11 Affordable/Effective Transit: The Westhills Community is expected to provide three fuel efficient mini-buses that would provide service between the smaller neighbourhood commercial centers and the village centre (the mixed use commercial core). The cost for this bus ridership would be free to the users and paid for through a local special tax levy.

4.3.12 Transit Amenities: The Westhills Community should provide enclosed transit shelters, benches, wayfinding signs and transit information within the Commercial Core. In the residential areas the mini bus schedule should be posted at convenient locations.

4.3.13 Access to Nearby Communities: The Westhills Community should provide safe and direct connections for pedestrians and cyclists as well as automobiles to local destinations and neighbourhoods by allowing a through street every 270 m where topography and legal access allows.

4.4 Resource Efficiency Objectives

4.4.1 Certified Green Buildings: All commercial and multi-family buildings in the Westhills Community will be registered and evaluated using the LEED NC methodology. These buildings will attain at least a Certified Rating. Single-family housing and townhouses will be constructed to meet or exceed the "Built Green"TM standard.

4.4.2 Energy Efficiency in Buildings: All buildings in the Westhills Community that exceed three storeys in height should attain a minimum of 10% better than the Model National energy code guideline. All buildings below three storeys should comply with Energy Star requirements.

4.4.3 Water Efficiency in Buildings: All of the buildings in the Westhills Community should minimize potable water use by using low-flow fixtures.

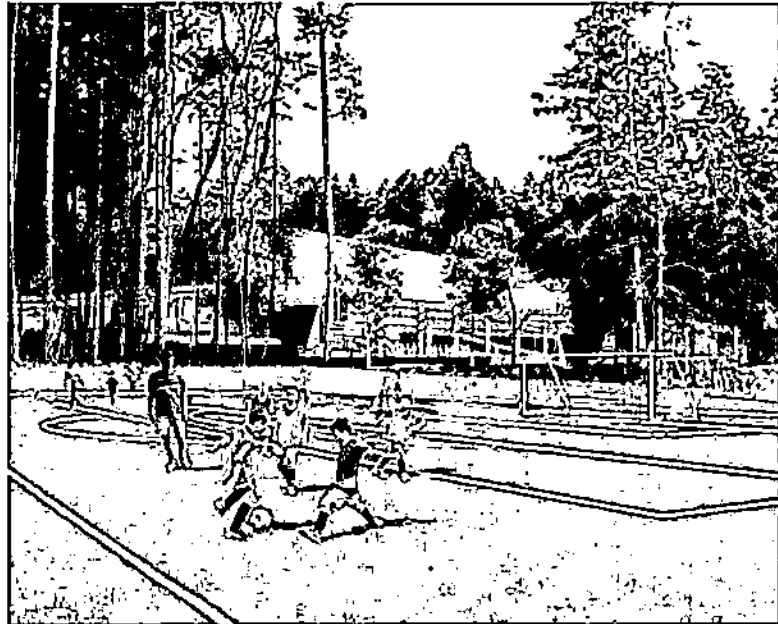
4.4.4 Infrastructure Energy Efficiency: Common and public amenities in the Westhills Community should include energy efficient infrastructure.

4.4.5 On Site Power Generation: If possible, the Westhills Community should develop a district energy utility to supplement conventional energy sources. To this end the City will encourage geothermal energy, micro-hydroelectric generation, heat harvesting from local sewers and treatment centers for renewable energy.

4.4.6 Irrigation Efficiency: The Westhills Community should incorporate high efficiency irrigation equipment.

4.4.7 Grey Water and Stormwater Reuse: The Westhills Community will attempt to capture and reuse the grey water and stormwater generated on site.

Fig. 4.5
Conceptual rendering playing field
community amenities in the
Westhills Green Community



4.4.8 Wastewater Management: The Westhills Community will attempt to treat and reuse the wastewater generated on site.

4.4.9 Recycled Content: The Westhills Community will attempt to use recycled materials for its common and public infrastructure.

4.4.10 Regionally Provided Materials: The Westhills Community should use locally available materials for its common and public infrastructure.

4.4.11 Construction Waste Management: The Westhills Community should promote efficiency in disposal of all construction and clearing waste by diverting up to 75% of construction waste materials from the landfill and by reusing up to 25% of the clearing material (tree fall) back on the site.

4.4.12 Comprehensive Waste Management: The Westhills Community should collect household recycling and provide an on-site composting facility that will allow residents to drop off composting materials.

4.4.13 Light Pollution Reduction: The Westhills Community should design its public and common lighting infrastructure to reduce light pollution. All exterior luminaires with more than 1000 initial lamp lumens should be shielded and all luminaires with more than 3500

initial lamp lumens should meet the full cutoff IESNA Classification. Where possible, solar powered lighting and energy-efficient LED lighting should be used in public infrastructure.

4.5 Innovation and LEED Professional Objectives

4.5.1 LEED Accredited Professional: The Westhills Community should retain a LEED accredited professional to assist in the overall site planning and documentation accumulation to allow the project to be evaluated for LEED certification.

4.5.2 Innovation: The Westhills Community should explore other innovative measures to further the general sustainability and green objectives of the plan.

5.0 Implementation and Monitoring

The City should employ a number of implementation and monitoring tools.

First, the Zoning Bylaw required to implement the plan will be written using the density bonusing provision of the *Local Government Act* to secure the amenities included in the proposed community such as:

- 40% open space;
- Commuter Rail contribution;
- Amenity fund;
- Land for a sports field;
- Village centre amphitheatre and cultural facility/building/
- Rental housing; and
- Secondary suites mandatory in dwellings built on lots over 550 m² in size and permissive on smaller lots.

The bylaw will also include the provisions for the mixed use and density allowances.

Second, a Development Agreement will be written and registered on title to affect all of the non-land use development requirements such as, but not limited to:

- Erosion and stormwater control plan;
- Local commuter busses;
- Construction waste management plan;
- Participation in local commuter bus tax levy;
- Requirement for LEED certification of all multi-family and commercial buildings (exclusive of townhouses and single-family dwellings);
- Requirement for all single-family dwellings and townhouses to be constructed to at least a "Bronze" Built Green™ standard;
- Provision of rental units;
- Trail and bikeway construction in accordance with the Trail Plan and Bicycle Plan; and
- Use of efficient water appliances in buildings.

Third, all relevant development approval processes in the City will be modified to effect the plan with its LEED requirements, and a protocol and monitoring agreement will be established to guide the relationship of the partners during the implementation of the project, and which lays out how LEED ND certification should be achieved and policy objectives satisfied.

Fourth, Westhills in consultation with the City of Langford will explore the possibility of partnering with other organizations to help build trails, bicycle infrastructure as well as other recreational facilities and community amenities.

Due to the scale of the Westhills project and the evolving nature of sustainable development planning and practice, the standards set out in this plan are guidelines that may be adjusted by the City in consultation with the landowner when appropriate.

Monitoring and assessment will be done through the LEED certification process and in accordance with the protocol agreement. As this is a third party, rigorous and scientific process, there is confidence in accurate monitoring.

Appendix Q: Valley View Neighbourhood Area Plan

1.0 Valley View Neighbourhood Area Plan

A comprehensive development plan for the Valley View Neighbourhood Area was submitted to the District of Langford in 2003. The Valley View Neighbourhood Area comprises approximately 14 ha of land. The Valley View Neighbourhood Plan proposes to develop the area shown on attached Map 10 as a new neighbourhood within the District of Langford. The neighbourhood is proposed to include residential, limited commercial, and community recreational uses.

In designing this new neighbourhood, the plan policies and development permit guidelines will encourage a development pattern that seeks to preserve and enhance existing landforms and vegetation wherever possible. In addition, comprehensive guidelines to protect the neighbourhood from interface fires will be followed.

A mix of housing styles are proposed and will include single-family residential, two-family residential, and secondary suites for a total of 160 dwelling units (not including suites). In addition, a comprehensive system of trails and parks will be dedicated and constructed by the owner of the lands. These parks will include linkage trails between residential streets and trails connecting to the District of Langford trail network. There will be a combination of natural open space around riparian areas and developed recreational park.

2.0 Planning Principles and Policies

Development of the Valley View Neighbourhood will be based on the following principles.

- 2.1 The development as a whole should exhibit an unified identity, achieved by similar and complimentary, but not identical architectural forms.
- 2.2 The neighbourhood will include a park and trail network that integrates with Langford's Trail Master Plan and Parks and Recreation Master Plan.
- 2.3 A comprehensive network of paths, sidewalks, and trails will link residential areas, parks, open spaces, and commercial areas.
- 2.4 Linkages to existing and future neighbourhoods adjacent to the Valley View Neighbourhood will be provided by way of roads and trails.
- 2.5 Development will be in accordance with environmental standards designed to integrate the development with the natural environment.
- 2.6 Environmental features will be respected and protected where possible during development and will be maintained. Development will be allowed to occur within a flexible zoning scheme; however, it will be regulated in accordance with development permit guidelines.

3.0 Development Permit Areas and Guidelines

In accordance with Section 919.1 of the *Local Government Act*, all of the lands identified within the Valley View Neighbourhood Plan are designated development permit areas for the following purposes.

- 3.1 Protection of the natural environment, its ecosystems and biological diversity.

- 3.2 Protection of development from hazardous conditions.
- 3.3 Establishment of objectives for the form and character of intensive residential developments.
- 3.4 Establishment of objectives for the form and character of commercial and multi-family residential developments.

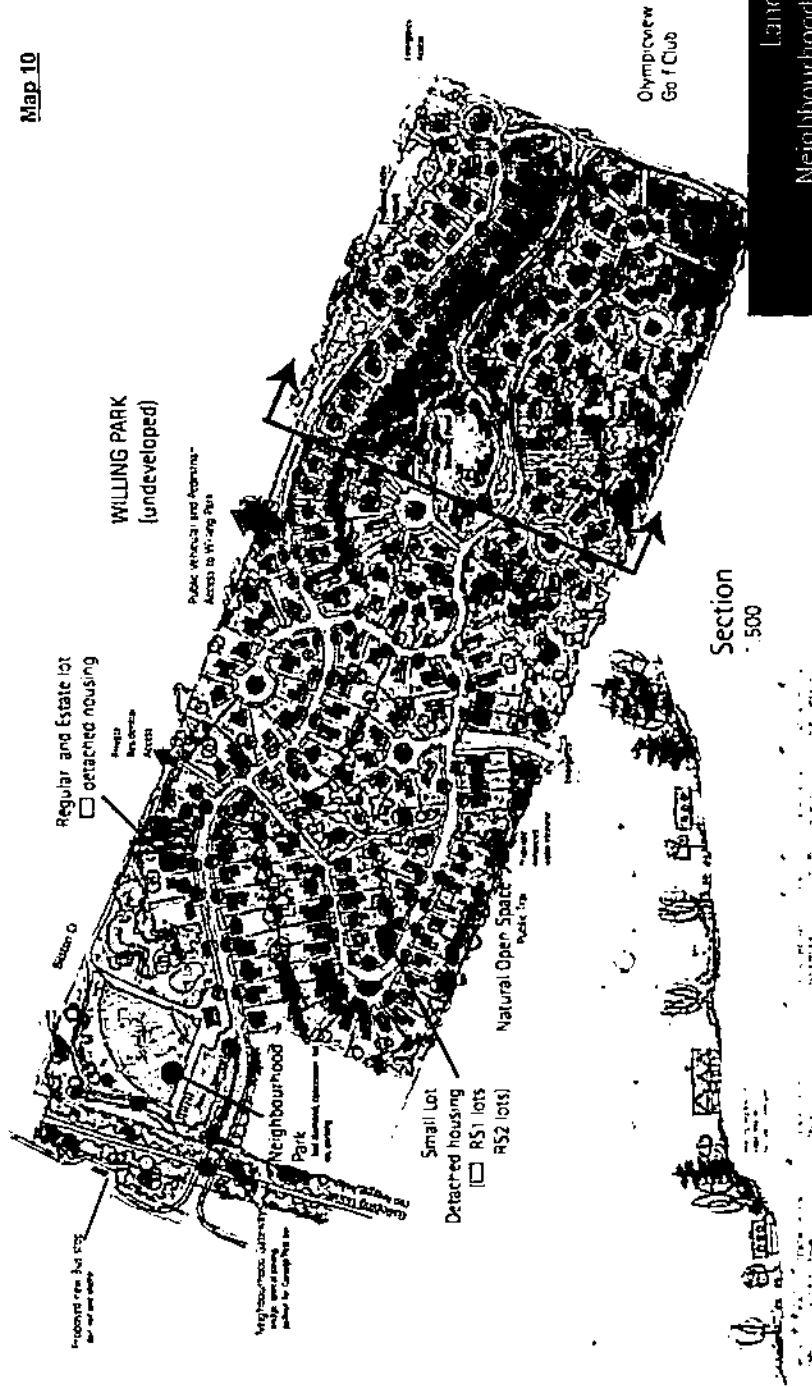
Council's general Development Permit Guidelines for environmental protection, commercial, multi-family and intensive residential form and character and interface fire hazard protection apply to this area.

The Valley View Neighbourhood Plan provides for multi-family, single-family, intensive residential, and commercial land uses. Map 10 (Land Use Concept Plan for Valley View Neighbourhood) identifies the general layout of open spaces, residential, and commercial uses. Within the residential designated areas, multi-family and intensive residential uses, in addition to single-family uses, may occur. Further, Map 10 will be applied as a development permit guideline, which provides overall direction for the layout of land uses. While this layout is not intended to be inflexible, any deviation from the plan must be supported by substantial compliance with relevant development permit guidelines and an amended concept plan demonstrating that the overall layout concept meets the relevant guidelines and bylaws of the City of Langford.

4.0 Exemptions

- 4.1 The following are exempted from a development permit application. Residential subdivision or intensive residential development of any land controlled by a section 219 covenant, approved by the City Planner, and registered in favour of the City of Langford, which controls the form and character of intensive residential development on the subject property.

Map 10



DISTRICT OF LANGFORD

land use concept

Appendix R: South Langford Neighbourhood Plan

EXECUTIVE SUMMARY

The South Langford Neighbourhood Plan is a new vision, a framework for shaping land use and density while preserving community values, in an area that may be defined as all those lands within the City of Langford that lie south of Highway No. 14 (Sooke Road).

Drafted under the direction of an Adhoc Steering Committee of Council that included councillors, staff, stakeholders as well as community and neighbourhood leaders, the South Langford Neighbourhood Plan was informed by numerous public open houses, surveys, web-based feedback and informal discussion. The overall result of which was a clear choice for change.

South Langford Neighbourhood Plan is intended to integrate fully with the City of Langford's Official Community Plan (OCP).

1.0 INTRODUCTION

1.1 PURPOSE OF THE SOUTH LANGFORD NEIGHBOURHOOD PLAN

The expansion of municipal services into the South Langford Neighbourhood and subsequent development pressures prompted a need for a local plan specific to South Langford. The purpose of the South Langford Neighbourhood Plan is to:

- create a framework for shaping land use and density in South Langford;
- reflect community values;
- produce a strategy that will ensure the full implementation of the community's vision for South Langford;

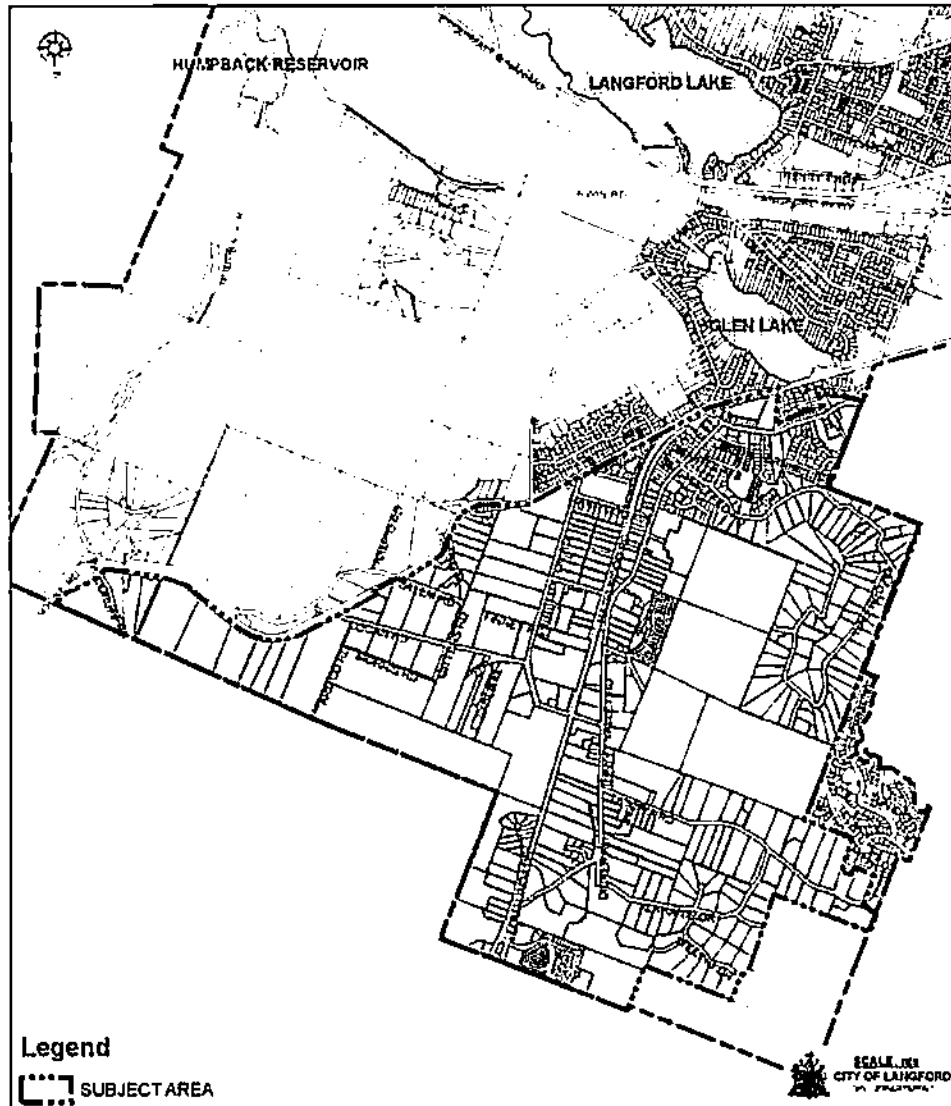
To this end, the City of Langford sought to bring together people from a wide range of backgrounds and interests – both residents and land owners from across the South Langford area – in order to build a vision for the future development of the South Langford community.

1.2 NEIGHBOURHOOD BOUNDARIES

The South Langford Neighbourhood is a group of many smaller neighbourhoods that cover approximately 6.4 km² (2.5 miles²) of land within the City of Langford. For the purpose of the planning process, the South Langford area was defined, approximately, as that portion of Langford that lies south of Highway No. 14 (Sooke Road)¹. The entire South Langford Neighbourhood is within the Capital Regional District's Regional Urban Containment and Servicing Policy Area as designated in the Regional Growth Strategy.

Figure 1: Map showing the boundary of the South Langford Neighbourhood

¹ There are some exceptions to this. Most notably the Sooke Road commercial core which includes lands on both the north and south side of Highway № 14, and the Sunheights neighbourhood which was excluded as development there has largely been completed.



1.3 REGIONAL AND CITY CONTEXT

The City of Langford is a rapidly growing community of 22,000 people in the Westshore area of Capital Regional District in British Columbia. The Regional Growth Strategy (Capital Regional District 2003) calls for approximately half of all regional growth to occur in the west shore, and the majority of that growth to be in the City of Langford.

1.4 NEIGHBOURHOOD PROFILE

The South Langford Neighbourhood offers a variety of landscapes, and is punctuated by areas of low elevation and steep slopes. The lower elevations are located in close proximity to the Bilston Creek watercourse and its tributaries. The areas that fall within the 200-year floodplain are particularly prominent in the southwest along Luxton Road and Logan Road. Bilston Creek has been identified as an important environmental asset, both by residents of the South Langford Neighbourhood and residents of Langford in general.

Steep slopes exist along the southwestern border of Langford (a border shared with the District of Metchosin) and atop Triangle Mountain. Most steep slope areas are undeveloped, although some are within large lots on Walfred Road (Triangle Mountain).

A significant portion of South Langford has been identified (BC Conservation Data Centre and Langford's OCP) as areas with sensitive ecosystem and areas of wildlife habitat and biodiversity. Some of these areas fall within existing and proposed City-owned parks, many are in undeveloped large lots, while several ecologically significant lands fall within some of the developed areas of the valley, particularly in the Walfred Road and Klahanie Drive areas of South Langford.

Much of South Langford is relatively rural and, as of September 2005, approximately 181 acres from South Langford fall within the Agricultural Land Reserve (ALR). Lands within the ALR are designated "RURAL" in the South Langford Neighbourhood Plan.

The northern boundary of the South Langford Neighbourhood is marked by a commercial area that runs along Sooke Road, intermittently from Jacklin Road to Glen Lake Road. There is also a small commercial node on Happy Valley Road at Flatman Avenue. There is an area of business park/light industrial uses on the south side of Sooke Road west of Luxton Road.

There are some areas of South Langford that have been developed to accommodate smaller lot residential developments. These are located mainly in the area north of Marwood Avenue, near to Happy Valley Road and Sooke Road. There are also denser areas of comprehensive residential development in the Olympic View Golf Course and Valley View (at the southern end of South Langford, adjoining the District of Metchosin) developments.

1.5 COMMUNITY VISION FOR SOUTH LANGFORD

Over the course of the SLNP planning process, residents and land owners have built a collective vision to guide their land use and implementation decisions for the South Langford Neighbourhood Plan.

The OCP anticipates that neighbourhood plans will be prepared from time to time (see Policy Section C) and that these plans will be appended to the OCP. To date, there have been local area plans created for areas such as Bear Mountain Estates and the Westhills neighbourhood.

The concept plan presented in this plan is intended to replace the preceding land use concept as outlined in Maps 2 and 3 of Bylaw No. 150 (Langford's OCP).

2.0 PLANNING PRINCIPLES

2.1 TEN GOALS FOR THE SOUTH LANGFORD NEIGHBOURHOOD PLAN

There are 10 goals that were originally proposed for the SLNP planning process, and presented to the South Langford community at a Public Workshop on July 7, 2005:

1. Ensure a high-quality, sustainable living environment that meets the needs and values of area residents.
2. Preserve the positive characteristics of Happy Valley.
3. Retain the Valley's environmentally sensitive areas and its viable agricultural land.
4. Create strategic greenbelts dedicated to wildlife corridors, recreation and trails.
5. Ensure that development patterns reflect the terrain of Happy Valley.
6. Share the market-valued financial benefits of development among all of the area's owners, by allowing transference of development rights between properties.
7. Include a vibrant commercial area along Sooke Road and within each neighbourhood.
8. Create a good network of transportation corridors, including connections for pedestrians, cyclists and motorists.
9. Incorporate a Happy Valley Safe Routes to School Plan.
10. Include a clearly-defined implementation strategy in the Happy Valley Plan.

3.0 PLANNING PROCESS

3.1 STEERING COMMITTEE

On the authority of Council, the South Langford Neighbourhood Plan Ad Hoc Steering Committee was struck in July 2005, and met for the first time in August 2005. As per the Terms of Reference for the Committee, its mandate has been to provide stakeholder perspective on the SLNP planning process. The Committee's objectives have been:

- To oversee the City's dissemination of information to community residents and land owners on the SLNP process, land use options and implementation strategy options;
- To monitor progress of the development of the South Langford Neighbourhood Plan; and
- To develop recommendations for consideration by the Planning and Zoning Committee on the following issues, as they relate to the South Langford Neighbourhood Plan:
 - Planning principles
 - Planning process
 - Land use and density
 - Commercial revitalization
 - Implementation of the SLNP

3.2 PUBLIC WORKSHOP

On July 7, 2005, the City of Langford hosted a public workshop for South Langford residents and land owners. It was attended by approximately 250 people. In addition to a presentation by staff and a question and answer period, workshop participants were also given the opportunity to provide their input on a number of themes:

- Land use and density
- Commercial development
- Environmentally sensitive areas and wildlife habitat
- Recreation and trails
- Heritage preservation
- Stormwater management and riparian protection
- Traffic management

This feedback from South Langford residents was used to create the initial three development concept options that were considered by the community in September and October, 2005.

A final public Open House was held on May 31st, 2006 to review the entire South Langford Neighbourhood Plan process to date, and to seek broad public support for taking a blended neighbourhood concept plan forward for Council's consideration.

On June 24th, 2006, the Planning and Zoning Committee of Council endorsed the South Langford Neighbourhood Plan, and recommended that Council proceed to create a bylaw to enact this plan.

As the South Langford Neighbourhood Plan has evolved, and as further information has been revealed, minor revisions to the plan and refinement of the plan has occurred since the public open house on May 31st.

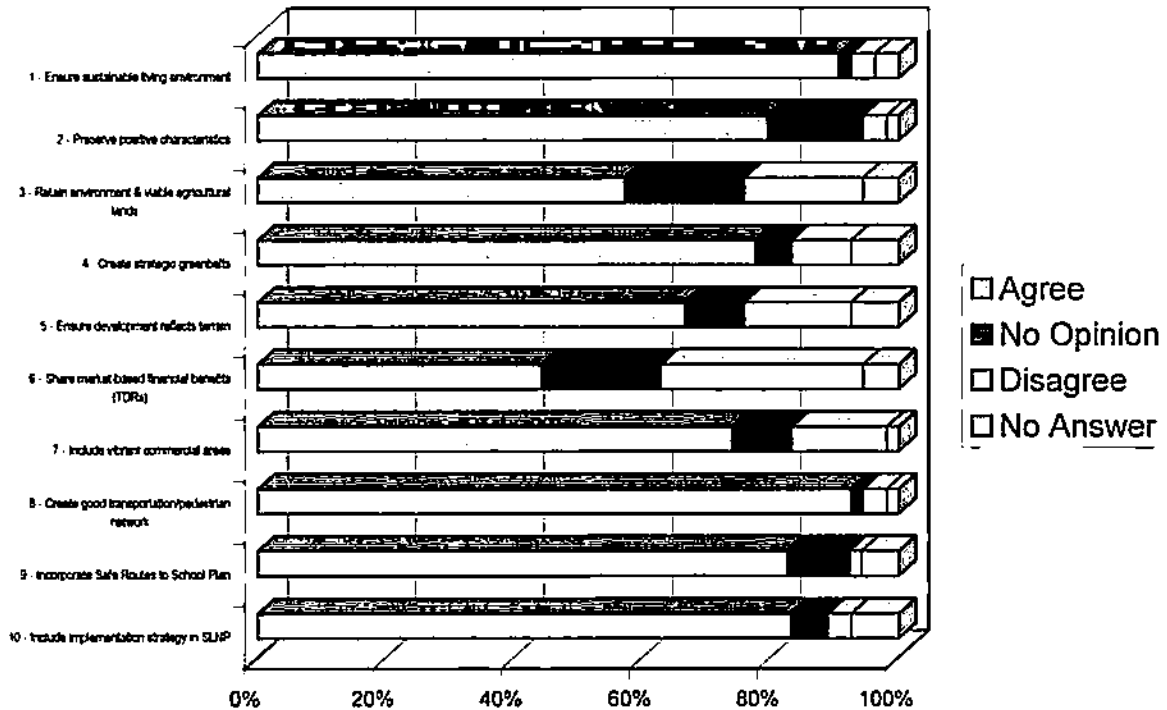
3.3 SURVEYS

Two public surveys were conducted by the City of Langford during the South Langford Neighbourhood Planning Process. The first survey was an informal survey made available to attendees of the July 7th, 2006 Public Workshop and via the City's website.

3.3.1 INITIAL SURVEY

The main goal of the initial survey was to gauge support for the 10 proposed planning goals for the South Langford Neighbourhood planning process. Approximately 29 hard copies of this survey were completed and submitted by workshop participants in the month following the workshop. Another 25 surveys were completed online. While 54 completed surveys represent a small portion of the 250 workshop participants, and an even smaller portion of the hundreds of residents and land owners in South Langford, the survey results provided the City with good feedback on implementation options for the South Langford Neighbourhood Plan. Overall, the 54 completed informal surveys generally showed strong support for all of the proposed goals, except for the proposal for the use of transferable development rights (TDR). Survey participants also used the informal survey as an opportunity to ask insightful questions about the potential development of a TDR program, and made useful suggestions in support of the SLNP planning process.

Figure 2: Survey support for 10 SLNP Goals



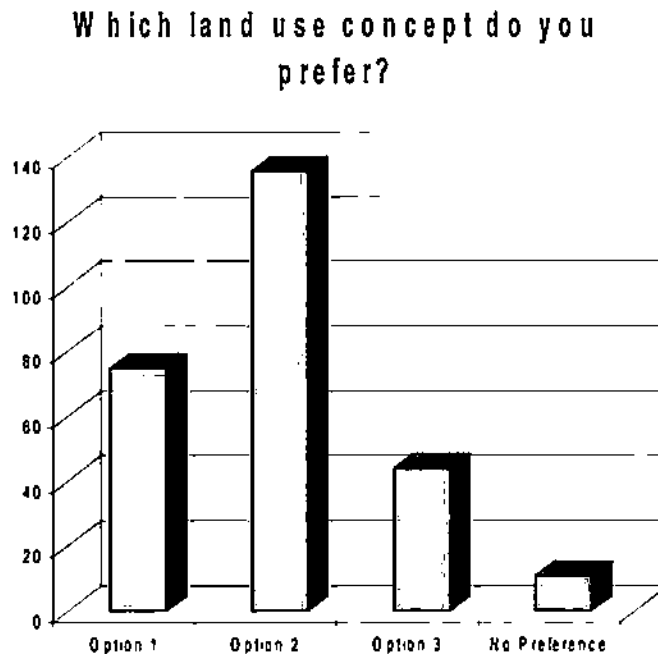
3.3.2 SECOND SURVEY

The second survey was a more formal appraisal of public opinion that involved the mailing of 811 surveys to residents and property owners in the South Langford Plan Area. The major goal of the second survey was to gather feedback on the three proposed land use options and the proposed Sooke Road Revitalization Plan. The list of survey recipients was taken from a list of all registered properties and property owners in the South Langford Plan Area, as compiled by the Land Title and Survey Authority of British Columbia in February, 2006.

Of the 760 surveys that were mailed out, 266 (35%) completed surveys were submitted to City Staff and 41 (5%) were returned by the Post Office because of incorrect addressing as of May 5, 2006.

A majority of respondents to the survey stated that they preferred Land Use Option 2 over the other options, giving it 136 preference votes (51%), while Option 1 was second with 75 preference votes (28%), and Option 3 (status quo) was third, receiving 44 preference votes (8%). Eleven (4%) submitted surveys did not specify which land use option they preferred, or did not prefer any of the proposed land use options.

Figure 3: Second Survey: Support for Development Concepts



The survey also asked respondents whether they supported the conceptual plan for the revitalization of Sooke Road between the intersections with Jacklin Road and Happy Valley Road. 193 (73%) respondents stated that they supported the revitalization plan, while 28 (10%) stated that they did not, and 45 (17%) respondents did not state whether they supported the Sooke Road Revitalization Plan as presented in the survey mail out.

In addition to the quantitative results, the South Langford Neighbourhood Plan Survey also solicited a large amount of qualitative information from respondents as presented below.

Survey respondents provided a diverse range of suggestions associated with the proposed land use concepts as well as a range of other issues making it difficult to summarize. However, the following recurring themes emerged from the respondents as suggestions for the future of the South Langford Neighbourhood:

- *Proposed Luxton Road Light Industrial Area*

A number of survey respondents provided a negative opinion of the "Light Industrial" designation in Land Use Concepts 1 and 2, while only 1 respondent stated support.

- *Development in the Walfred Road and Klahanie Road Large Lot Residential Areas*

Written comments and suggestions were split between those who wanted to see increased development in the Walfred Road and Klahanie Road large lot residential areas and those who did not. Roughly half of the respondents who stated that they wanted increased opportunity for residential development in these areas stated that they wanted only a moderate increase in development that would not greatly change the rural residential character of the neighbourhood. A

sizeable number of comments were received from residents living along Walfred Road that indicated that they were in favour of increased development potential. Several comments that were in support of increased development were also received from residents living along Willing Drive. However, comments from those who did not want increased development were also received from residents in these areas.

- *Methods of Park Acquisition*

Several survey respondents suggested that parks should be purchased by the City at fair market value or through other means that protected the private property rights of the owners of the lands that were designated as parks in proposed land use concepts 1 and 2.

- *Increased Density Near Happy Valley School*

Survey respondents have repeatedly suggested that increased density in the form of townhouses and condominiums be developed in areas adjacent to Happy Valley School.

- *Environmental Protection*

The largest amount of comments and suggestions concerning the proposed land use concepts concerned the protection of the environment, green space and wildlife habitat. This suggests that a large proportion of the survey respondents felt that the South Langford Neighbourhood Plan should be designed in such a way that ecological resources and processes are preserved into the future.

- *Opportunities for Development*

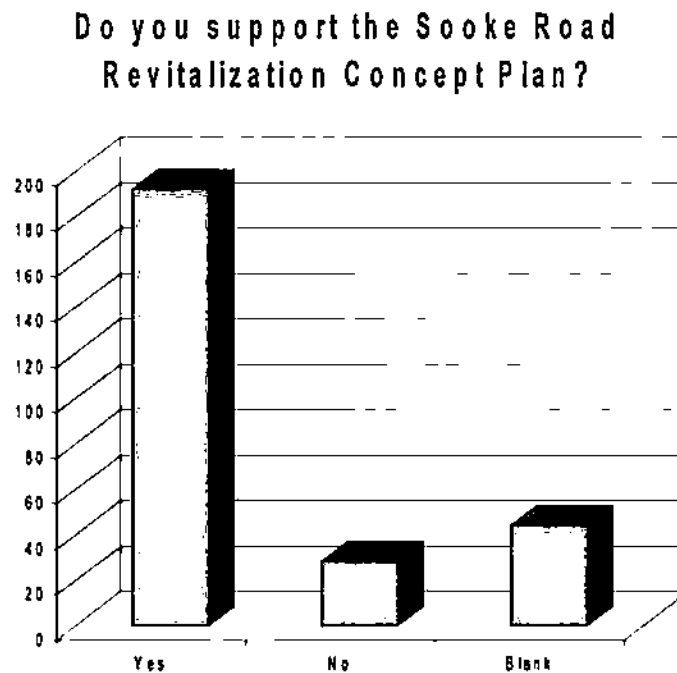
Many suggestions were made regarding the amount of development potential represented in the two new land use concepts and the status quo OCP designations. Comments regarding development potential were equally split between those that wanted much higher and moderately higher development potential and those who wanted lower development potential or no increased commercial development or residential densities beyond the current OCP designations.

- *Smart, Sensitive Growth*

A number of respondents suggested that the South Langford Neighbourhood Plan should ensure that development take place in a manner that is sensitive to aspects of the local environment such as flood plains and that it positively impacts the liveability of the area through the provision of traffic calming, sidewalks, availability of transit and other services, good neighbourhood aesthetics and unique built environments.

The second survey also asked respondents to check a box, which indicate whether or not they support the Sooke Road Revitalization concept. The survey then asked respondents to make suggestions regarding the proposed Sooke Road Revitalization Plan.

Figure 4: Second Survey: Support for Revitalization Concept Plan



- ***Strong Support for the Plan***

Many positive comments were made regarding this plan that reflected the overwhelming support it received when respondents were asked whether they supported the plan or not. Respondents repeatedly stated that improvements to the area's streetscape through the provision of sidewalks, trees, and trails, as well as redevelopment of fronting properties, traffic calming and a general "clean-up" of the neighbourhood.

- ***Concerns***

By far the most common negative comments regarding the Revitalization Plan for Sooke Road were those from people who do not want a traffic circle installed at the intersection of Sooke and Happy Valley Roads. Second most common were comments on the need to improve safety for pedestrians, motorists and bicyclists at the intersections between Sooke Road, the Galloping Goose Trail and Happy Valley Road. A large number of concerns relating to the potential for increased traffic congestion and traffic were also received from respondents. Several other respondents also voiced concerns regarding land expropriation, and the potential for increased pollution and noise.

- ***Support for Multi-storey Development***

Another common theme among a number of the comments was support for the construction of limited multi-storey residential and mixed use buildings along Sooke Road. Some respondents specified that they would like to see "high-rise" type development, along Sooke Road. A small number of comments did not support "high-rise" multi-storey development along Sooke Road.

- *Alternatives and Suggestions*

The last section of the survey asked respondents to provide any alternative suggestions for the future development of the South Langford Area. Fewer comments were made in this section than in others and responses were very diverse and in many cases generally reiterated the comments and suggestions that were left in other areas of the survey.

Suggestions in this section included proposals for a small private plane airport, community care facility, a park and green corridor behind Happy Valley School, municipal support for small-scale agribusinesses, engineered solutions to allow floodplain development, a park at the top of the hill in the Walfred Road Area, larger tree and habitat protection areas, a traffic circle at the intersection of Walfred Road and Cuaulta Crescent, replacement of the gas station on Sooke Road, improvements to the road system and water mains, a walking trail in the Desmond Crescent and Latoria Road area. The majority of these issues can be addressed through rezoning, without making any significant changes at this time to the plan as proposed.

As illustrated by the input outlined above, the second more formal survey provided a wealth of information regarding stakeholders opinions on future of the South Langford Neighbourhood. Much of this information was useful in guiding the final drafts of this plan.

3.4 WEBSITE

The City of Langford has regularly used its homepage (www.cityoflangford.ca) to give updates on the SLNP planning process. All of the information that has become available over the course of the planning process (including a Powerpoint presentation, a brochure, poster boards, and Word documents) has been made available for download from online. The City's website has also provided the public with an opportunity to respond to the survey and to give their input online for Frequently Asked Questions and on a discussion board. The discussion board was particularly useful in stimulating constructive deliberations on the three initial development concept options for the South Langford area.

3.5 UPDATES BY MAIL AND E-MAIL

Residents and land owners in the South Langford area were notified by mail of the SLNP planning process and the public workshop to be held on July 7, 2005. A notice of this workshop was also made in the local newspaper, welcoming all interested participants.

Once the three development concepts were drawn up and approved by the SLNP Steering Committee, these were also distributed by mail and by e-mail to all residents and land owners who had expressed interest in the planning process, and provided their contact information in person (at the workshop or at City Hall), by telephone, or online.

3.6 OCP AMENDMENT PROCESS

The OCP amendment process was first set in motion by a Staff Report to the Planning and Zoning Committee on April 25, 2005, which set out the process, timeframe and budget for the SLNP. The OCP amendment process to support the adoption of the South Langford Neighbourhood Plan returned to Council on July 6th, 2006 with the submission of a staff report to the Planning and Zoning Committee (dated July 24th, 2006) and a recommendation from that Committee to proceed to bylaw.

The South Langford Plan was reviewed for the first time in its entirety by Council in August, 2006.

4.0 LAND USE AND DENSITY

The overall development concept for the South Langford Neighbourhood is illustrated by the Growth Management and Land Use Strategy of the Official Community Plan, as amended by Map 1 attached to and forming part of this bylaw as Appendix "A". This overall development concept provides a general indication of future land use and density. As with the Official Community Plan in general, the development concept illustrated by Map 1 is not intended to be implemented by amendments to Langford's zoning regulations immediately upon the adoption of this Plan, rather the timing of zoning decisions will depend on various factors including: the availability of services, public facilities such as schools and parks, transportation networks, the objectives of property owners and community opinion. Also, as with the OCP in general, it is anticipated that Council will, from time to time, adopt amendments to zoning regulations or revise the Zoning Bylaw in its entirety to improve the clarity and formability of the regulations. It is not the intention of this plan that such amendments will implement any or all of the land use designations set out in the development concept.

The following are specific policies pertaining to the South Langford Neighbourhood area as defined by Figure 1 (see above). These policies in no way diminish or alter either other areas of Langford, or the effect of other policies contained within the Langford Official Community Plan which may have impact on, or relate to lands within the South Langford Neighbourhood area.

4.1 DEFINITIONS

In brief, and in addition to the definitions found in Section 2.01 of Langford's OCP, the terminology used on the Concept Plan is defined as follows:

Country Residential – applies to that area adjacent to Walfred Road, south of Cuaulta Crescent, and that area bounded by Luxton, Marwood, Englewood and Hazelwood roads. Subject to the provision of sewer services, water, a road network and emergency access, the preservation of designated sensitive ecosystems, areas of steep slopes and the provision of a satisfactory interface fire hazard plan and archaeological impact assessment, a maximum density of 1 lot per 1 acre will be applicable. Clustering of density using density lot averaging may be considered by Council to minimize impact in this area;

Large Lot Residential – applies to areas where existing development patterns allow for limited infill development opportunities. A maximum density of 1 lot per 1000 m² (4 units per acre) is applicable.

Urban Residential – applies to areas where the availability of municipal services allows for one- and two-family residential dwellings at densities that are similar to areas of existing, conventional (not small lot) residential development in Langford. A minimum lot size of 550 m² (5920 ft²) is applicable.

Intensive Residential – applies to areas where the availability of municipal services allows for a variety of residential development that is more intensive than Urban Residential, and that may include single family residential dwellings without secondary suites, townhousing and attached housing. A maximum density of one unit per 250 m² (2690 ft²) is applicable.

Comprehensive Development – applies to areas where a mix of uses including detached, small lot, attached, townhousing and apartment type housing as well as some commercial and institutional uses may be considered. Density of development shall be determined by zoning.

Neighbourhood Commercial – defines an area that is intended to support commercial business activities that are focused on a local, neighbourhood market. Density of development shall be determined by zoning.

Mixed Use Commercial – defines an area that is intended to support principally multi-storey, multi-family residential development with ground oriented commercial activity. Uses that are either single-storey, wholly residential or wholly commercial may be considered. The use and mix of uses along with the density of development shall be determined by zoning.

Rural with Heritage Values – applies to areas where the traditional land use of a property is of significance to the history and culture of Langford. An average density of 1 lot per 12 ha (30 acres) applies to this designation. Council may support rezoning which supports the ongoing traditional use of lands within this designation.

Urban Residential – applies to areas where the availability of municipal services allows for a variety of residential development that is more intensive than suburban residential, and that may include single-family residential dwellings without secondary suites, townhousing, and attached housing. A maximum density of one unit per 200 m² (2152.85 ft²) is applicable.

5.0 ECOSYSTEMS

The protection of natural ecosystems and highly valued natural areas has been among the primary goals of the South Langford Neighbourhood Planning Process (see Section 2.1 above). To meet these objectives, studies were undertaken to identify important habitat and areas of environmental sensitivity, and this information was then used to designate areas of increased development potential in a manner that helps to ensure the protection of large areas of open space and important ecosystems.

5.1 GREENWAYS AND OPEN SPACE

The opportunity to create wildlife corridors and natural greenways was given careful consideration in the preparation of this plan. Maintaining connectivity between Triangle Mountain and the southern end of the municipality for wildlife can be accomplished through the land use and density designations of lands on Triangle Mountain, along Latoria Road and in the Klahane Estates.

5.1.1 *Council will continue to seek opportunities, as lands develop in accordance with this plan, to create greenways and preserve open spaces for both active and passive recreational uses and identified environmental needs;*

5.1.2 *All new development outside of the town centre area, as defined on Map 3, shall provide twenty-five percent (25%) open space and this may include lands up to 30% slope. This provision does not apply to any lands already zoned for R1 or R2 densities;*

5.2 PROTECTION OF ENVIRONMENTALLY SENSITIVE AREAS

The Langford Plan identifies and protects various sensitive ecosystems through the Development Permit process (see Part IV of Langford's Official Community Plan). Nothing contained within this plan is meant to contradict or diminish these existing protective measures.

Those areas, designated as Country Residential and Large Lot Residential that are adjacent to Walfred Road, and south and east of Jacklin Road, are designated as an area of Potential Wildlife Habitat and Biodiversity and a Development Approval Information area, with the objective of preserving and enhancing sensitive ecosystems and wildlife habitat wherever possible and to cluster development away from these areas. A Development Permit, or specific exemption from the requirements of a development permit, is required before land within this designation is altered in any way. The justification and objective for this designation is to ensure that Council has the ability to secure the necessary information for considering development applications in order that development is well managed and that rare, endangered, or sensitive ecosystems, plants and animals are protected.

6.0 PARKS, RECREATION AND COMMUNITY SERVICES

6.1 SOUTH LANGFORD PARKS AND RECREATION FACILITIES

South Langford offers an important recreational network for both the community and the region. There are a number of pedestrian trails that wind through the South Langford area, particularly in the steep slope areas that fall between Happy Valley Road and Walfred Road. The Galloping Goose Regional Trail acts as a central spine that holds the South Langford area together. The Olympic View Golf Course anchors the southeastern corner of South Langford. Since incorporation in 1993, the total area of City parks within South Langford has been increased by 31 acres to 36.7 acres.

6.1.1 *In addition to meeting the objectives of Policy Section G, Council will consider accepting donations of park land off-site, through a transfer of development rights (ie: not part of the lands being considered for development) at a rate to be determined by Council;*

6.2 CYCLING ROUTES

6.2.1 *Council will seek to identify and either obtain, construct or enhance cycling routes in the South Langford Area as part of an overall community network of trails and walking paths. Cycling routes may be either as identified in the City's Bicycle Plan, or in addition to those routes identified in the Bicycle Plan.*

6.3 TRAIL NETWORK

6.3.1 *Council will seek to identify and either obtain, construct or enhance trails within in the South Langford Area as part of an overall community network of trails and walking paths. These trails and walking paths may be either as identified in the City's Trail Plan, or in addition to those routes identified in the Trail Plan.*

6.4 HERITAGE

Residents have identified certain areas within the South Langford Neighbourhood as areas with heritage value. These areas include: the Luxton Fairgrounds, the Lavender Farm on Happy Valley Road and the SVI Rangers complex. These sites are also noted on Map 11 with the designation "Rural with Heritage Values".

Developers should be aware that the *Heritage Conservation Act* prohibits the disturbance of any heritage site. If it is suspected that there are any heritage sites on a property, it is strongly recommended that a professional archaeologist be retained to assess the site.

6.5 COMMUNITY SERVICES

6.5.1 *Council will seek appropriate locations for community uses that have a relatively small service area and are closely associated with residential areas, such as churches, schools and group day care centres. These uses may occur anywhere within the area encompassed by the South Langford Neighbourhood Plan, as determined by rezoning;*

7.0 COMMERCIAL DEVELOPMENT AND REVITALIZATION

The following general principles were recurring themes in the public discussion that occurred during the preparation of this plan:

- Commercial development should be limited to the Sooke/Jacklin intersection and Sooke Road from Wilshire or Anders to Happy Valley Road;
- Commercial/Business Park/Industrial development should be limited first to the area west of Luxton Road, south of Sooke Road;
- There should be no other commercial areas in the South Langford Area;
- Commercial areas should be concentrated so that they are compact and vibrant/not spread out;
- Commercial development along Sooke Road should be mixed use (residential apartments over shops and offices);
- A streetscape similar to Goldstream Avenue is desirable;
- Council may consider some multi-storey buildings.

The following specific ideas were put forward by the public at meetings to discuss this plan:

- Access and egress from commercial properties is now unsafe. This should be addressed;
- Commercial area should have a pedestrian scale. Shops should be within walking distance of each other and should relate to the South Langford neighbourhood with pedestrian linkages. More walking areas and more sidewalks will make for healthier living;
- A pedestrian-oriented commercial area, similar to Mattick's Farm, with shopping, benches and a fountain is desirable. Strong support for a pedestrian-commercial "axis" running from Glen Lake to playing fields in Isabell/Walfred development. The concept of a pier at Glen Lake and a Park at Sooke/Walfred/Isabell as the two terminal points of this axis was viewed favourably;
- There should be more trees planted in the commercial area (along Sooke Road);

- Heavy industrial uses (steel mills, fabricating plants) should be excluded from the Sooke Road Industrial/Business Park area;
- Commercial uses in the Sooke Road Industrial/Business Park area should not have direct access from Luxton Road;
- Uses in the commercial area to be small retail shops and services with residential above;
- Frontage (backage) roads preferable to multiple driveway accesses onto Sooke Road.

7.1 SOOKE ROAD COMMERCIAL REVITALIZATION

In 2002, City of Langford staff undertook a preliminary study of the Sooke Road Commercial Area, and possible opportunities for commercial development and revitalization of this area. This analysis included a design workshop with City staff and contracted designers that resulted in a conceptual plan for the revitalization of Sooke Road (see Schedule "A" attached).

The open house held at the legion on July 7th, 2005, offered residents of the South Langford Neighbourhood area an opportunity to discuss commercial development and/or revitalization in the south Langford area in general, and in particular to discuss the conceptual plan that had been prepared for the Sooke Road corridor. Comments with regards to commercial development were very positive. The public were generally supportive of the 2002 conceptual plan for the revitalization of Sooke Road.

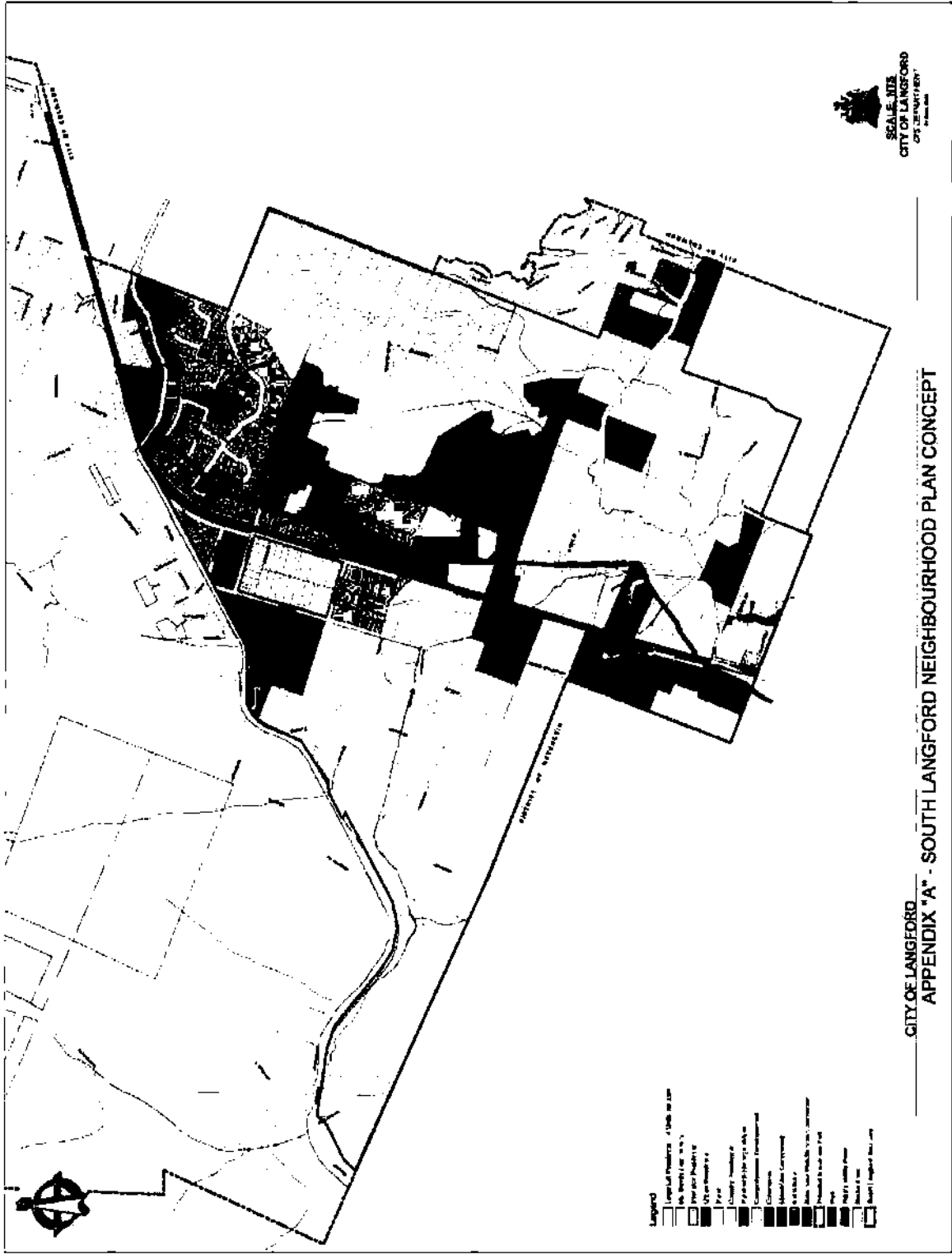
- 7.1.1 Council will seek opportunities to create notable gateway features at both the eastern and western entrances to the Sooke Road commercial area;**
- 7.1.2 Council encourages mixed use commercial and multi-family residential development in the Sooke Road commercial area. Multi-storey, multi-family residential buildings with limited or no commercial uses may be considered at zoning.**
- 7.1.3 Building height will be determined by rezoning. A maximum building height of four storeys is generally, but not exclusively, considered appropriate for the Sooke Road commercial area;**
- 7.1.4 Council will seek opportunities to create a streetscape similar to that which exists on Goldstream Avenue, complete with paver sidewalks, boulevard plantings, medians and where possible and practical underground wiring;**
- 7.1.5 Council encourages development that reflects the rural/agrarian history of South Langford in general, and in particular Happy Valley and Luxton, through the use of architectural details, colours and materials. Towards this aim, Council may establish specific design guidelines for commercial development in the Sooke Road commercial revitalization area;**
- 7.1.6 In accordance with the conceptual plan for the revitalization of Sooke Road, Council will seek opportunities to create a pedestrian focussed greenway and view corridors between Fisher's Field and the Sooke Road commercial area and to connect Glen Lake to the Galloping Goose Regional Trail and the Sooke Road commercial area;**

7.1.7 *The redevelopment and revitalization of the Sooke Road commercial area will have a mixed commercial and residential focus, but will also embrace outdoor recreational opportunities. Council will seek opportunities to create a pavilion on the Trans-Canada Trail at or near the Sooke Road commercial area, and possibly integrate a pier on Glen Lake with an outdoor market, outdoor recreation, outdoor restaurants or an outdoor public plaza within the revitalization area;*

7.2 LOCAL BUSINESS PARKS

The South Langford Neighbourhood Plan identifies the emerging business park that exists on the west side of Luxton Road, south of Sooke Road. The plan also identifies an area of Country Residential/Future Business Park in the area bounded by Luxton, Marwood, Englewood and Hazelwood Roads.

7.2.1 *The area bounded by Luxton, Marwood, Englewood and Hazelwood Roads is intended to remain as an area of country residential development until such time as a comprehensive plan for a business park emerges and all of the lands in this area can be considered for rezoning;*



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May 5th, 2008

In-Camera Resolution

- a) That it is the opinion of Council that the public interest requires that persons other than members of Council and Officers be excluded from the meeting to consider confidential information regarding Legal Matters, New Services and Property Acquisition under section 90 (e), (f) and (k) of the Community Charter;
- b) That Council continues the meeting in closed session.